

## INDEX OF SHEETS

SHEET	DESCRIPTION
1	TITLE SHEET
2	TYPICAL SECTIONS/GENERAL NOTES
3-4	PLAN/PROFILE
5-8	INTERSECTION DETAILS
9	PARKING LOT DETAIL
10-12	PAVING/SIDEWALK DETAILS
13	PAVEMENT MARKING DETAILS
15-21	TRAFFIC CONTROL
22-23	EROSION CONTROL
24-27	CROSS SECTIONS

**CITY OF McPHERSON, KANSAS**  
**DEPARTMENT OF PUBLIC WORKS**

**2016 STREET IMPROVEMENTS**

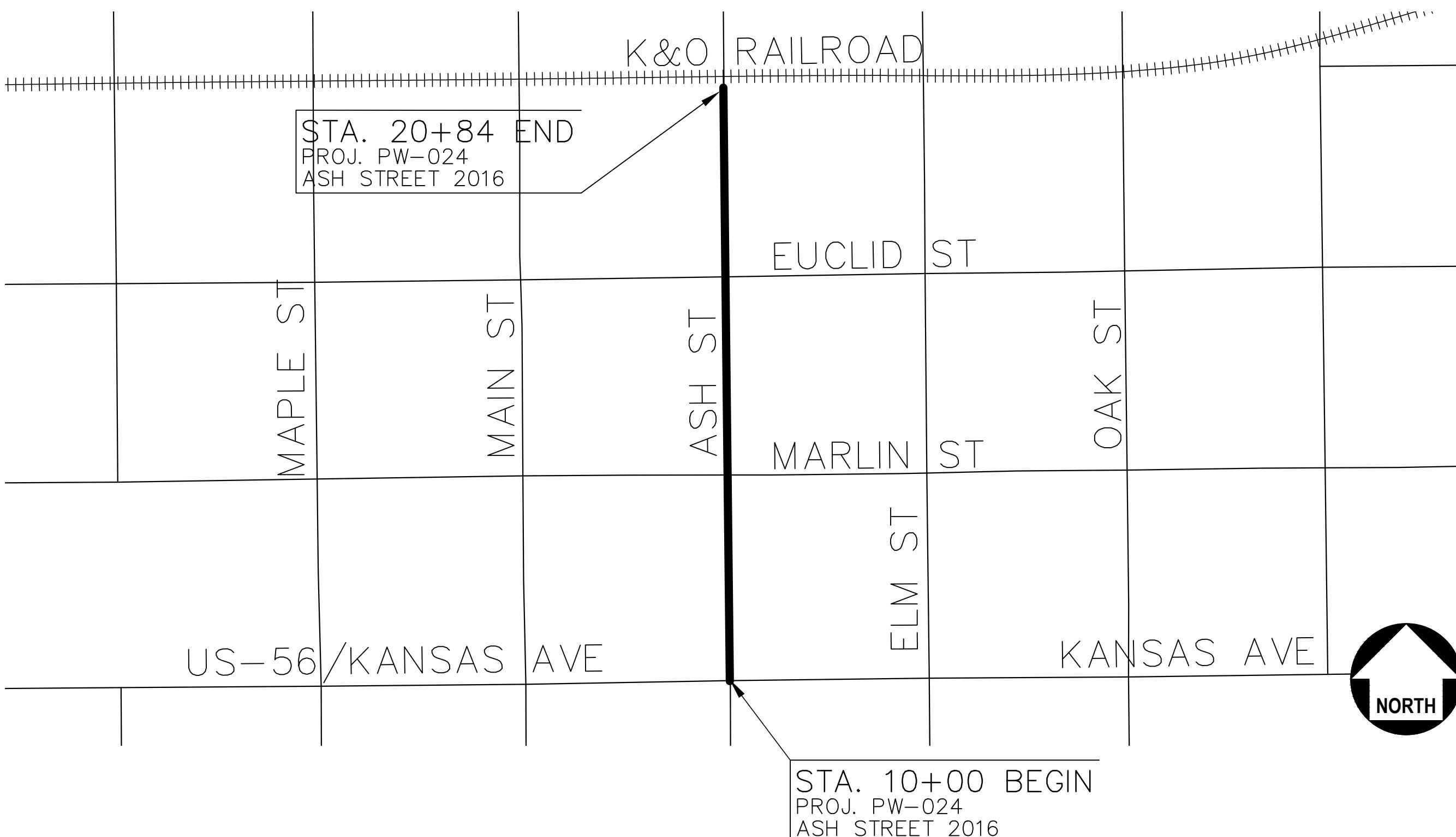
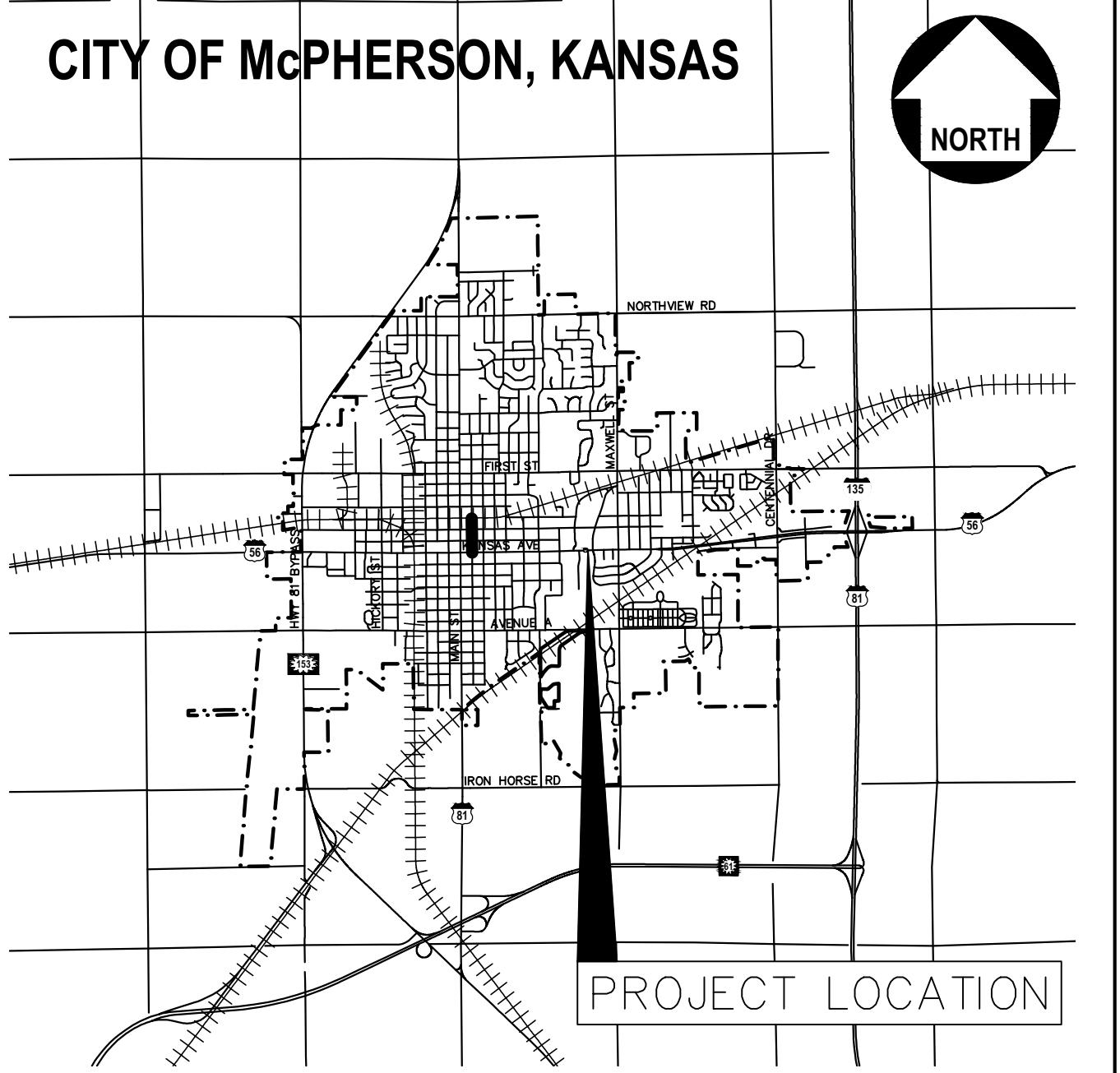
**ASH ST**

**KANSAS AVE TO K&O RAILROAD**

**P.W. PROJECT - 024**

PLANS	BY	DATE
DESIGNED:	JUSTIN WALKER	2/01/2016
DRAWN:	JUSTIN WALKER	3/01/2016
CHECKED:	JEFF WOODWARD	5/03/2016

**P.W. PROJECT - 024**



**MILLING**  
**GRADING (8" GeoGrid/Aggregate Base)**  
**BITUMINOUS PAVEMENT SURFACE (10")**  
**BITUMINOUS PAVEMENT OVERLAY (2")**  
**ADA RAMPS (By City Forces)**  
**SIDEWALK (By City Forces)**  
**OFF-STREET PARKING (By City Forces)**  
**VALLEY GUTTER (By City Forces)**

UTILITY OWNERS

Electric & Water –  
 Board of Public Utilities  
 Mark Wurm, PE  
 401 W Kansas Ave  
 McPherson, Ks 67460  
 620-755-5665

Storm and Sanitary Sewer –  
 City of McPherson  
 Jeff Woodward, PE  
 400 E Kansas Ave  
 McPherson, Ks 67460  
 620-245-2545

Gas Service –  
 Kansas Gas Service  
 Dennis Alexander  
 1644 W Kansas Ave  
 McPherson, Ks 67460  
 620-241-0837 ext 224

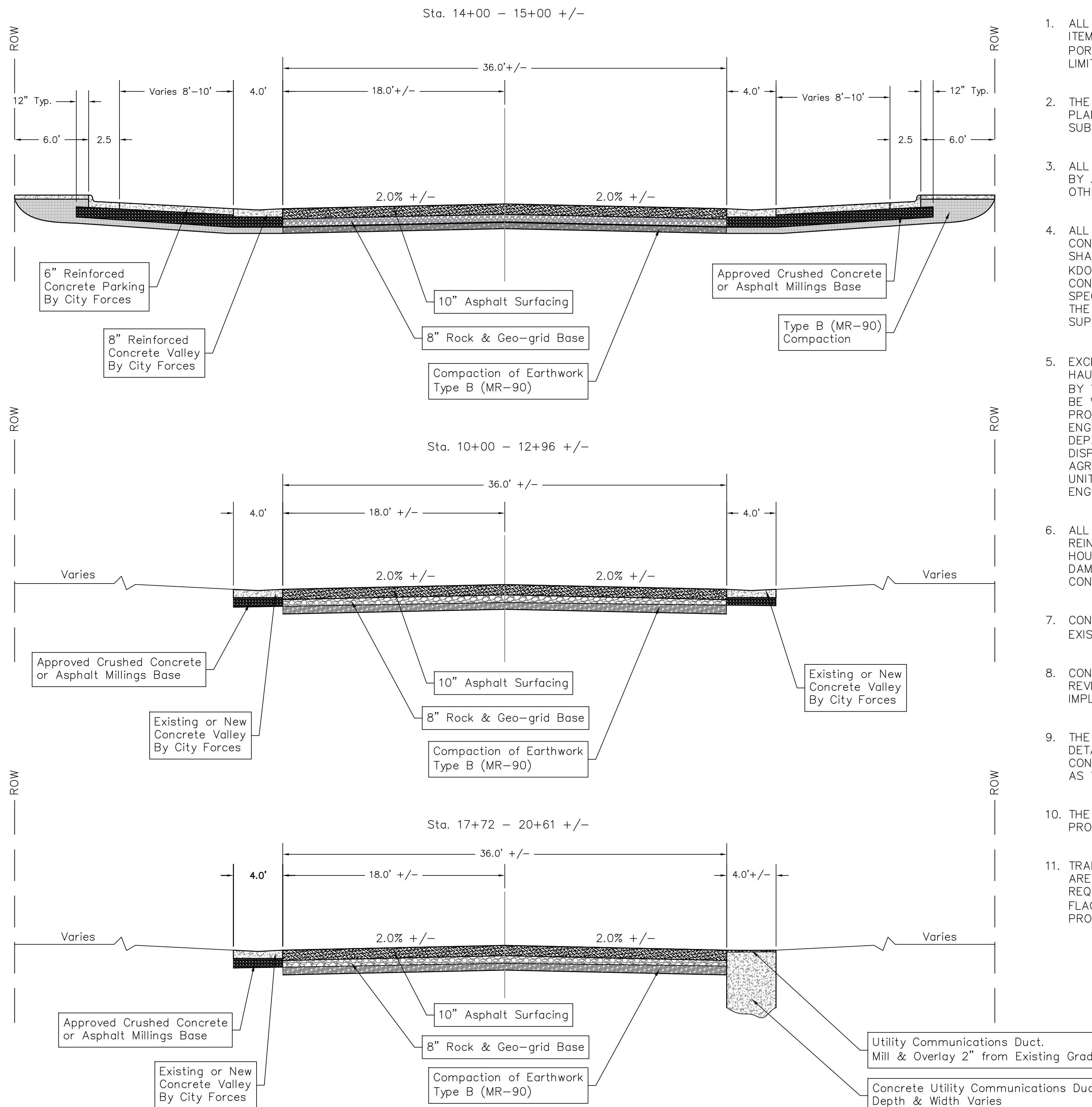
Telephone –  
 AT&T  
 Scott Dunlap  
 137 S 7th  
 Salina, Ks 67401  
 620-665-1939

PLANS REVIEWED BY	DATE	APP'D	DATE	REVISION	BY	APP'D	FINAL PLANS	RECOMMENDED BY:	APPROVED BY:	PROJECT NO: P.W. PROJECT - 024	FILE: PW-024_TitleSheet.dwg
							APPROVED FOR CONSTRUCTION			WORK ORDER NO: -----	PLOT BY: Justin W
							JEFF WOODWARD, P.E.	DATE	LARRY E WEINS	CONSTR. COMPL:	PLOT DATE: 2016-05-06
							DIRECTOR OF PUBLIC WORKS		COMMISSIONER OF STREETS & UTILITIES	FIELD REVISIONS:	SHEET 1 OF 27

STREET DESIGN CRITERIA					
STREET	LENGTH Ft.	LANES	AADT 2013	AADT 2033	% TRUCKS
ASH STREET	1084	2	9590	14250	6%

TOTAL LENGTH: 1084 FT  
0.205 Miles

LANE MILES: 0.410 Miles



## GENERAL NOTES:

1. ALL SAW CUTS SHALL BE FULL DEPTH AND SUBSIDIARY TO OTHER BID ITEMS. FULL DEPTH SAW CUTS SHALL BE USED TO REMOVE ANY PORTIONS OF PAVEMENT AND/OR CURBS AND CUTTER. SAW CUTS FOR LIMITS OF CONSTRUCTION SHALL BE MARKED BY CITY.
2. THE CONTRACTOR SHALL SUBMIT A STORMWATER POLLUTION PREVENTION PLAN FOR KDHE PERMIT PRIOR TO REMOVING ANY SOIL. THE CITY WILL SUBMIT FOR THE NOI.
3. ALL PROPERTY PINS DISTURBED BY CONSTRUCTION SHALL BE REPLACED BY A LICENSED LAND SURVEYOR. THIS WORK SHALL BE SUBSIDIARY TO OTHER ITEMS OF THE CONTRACT.
4. ALL CONSTRUCTION METHODS AND MATERIALS USED IN THE CONSTRUCTION OF THE IMPROVEMENTS COVERED BY THESE PLANS SHALL BE IN ACCORDANCE WITH THE MOST RECENT VERSION OF KDOT'S STANDARD SPECIFICATIONS FOR STATE ROAD AND BRIDGE CONSTRUCTION. CITY OF MCPHERSON STANDARD TECHNICAL SPECIFICATIONS AND CURRENT SPECIAL PROVISIONS AS APPLICABLE TO THE PROJECT OR AS OTHERWISE EXCLUDED IN THESE PLANS OR SUPPLEMENTARY CONDITIONS.
5. EXCESS MILLINGS AND CLEAN CONCRETE RUBBLE (NO SOIL) SHALL BE HAULED TO THE CITY DEBRIS SITE, AT 1320 TREATMENT PLANT ROAD, BY THE CONTRACTOR. ALL OTHER EXCESS MATERIALS (ROCK, ETC.) SHALL BE WASTED AT LOCATIONS FURNISHED BY THE CONTRACTOR OFF THE PROJECT SITE. WASTE LOCATIONS ARE SUBJECT TO APPROVAL BY THE ENGINEER. ALL DISPOSAL SITES MUST BE APPROVED BY THE KANSAS DEPARTMENT OF HEALTH AND ENVIRONMENT. MATERIALS STOCKPILED OR DISPOSED OF IN A FLOOD PLAIN WOULD REQUIRE A KANSAS BOARD OF AGRICULTURE PERMIT. ANY MATERIAL DUMPED IN WATERS OF THE UNITED STATES OF WETLANDS IS SUBJECT TO U.S. ARMY CORP OF ENGINEERS PERMITTING REGULATIONS.
6. ALL EXISTING SIGNS, PARKING STOPS, ARE TO BE REMOVED AND REINSTALLED BY THE CITY. CONTRACTOR SHALL NOTIFY THE CITY 72 HOURS PRIOR TO NEEDING SIGN REMOVAL OR INSTALLATION. ANY SIGN DAMAGED BY THE CONTRACTOR SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.
7. CONTRACTORS SHALL MAINTAIN CONSTRUCTION LIMITS WITHIN THE EXISTING AND/OR PROPOSED RIGHT-OF-WAY AND EASEMENTS.
8. CONTRACTOR SHALL SUBMIT A DETAILED TRAFFIC CONTROL PLAN FOR REVIEW AND APPROVAL PRIOR TO CONSTRUCTIONS AND IMPLEMENTATIONS.
9. THE CONTRACTOR SHALL SUBMIT TO THE ENGINEER, FOR APPROVAL, A DETAILED WORK SCHEDULE OUTLINING ESTIMATED TIME PERIODS FOR CONSTRUCTION OF SEGMENTS. THE WORK SCHEDULE SHALL BE UPDATED AS WORK PROGRESSES, AND AT LEAST UPDATED EVERY 2 WEEKS.
10. THE CONTRACTOR WILL PROVIDE CONSTRUCTION STAKING FOR THE PROJECT.
11. TRAFFIC CONTROL SHOWN IN PLANS AND TRAFFIC CONTROL DEVICES TAB ARE SHOWN FOR INFORMATION ONLY. FLAGGER OPERATIONS SHALL BE REQUIRED ANY TIME THAT TRAFFIC IS LIMITED TO A SINGLE LANE. FLAGGER OPERATIONS AND/OR SINGLE LANE TWO WAY TRAFFIC IS PROHIBITED DURING NIGHT-TIME HOURS.
12. UTILITY POLES WILL REMAIN IN PLACE EXCEPT WHERE RELOCATED BY THE OWNERS. THE CONTRACTOR SHALL BE REQUIRED TO WORK AROUND THESE POLES TO COMPLETE THE WORK.
13. ALL TREES SHALL BE SAVED WITHIN THE RIGHT-OF-WAY. UNLESS NOTED TO BE REMOVED ON THE PLANS OR AS DIRECTED BY THE ENGINEER. SOME HAND WORK MAY BE REQUIRED.
14. ALL CONNECTIONS TO THE EXISTING STORM SEWERS, INLETS, AND MANHOLES SHALL NOT BE PAID FOR DIRECTLY BUT SHALL BE CONSIDERED SUBSIDIARY TO OTHER ITEMS OF THE CONTRACT. PLUGGING OF HOLES IN EXISTING STRUCTURES AFTER REMOVAL OF PIPES AND PIPE ENDS EXPOSED BY CONSTRUCTION SHALL NOT BE PAID DIRECTLY BUT SHALL BE CONSIDERED SUBSIDIARY TO OTHER ITEMS OF THE CONTRACT.
15. THE LOCATIONS OF ALL UNDERGROUND UTILITIES MAY VARY FROM WHAT IS INDICATED IN THESE PLANS. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO COORDINATE WITH ALL UTILITY OWNERS TO LOCATE AND FLAG ALL UNDERGROUND UTILITIES WHETHER INDICATED OR NOT. NO EXCAVATIONS SHALL BE PERMITTED IN THE AREA OF UNDERGROUND UTILITIES UNTIL ALL SUCH UTILITIES HAVE BEEN LOCATED AND IDENTIFIED TO THE SATISFACTION OF ALL PARTIES. ANY DAMAGE TO THE UTILITY SHALL BE REPAIRED OR REPLACED BY THE UTILITY COMPANY AT THE EXPENSE TO THE CONTRACTOR.
16. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE COORDINATION WITH ALL UTILITY OWNERS FOR THE UTILITY RELOCATION AND ADJUSTMENT DURING CONSTRUCTION. CONTRACTOR SHALL PROVIDE CONSTRUCTION STAKING OF THE IMPROVEMENTS SO THAT UTILITIES OWNERS CAN RELOCATE THE NEW UTILITIES OUTSIDE OF THE PROPOSED IMPROVEMENTS. NO ADDITIONAL PAYMENT WILL BE GIVEN TO THE CONTRACT FOR TIME DELAYS OR EXTRA EXPENSE FROM THE COORDINATION AND RELOCATION OF THE UTILITIES.
17. NO WORK SHALL TAKE PLACE ON THE K&O RAILROAD RIGHT OF WAY. THE CONTRACTOR SHALL BE RESPONSIBLE TO CONTACT, WORK, AND COORDINATE WITH THE K&O RAILROAD. NO WORK OR EXCAVATIONS WILL BE ALLOWED WITHIN THE RAILROAD RIGHT-OF-WAY UNTIL A RAILROAD REPRESENTATIVE HAS BEEN NOTIFIED AND A RAILROAD REPRESENTATIVE IS ON SITE. IF REQUIRED, THE CONTRACTOR SHALL BE REQUIRED TO WORK AROUND AND NOT DISTURB THE RAILROAD COMMUNICATIONS OR SIGNAL POLES OR LINES. NO ADDITIONAL PAYMENT WILL BE GIVEN TO THE CONTRACTOR FOR TIME DELAYS OR EXTRA EXPENSE FROM THE COORDINATION OF RAILROAD WORK. THE CITY HAS INITIALLY CONTACTED THE RAILROAD AND NO WORK SHALL TAKE PLACE ON RAILROAD RIGHT-OF-WAY.

## RECAPITULATION OF ROADWORK QUANTITIES

ITEM	QUANTITY	UNIT
BITUMINOUS PAVEMENT (8")	2115	TON
ADDITIONAL 2" BITUMINOUS PAVEMENT	529	TON
MACHINE MILLING (FULL DEPTH)	4413	SY
GEOGRID REINFORCED AGGREGATE BASE (8")	4863	SY
COMMON EXCAVATION	2044	SY
CONCRETE PAVEMENT REMOVAL	89	SY
BRICK PAVER W ASPHALT OVERLAY REMOVAL	450	SY
ADDITIONAL 2" COMMON EXCAVATION FOR ADDITIONAL BASE ROCK	0	CY
SANITARY SEWER MANHOLE ADJUSTMENT	1	EACH
CONTRACTOR CONSTRUCTION STAKING	1	LS
EROSION CONTROL	1	LS
PEDESTRIAN TRAFFIC CONTROL	1	LS
TRAFFIC CONTROL	1	LS
MOBILIZATION	1	LS

EVISIONS BY APP'D Ash Street Partial Reconstruction

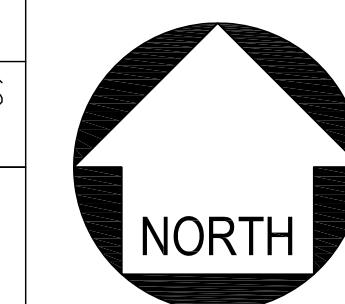
## Ash Street – Typical Sections/Notes/Quarantine

McPherson

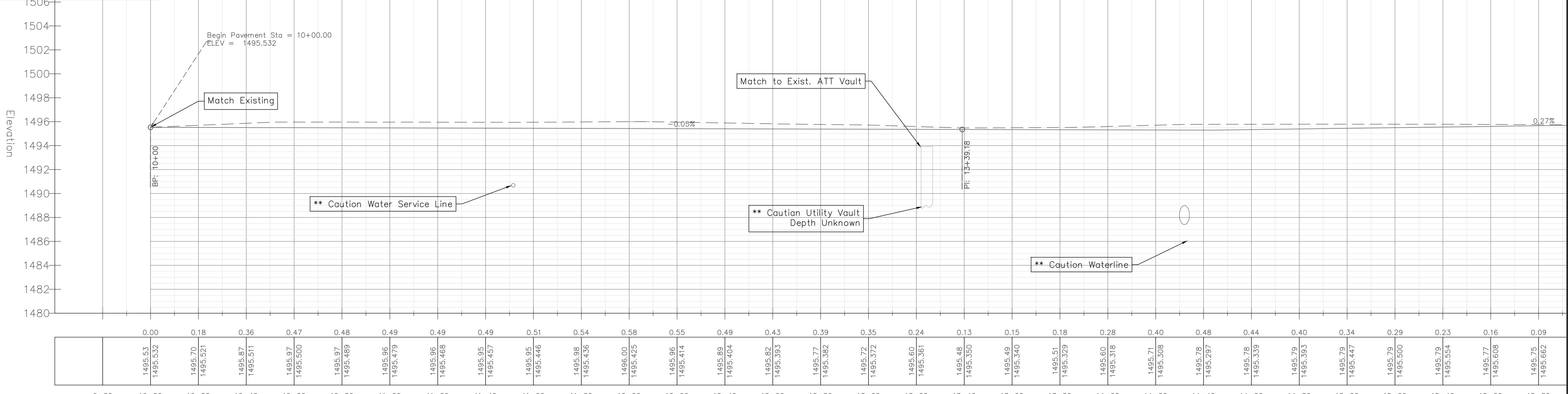
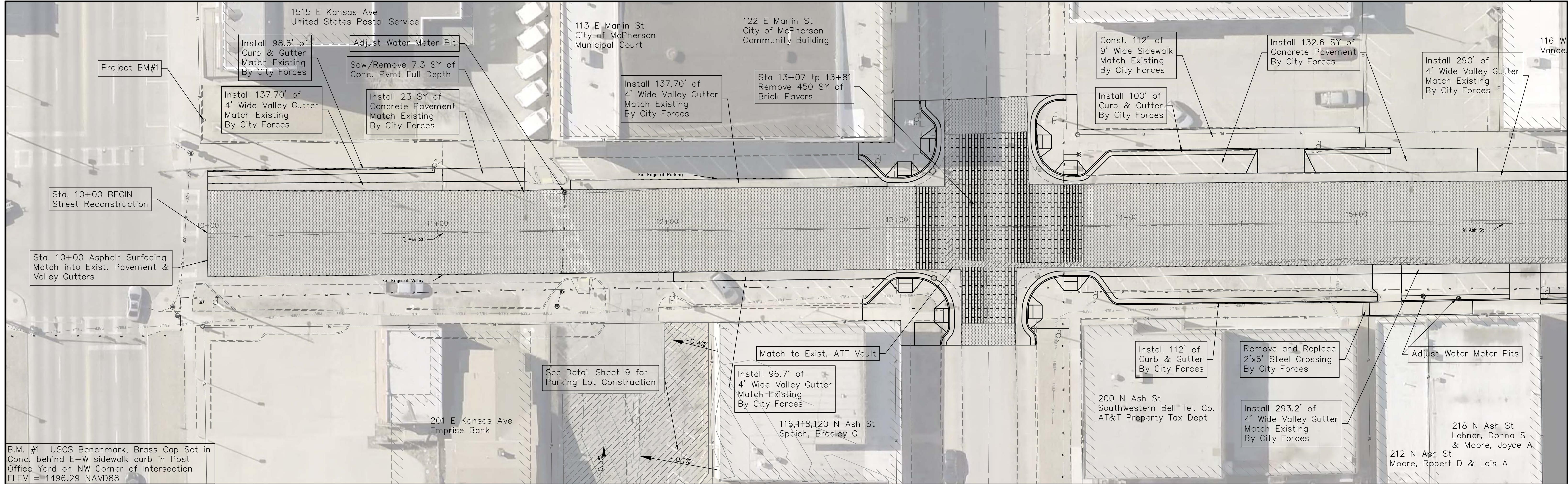
CITY OF McPHERSON, KA  
PUBLIC WORKS DEPT.

# FINAL PLANS

## APPROVED FOR CONSTRUCTION

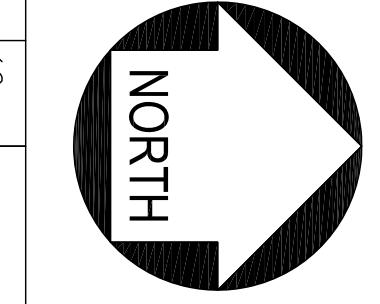


PLANS	BY	DATE
SURVEY:	RichardH/JustinW	1/18/2016
DESIGNED:	JustinW	2/3/2016
DRAWN:	JustinW	3/17/2016
CHECKED:	JeffW	2016-05-10
PLOT BY:	JustinW	2016-05-10
FILE:	2016-05-03_AshStDesign.dwg	



# McPHERSON

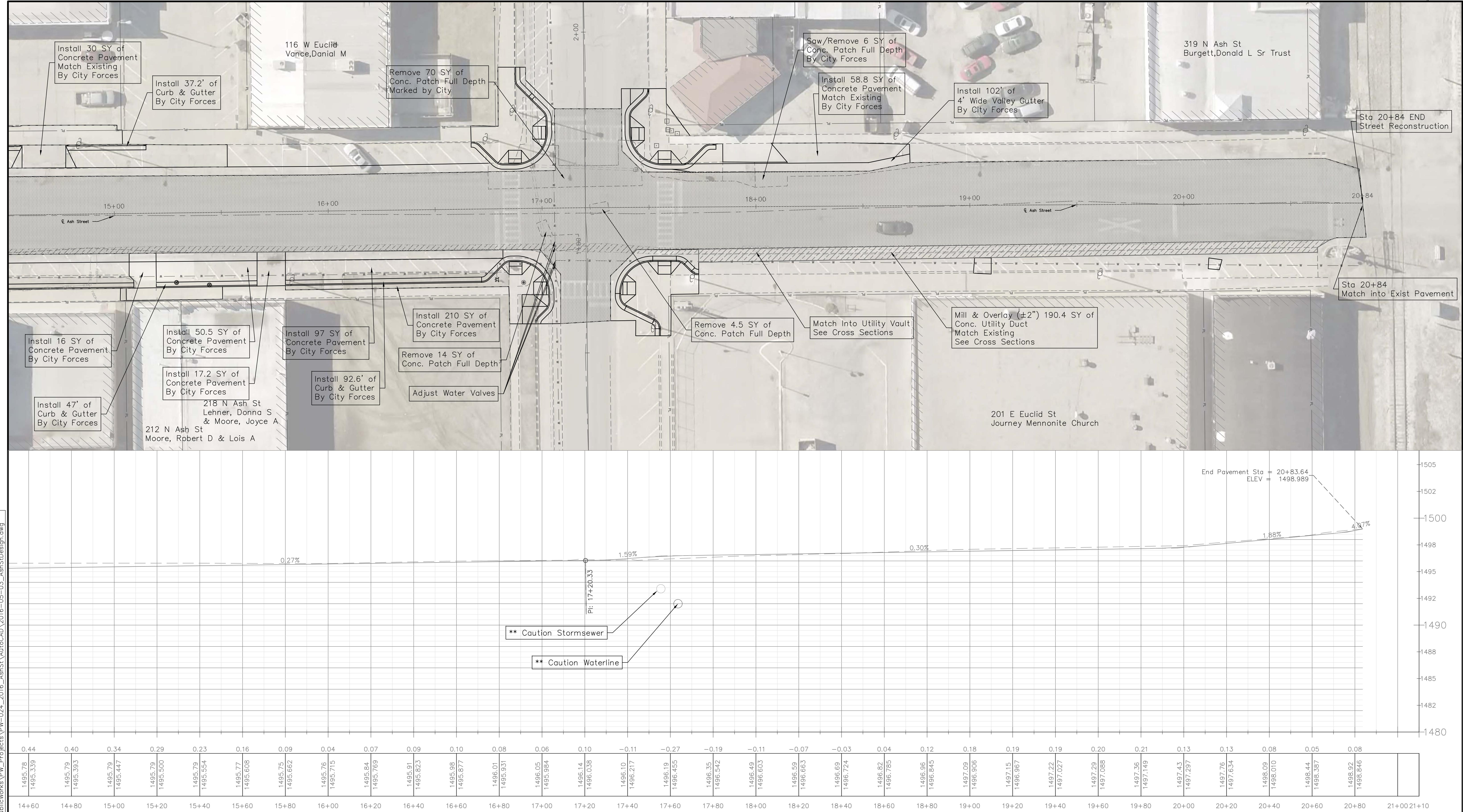
CITY OF McPHERSON, KANSAS  
PUBLIC WORKS DEPT.



# FINAL PLANS

## APPROVED FOR CONSTRUCTION

PLANS	BY	DATE
SURVEY:	RichardH/JustinW	1/18/2016
DESIGNED:	JustinW	2/3/2016
DRAWN:	JustinW	3/17/2016
CHECKED:	JeffW	2016-05-06
PLOT BY:	JustinW	2016-05-06
FILE:	2016-05-03_AshStDesign.dwg	
PROJECT NO:	PW-024	SHEET 3 OF 27



DATE	REVISIONS	BY	APP'D
			Ash Street Partial Reconstruction
			Ash Street from Kansas Ave to K&O RR
			16+00-20+84
			1/32"
			0.05"
			20 40 60 80
			1 2 3 4 1/16"
			VERTICAL HORIZONTAL

ENG  
SHEET

SCALE

1/32"

0.05"

20

40

60

80

1 2 3 4

1/16"

VERTICAL HORIZONTAL

1 2 3 4

1/16"

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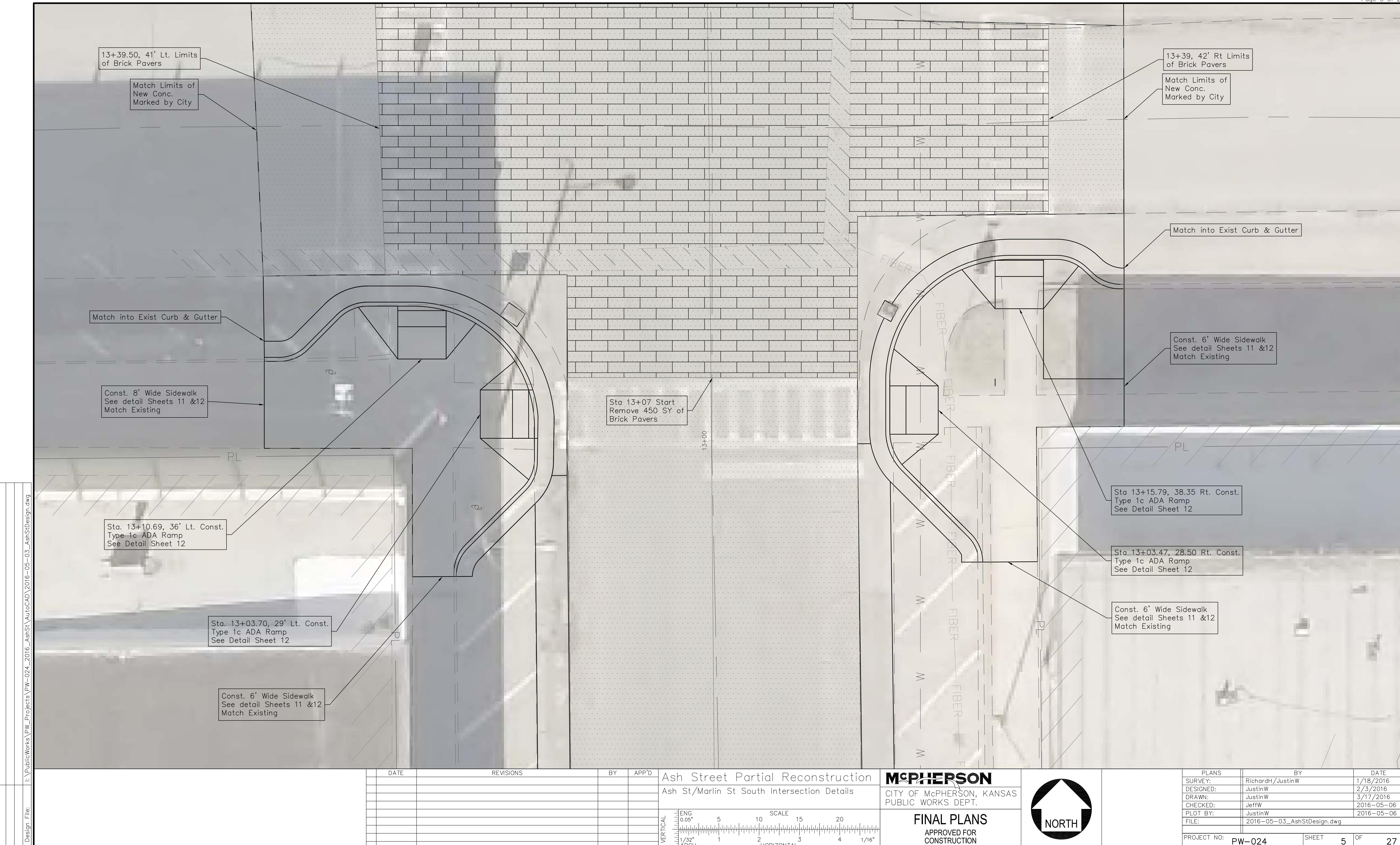
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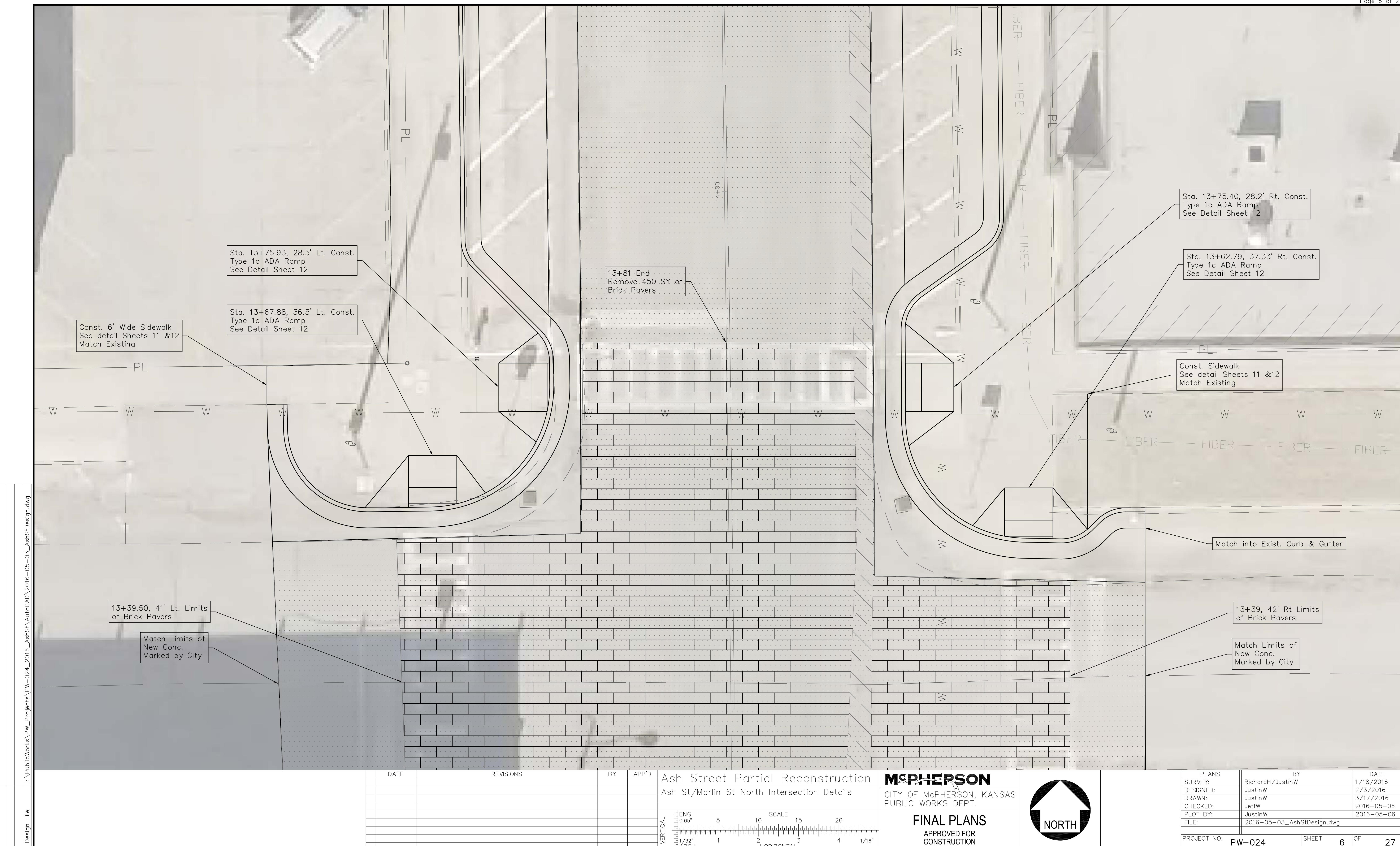
1/16"

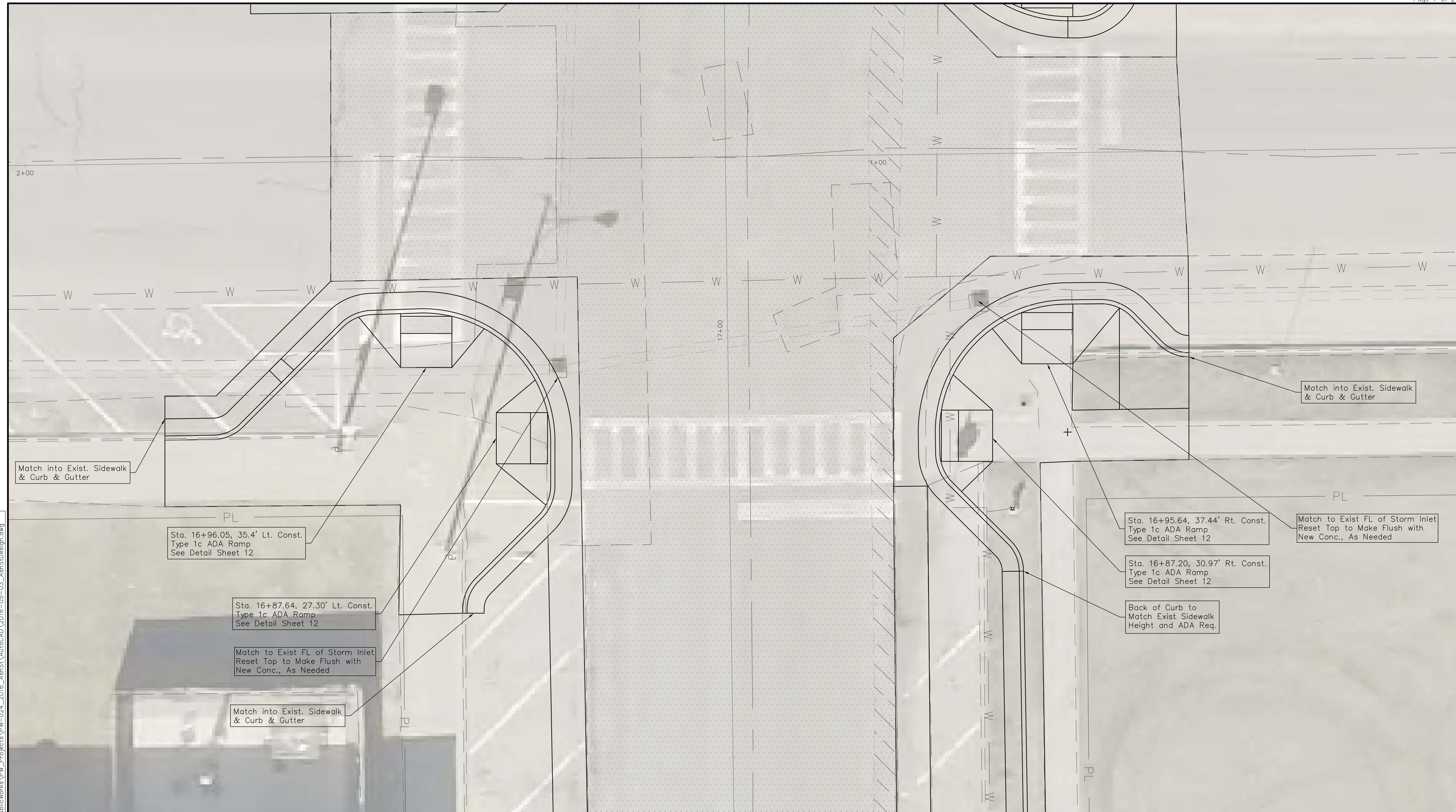
1 2 3 4

1/16"

1 2 3 4







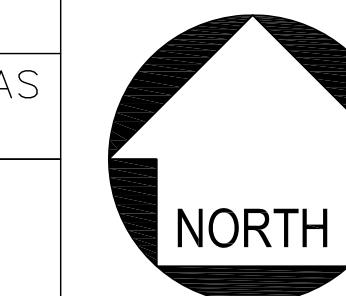
DATE	REVISIONS	BY	APP'D

Ash Street Partial Reconstruction  
Ash St/Euclid St South Intersection Details

**McPHERSON**

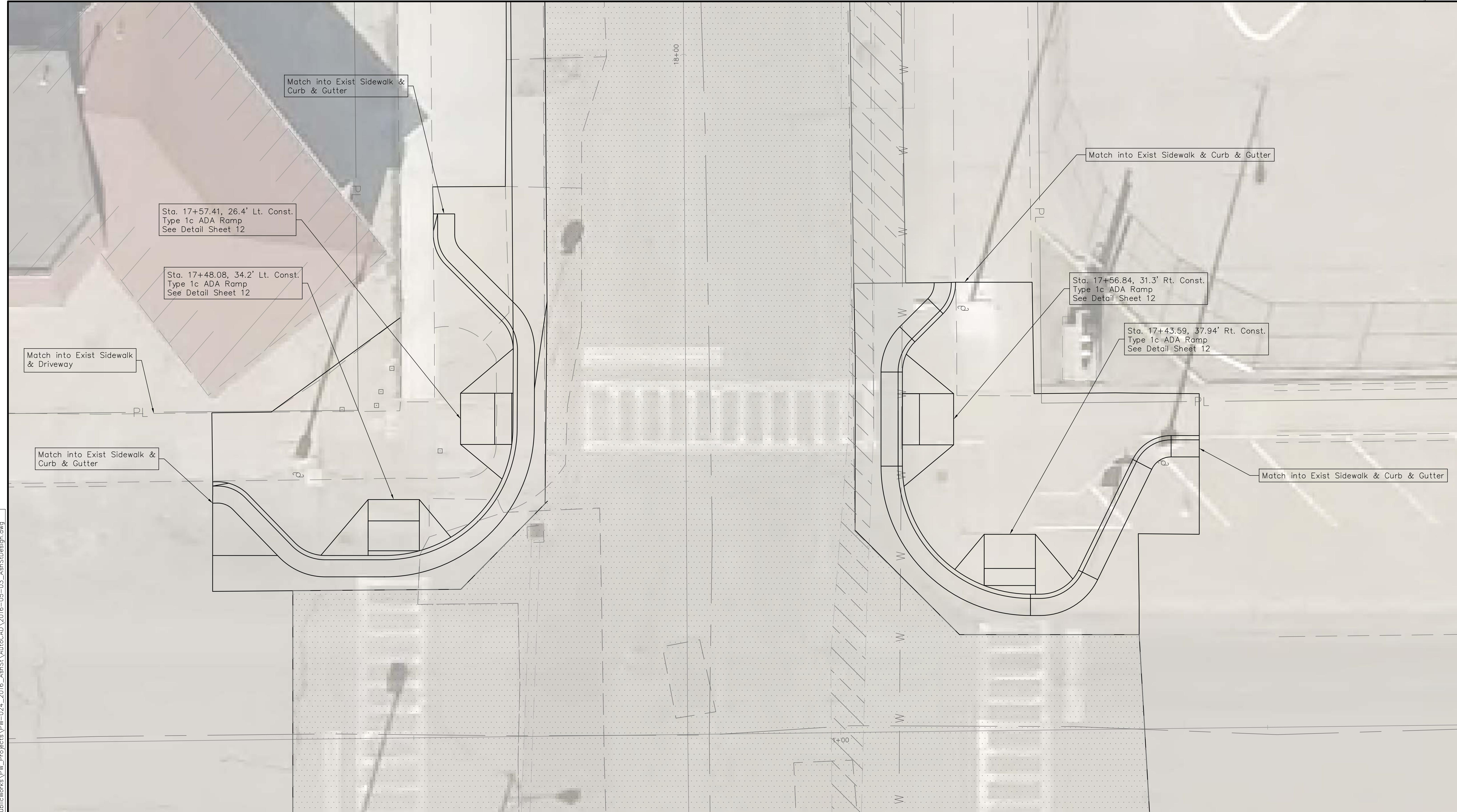
CITY OF MCPHERSON, KANSAS  
PUBLIC WORKS DEPT.

**FINAL PLANS**  
APPROVED FOR  
CONSTRUCTION



PLANS	BY	DATE
SURVEY:	RichardH/JustinW	1/18/2016
DESIGNED:	JustinW	2/3/2016
DRAWN:	JustinW	3/17/2016
CHECKED:	JeffW	2016-05-06
PLOT BY:	JustinW	2016-05-06
FILE:	2016-05-03_AshStDesign.dwg	

PROJECT NO: PW-024 SHEET 7 OF 27



DATE

REVISIONS

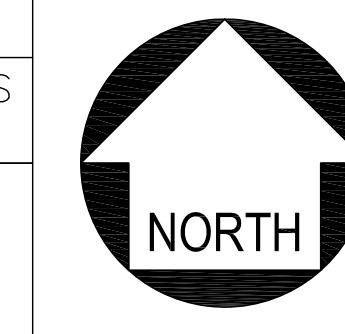
BY

APP'D

Ash Street Partial Reconstruction  
Ash St/Euclid St North Intersection Details

**McPHERSON**  
CITY OF MCPHERSON, KANSAS  
PUBLIC WORKS DEPT.

**FINAL PLANS**  
APPROVED FOR  
CONSTRUCTION



PLANS	BY	DATE
SURVEY:	RichardH/JustinW	1/18/2016
DESIGNED:	JustinW	2/3/2016
DRAWN:	JustinW	3/17/2016
CHECKED:	JeffW	2016-05-06
PLOT BY:	JustinW	2016-05-06
FILE:	2016-05-03_AshStDesign.dwg	
PROJECT NO:	PW-024	SHEET 8 OF 27

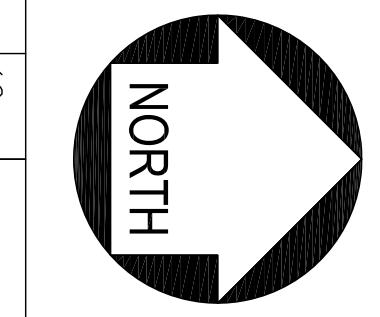
SCALE  
1/32" ARCH 5 10 15 20  
1 2 3 4 1/16" HORIZONTAL



DATE	REVISIONS	BY	APP'D

Ash Street Partial Reconstruction		McPHERSON		PLANS	BY	DATE						
Ash Street Public Parking Lot Resurfacing		CITY OF MCPHERSON, KANSAS										
VERTICAL	SCALE	ENG	20	40	60	80	1/32"	1	2	3	4	1/16"
ARCH	HORIZONTAL											

FINAL PLANS  
APPROVED FOR  
CONSTRUCTION



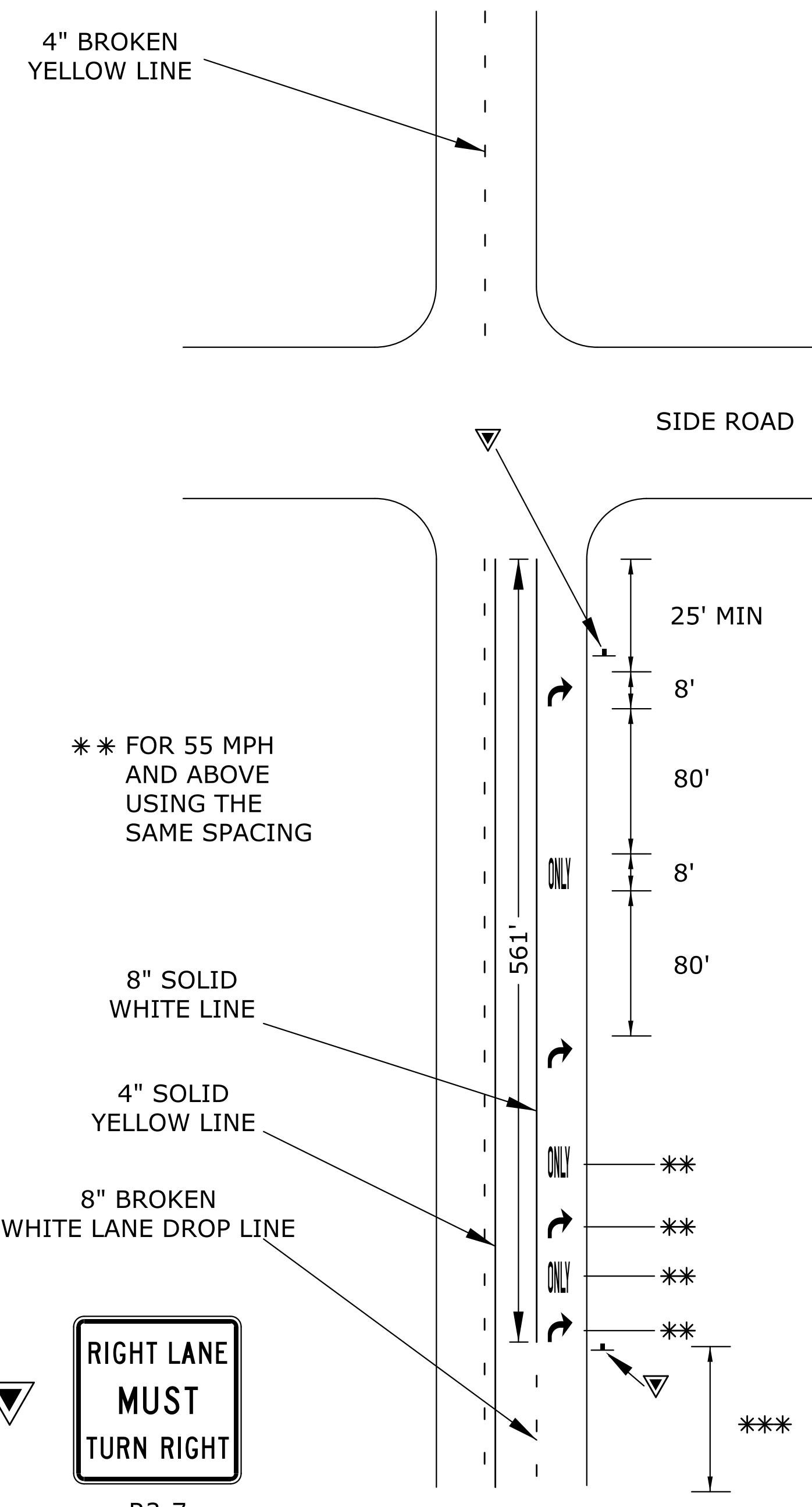
PROJECT NO:	PW-024	SHEET	9	OF	27
SURVEY:	RichardH/JustinW	BY			
DESIGNED:	JustinW		1/18/2016		
DRAWN:	JustinW		2/3/2016		
CHECKED:	JeffW		3/17/2016		
PLOT BY:	JustinW		2016-05-06		
FILE:	2016-05-03_AshStDesign.dwg				



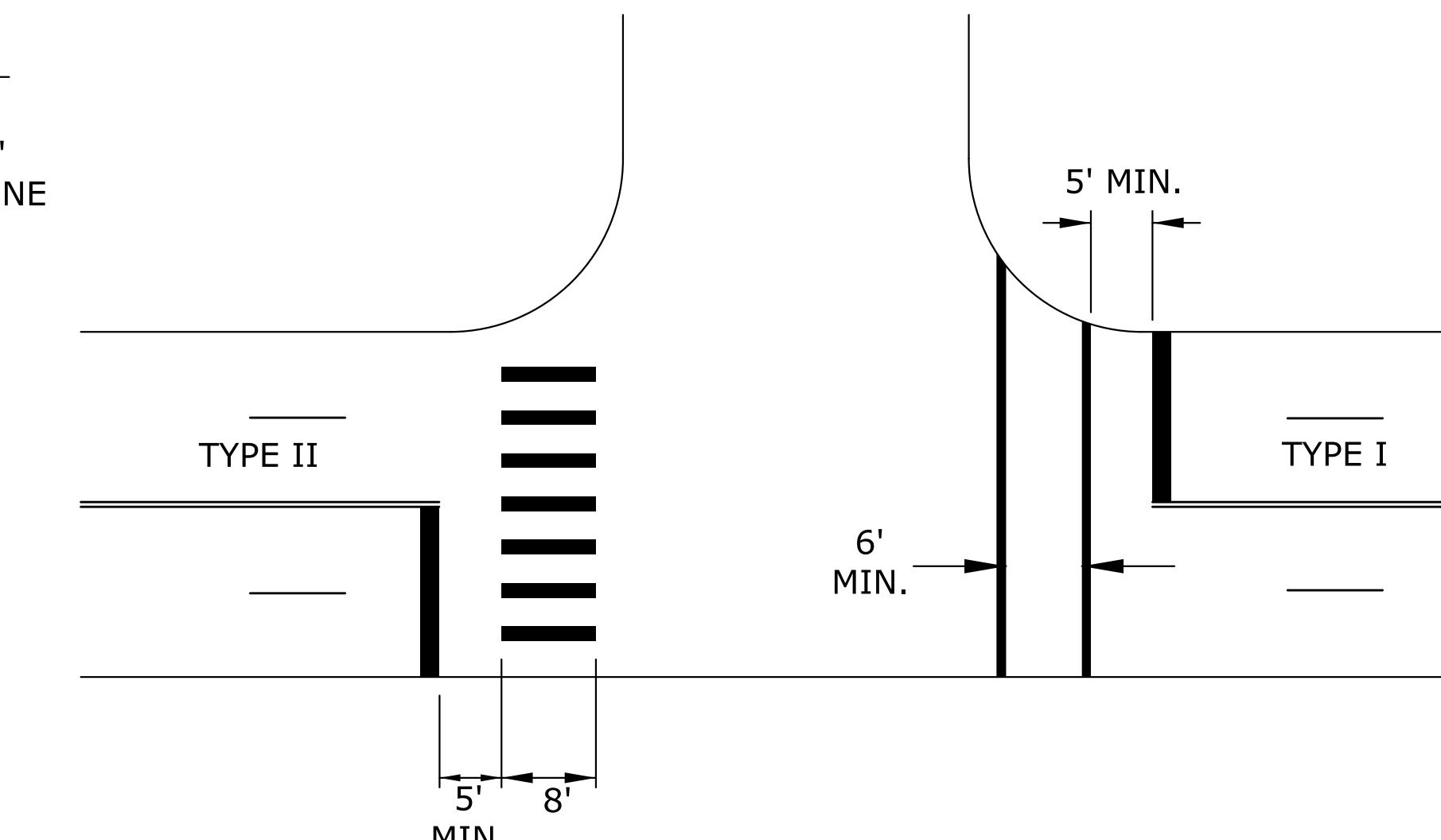




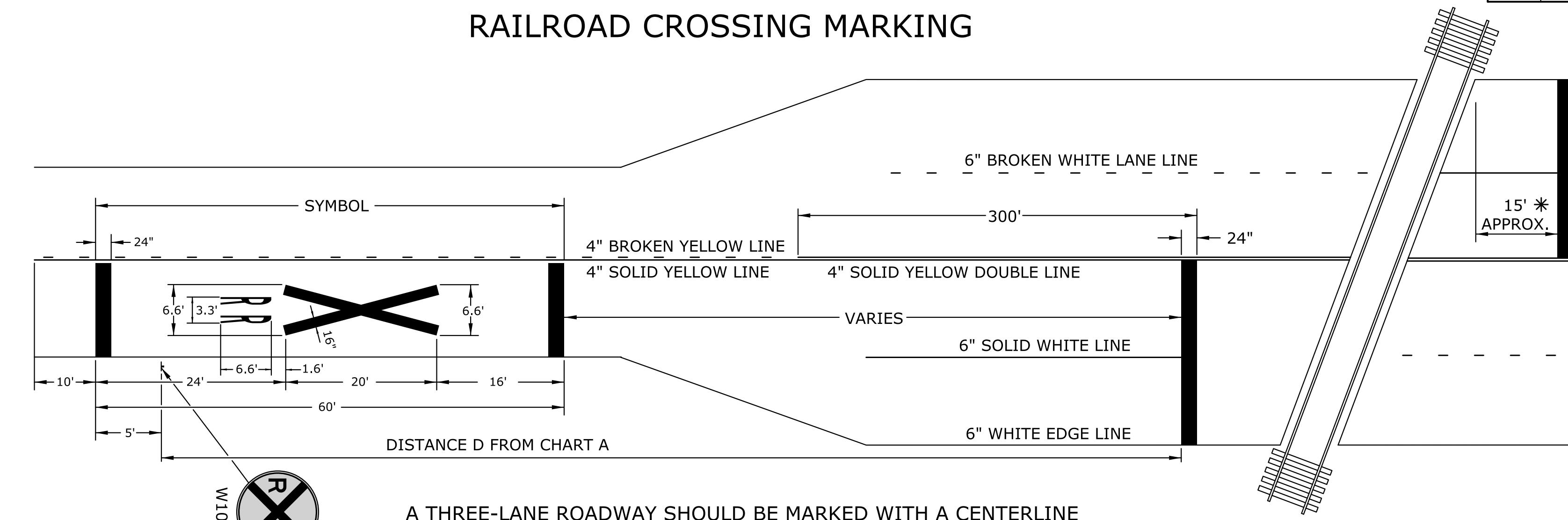
# TYPICAL SIGNING AND MARKING FOR RIGHT LANE MUST TURN RIGHT



\*\*\* THE LANE DROP MARKINGS LENGTH IS A MINIMUM OF 100' AND A MAXIMUM OF 250' PRIOR TO THE 8" SOLID WHITE LINE.



# RAILROAD CROSSING MARKING



A THREE-LANE ROADWAY SHOULD BE MARKED WITH A CENTERLINE FOR TWO-LANE APPROACH OPERATION ON THE APPROACH TO A CROSSING.

ON MULTI-LANE ROADS THE TRANSVERSE BANDS SHOULD EXTEND ACROSS ALL APPROACH LANES, AND INDIVIDUAL R X R SYMBOLS SHOULD BE USED IN EACH APPROACH LANE.

REFER TO STANDARD ALPHABET FOR HIGHWAY SIGNS AND MARKINGS FOR R X R SYMBOLS DETAILS.

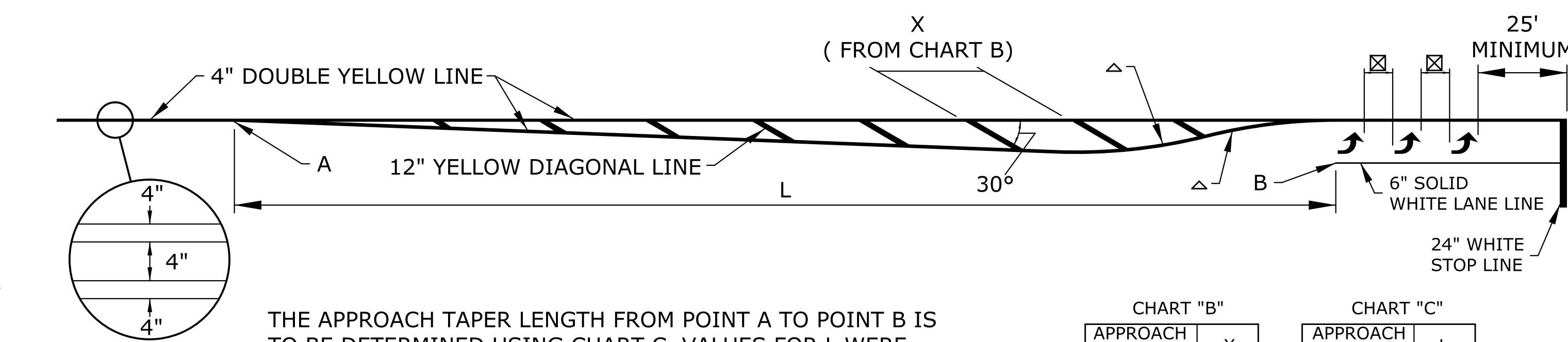
\*STOP LINE 8' FROM NEAR EDGE OF GATE OR CANTILEVER, IF PRESENT.

NOTE:  
ON NON I, US, AND K ROUTES, 4" EDGE LINES MAY BE INSTALLED.  
6" EDGE LINES ARE NOT REQUIRED ON NON I, US, AND K ROUTES.

(X) PLACEMENT LOCATION IS DEPENDENT ON SITE CONDITIONS AND OTHER SIGNING TO PROVIDE ADEQUATE ADVANCE WARNING TO THE DRIVER

ALL DISTANCES  
ARE MINIMUM.

# TYPICAL APPROACH TAPER DETAIL



THE APPROACH TAPER LENGTH FROM POINT A TO POINT B IS TO BE DETERMINED USING CHART C. VALUES FOR L WERE CALCULATED USING THE EQUATIONS BELOW AND INCREASED TO THE NEXT HIGHER 5 MPH INCREMENT.

- SPEEDS < 45 MPH  $L = \frac{W^*}{60}$

IF ARROWS ARE USED AND UNLESS OTHERWISE SPECIFIED THE SPACE BETWEEN LINES SHOULD BE AT LEAST FOUR TIMES THE HEIGHT OF THE CHARACTERS FOR LOW SPEED ROADS BUT NOT MORE THAN TEN TIMES THE HEIGHT OF THE CHARACTERS. UNDER ANY CONDITIONS

△ FOR SPEEDS LESS THAN OR EQUAL TO 40 MPH, R=150'.  
FOR SPEEDS GREATER THAN OR EQUAL TO 45 MPH, R=300'

CHART "B"		CHART "C"	
APPROACH SPEED	X	APPROACH SPEED	L
20 MPH	20'	20 MPH	80
25 MPH	25'	25 MPH	125
30 MPH	30'	30 MPH	180
35 MPH	35'	35 MPH	245
40 MPH	40'	40 MPH	320
45 MPH	45'	45 MPH	540
50 MPH	50'	50 MPH	600
55 MPH	55'	55 MPH	660
60 MPH	60'	60 MPH	720
65 MPH	65'	65 MPH	780
70 MPH	70'	70 MPH	840

3	5/25/12	Updated Chart B and Lane Drop Lines	B.A.H.	B.D.G.
2	10/20/06	RR Xing Symbol Changed from 18" to 16"	T.L.H.	B.D.G.
1	9/20/05	Added 4" Solid Yellow Double Line to RRXing	J.F.F.	B.D.G.

STATE	PROJECT NO.	YEAR	SHEET NO.	TOTAL SHEETS
KANSAS	PW-024	2016	14	27

# SUMMARY OF PAVEMENT MARKINGS

LOCATION	4" Solid WHITE Edge Line	6" Solid WHITE Edge Line	6" Broken WHITE Lane Line	6" Broken WHITE Lane Line (PCP)	6" Dotted WHITE Extension Line	6" Broken WHITE Lane Drop Line	6" Solid WHITE Lane Line	8" Broken WHITE Lane Drop Line	8" Solid WHITE Gore Line	8" Dotted WHITE Extension Line	12" Solid WHITE Diagonal Line	12" Solid WHITE Chevron Line	12" Solid WHITE Type I Crosswalk Line	24" Solid WHITE Type II Crosswalk Line	24" Solid WHITE Stop Line	4" Solid YELLOW Edge Line	4" Solid YELLOW Double Line	4" Solid YELLOW Line	4" Broken YELLOW Line	6" Solid YELLOW Edge Line	12" Solid YELLOW Diagonal Line
10+03															18'						
10+00 - 12+83		420'																			
11+56														80'							
12+03 - 12+83		140'																			
12+96															14'						
13+03														80'							
13+39 Rt															12'						
13+39 Lt														76'		16'					
13+39 Rt														48'							
13+77														74'							
13+84															14'						
13+89 - 16+64 Lt		400'																			
13+89 - 16+64 Rt		420'																			
16+79															18'						
16+85														80'							
17+20, 47' Lt																12'					
17+33 Rt														56'							
17+33 Lt														48'							
17+35																12'					
17+57														80'							
17+64															14'						
18+20 - 20+55		400'																			
19+78 - 20+65		180'																			
20+59															14'						
Public Parking Lot		660'																			
<b>TOTALS</b>		2620'												622'		144'					

# RECAPITULATION OF QUANTITIES

ITEMS	TOTAL	UNITS
PAVEMENT MARKING (MULTI-COMPONENT)(WHITE)(4")		FT
PAVEMENT MARKING (MULTI-COMPONENT)(WHITE)(6")		FT
PAVEMENT MARKING (MULTI-COMPONENT)(WHITE)(8")		FT
PAVEMENT MARKING (MULTI-COMPONENT)(WHITE)(12")		FT
PAVEMENT MARKING (MULTI-COMPONENT)(YELLOW)(4")		FT
PAVEMENT MARKING (MULTI-COMPONENT)(YELLOW)(6")		FT
PAVEMENT MARKING (MULTI-COMPONENT)(YELLOW)(12")		FT
PAVEMENT MARKING (THERMOPLASTIC)(WHITE)(4")		FT
PAVEMENT MARKING (THERMOPLASTIC)(WHITE)(6")		FT
PAVEMENT MARKING (THERMOPLASTIC)(WHITE)(8")		FT
PAVEMENT MARKING (THERMOPLASTIC)(WHITE)(12")		FT
PAVEMENT MARKING (THERMOPLASTIC)(YELLOW)(4")		FT
PAVEMENT MARKING (THERMOPLASTIC)(YELLOW)(6")		FT
PAVEMENT MARKING (THERMOPLASTIC)(YELLOW)(12")		FT
PAVEMENT MARKING (EPOXY)(WHITE)(4")		FT
PAVEMENT MARKING (EPOXY)(WHITE)(6")	2620	FT
PAVEMENT MARKING (EPOXY)(WHITE)(8")		FT
PAVEMENT MARKING (EPOXY)(WHITE)(12")		FT
PAVEMENT MARKING (EPOXY)(YELLOW)(4")		FT
PAVEMENT MARKING (EPOXY)(YELLOW)(6")		FT
PAVEMENT MARKING (EPOXY)(YELLOW)(12")		FT
PAVEMENT MARKING (INTERSECTION GRADE)(WHITE)(12")	622	FT
PAVEMENT MARKING (INTERSECTION GRADE)(WHITE)(24")	144	FT
PAVEMENT MARKING (INTERSECTION GRADE)(YELLOW)(12")		FT
PAVEMENT MARKING SYMBOL (INTERSECTION GRADE)(WHITE)(Railroad X)	1	EACH
PAVEMENT MARKING SYMBOL (INTERSECTION GRADE)(WHITE)( )		EACH
PAVEMENT MARKING SYMBOL (INTERSECTION GRADE)(WHITE)( )		EACH
PAVEMENT MARKING SYMBOL (INTERSECTION GRADE)(WHITE)( )		EACH
PAVEMENT MARKING SYMBOL (INTERSECTION GRADE)(WHITE)( )		EACH
PAVEMENT MARKING SYMBOL (INTERSECTION GRADE)(US-SHIELD)( )		EACH
PAVEMENT MARKING SYMBOL (INTERSECTION GRADE)(K-SHIELD)( )		EACH
PAVEMENT MARKING SYMBOL (INTERSECTION GRADE)(I-SHIELD)( )		EACH
PAVEMENT MARKING (PATTERNED COLD PLASTIC)(WHITE)(6")		FT
PAVEMENT MARKING (PATTERNED COLD PLASTIC)(WHITE)(8")		FT
PAVEMENT MARKING (PATTERNED COLD PLASTIC)(WHITE)(12")		FT
PAVEMENT MARKING REMOVAL		FT

## SUMMARY OF WORD & SYMBOL MARKINGS

NOTE:  
WORDS & SYMBOLS SHALL CONFORM TO THE LATEST EDITION OF  
STANDARD ALPHABETS FOR HIGHWAY SIGNS AND PAVEMENT  
MARKINGS" PRINTED BY THE U.S. DEPARTMENT OF TRANSPORTATION,  
FEDERAL HIGHWAY ADMINISTRATION.

PRIOR TO COMMENCEMENT OF PAVEMENT MARKING WORK THE ENGINEER WILL ESTABLISH THE LIMITS FOR "NO PASSING" ZONES. THESE LIMITS SHALL BE USED FOR THE LOCATION OF "NO PASSING" LINES AND FOR THE COMPUTATION OF ACTUAL MARKING QUANTITIES FOR THIS LINE TYPE.

2	5/25/12	Added Line Types, Symbols, and Shields	B.A.H.	B.D.G.
1	7/26/05	New FHWA Approval Date	J.F.F.	B.D.G.
10.	DATE	REVISIONS	BY	APP'D

KANSAS DEPARTMENT OF TRANSPORTATION  
SUMMARY AND RECAPITULATION  
OF PAVEMENT MARKING  
QUANTITIES

NOTE: FOR SPECIFIC PAVEMENT MARKING DETAILS AND DIMENSIONS SEE PLAN SHEETS

NOTE: ALL TOTALS REFLECT ACTUAL QUANTITY OF PAVEMENT MARKING MATERIALS REQUIRED.

1) Design Speed: Those items delegated to temporary traffic control should be designed and installed using the posted/legal speed of the roadway prior to work starting.

2) Minimum lane width: Lane widths shall be a minimum of 11' (measured between centerlines of pavement markings) or as shown on the plans, or as directed by the engineer. A lane width less than 11' may require restricted roadway width signing.

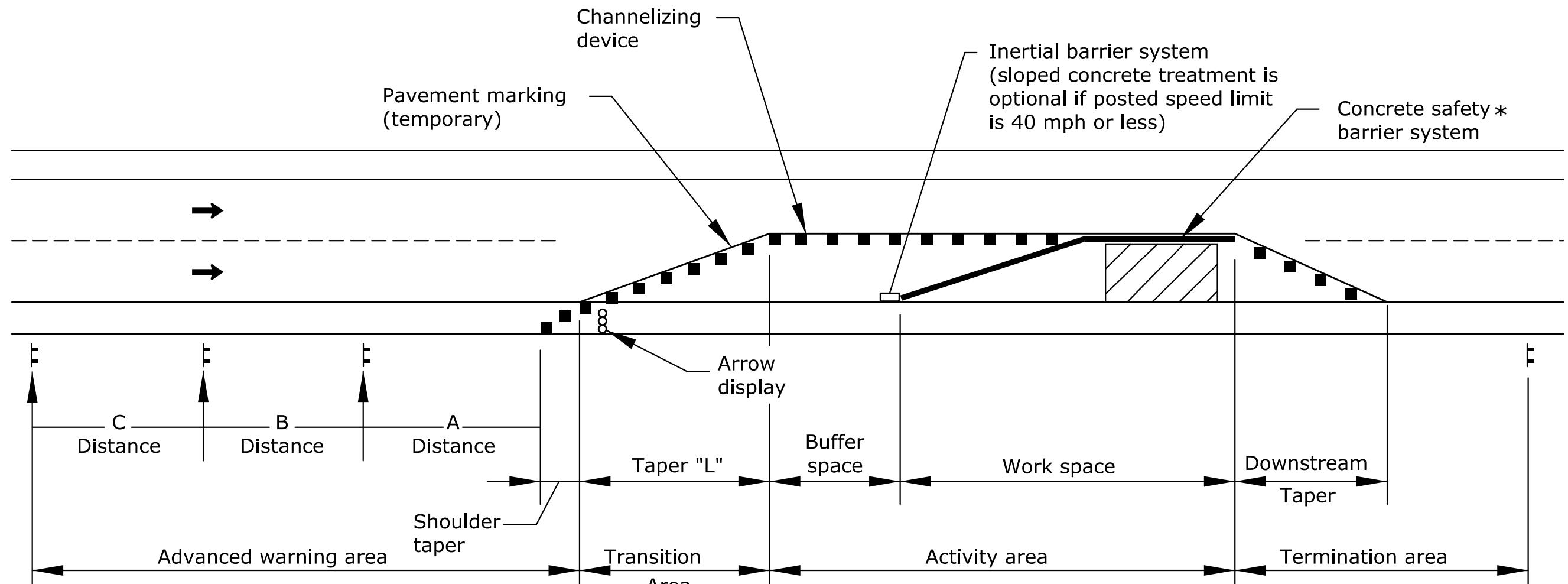
3) Consideration should be made to separate pedestrian and, if needed, bicycle movements from both work site activity and vehicular traffic. Unless a reasonable safe route that does not involve crossing the roadway can be provided, pedestrians should be appropriately directed with advance signing that encourages them to cross to the opposite side of the roadway. In urban and suburban areas with high vehicular traffic volumes, these signs should be placed at intersections (rather than midblock locations) so that pedestrians are not confronted with midblock work sites that will induce them to attempt skirting the work site or making a midblock crossing.

4) When existing pedestrian facilities are disrupted, closed, or relocated, the temporary facilities shall be detectable and include accessibility features consistent with the features present in the existing pedestrian facility.

5) When the driving surface open to traffic is milled, is a temporary surface made of loose material, or when directed by the engineer use the W8-15 (Grooved Pavement) or W8-7(Loose Gravel) a "C" distance after the W20-1 (Road Work Ahead) on mainline approaches. Signs may be used with the W8-15p motorcycle plaque as directed by the engineer. Display signs in advance of the condition as long as the condition is present.

6) Alternative temporary rumble strip options may be available. Please contact the Temporary Traffic Control Unit for more information at 785-296-0355 or 785-296-1183.

STATE	PROJECT NO.	YEAR	sheet no.	total sheets
KANSAS	PW-024	2016	15	27



## TYPICAL WORK ZONE COMPONENTS

\* When concrete barrier system is used, portable channelizing devices are not needed along the tangent barrier section.

### Minimum advance warning sign spacing (in feet):

SPEED (MPH) *	A	B	C
URBAN (40 MPH OR LOWER)	100	100	100
URBAN (45 MPH OR HIGHER)	350	350	350
RURAL (55 MPH OR LOWER)	500	500	500
RURAL (60 MPH OR HIGHER)	750	750	750
EXPRESSWAY/FREEWAY	1000	1500	2640

\* Posted speed prior to work starting

The minimum spacing between signs shall be no less than 100', unless directed by the engineer.

The spacing between any signs may be increased beyond the minimum values in the table above as approved by the engineer in order to maximize visibility.

### Taper Formulas:

$L = WS$  for speeds of 45 MPH or more

$L = WS^2/60$  for speeds of 40 MPH or less

Where:  $L$  = Minimum length of taper in feet  
 $S$  = Numerical value of posted speed prior to work starting in MPH  
 $W$  = Width in offset feet

Shifting taper =  $1/2 L$   
 Shoulder taper =  $1/3 L$

### Channelizer placement:

(1) The spacing between devices in transition area (taper) should not exceed a distance in feet equal to  $1/2$  the posted speed limit in mph prior to work starting.

(2) The spacing between devices in the advanced warning area and the activity area should not exceed a distance in feet equal to two times the posted speed limit in mph prior to work starting.

(3) Channelizing devices shall be placed for optimum visibility, normally at right angles to the traffic flow.

(4) Place directional indicator barricades in series to direct traffic onto the new path. The arrow sign should not be visible to opposing traffic.

(5) Alternating diagonal orange and white striping must slope downward in the direction traffic is expected to pass.

### Buffer Space

SPEED (MPH) *	20	25	30	35	40	45	50	55	60	65	70	75
LENGTH (ft)	115	155	200	250	305	360	425	495	570	645	730	820

\* Posted speed prior to work starting

Neither work activity nor storage of equipment, vehicles, or material should occur in the buffer space. When a protection vehicle is placed in advance of the work space, only the space upstream of the vehicle constitutes the buffer space.

If temporary concrete safety barrier system is used to separate approaching traffic from the work space, the barrier system shall be considered part of the activity area. A full lane width should be available throughout the length of the buffer space. See typical work zone components above.

3			
2			
1	08/18/15	Channelizer spacing info	R.W.B. K.E.
NO.	DATE	REVISIONS	BY APP'D

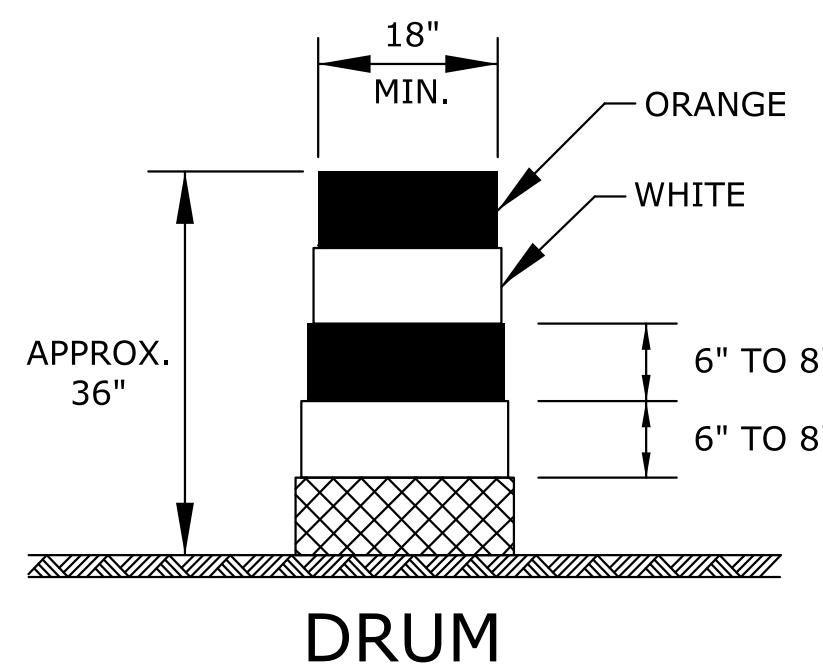
KANSAS DEPARTMENT OF TRANSPORTATION

## TRAFFIC CONTROL GENERAL NOTES

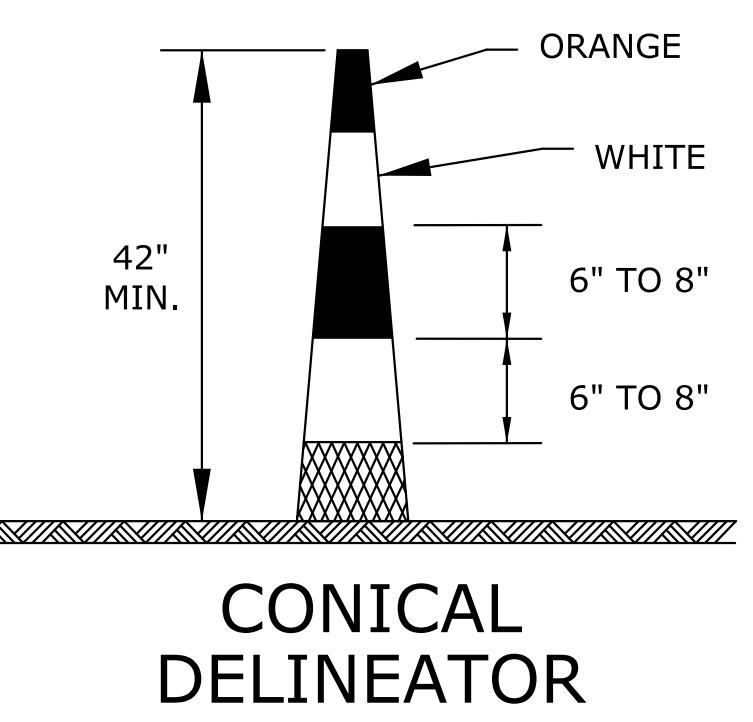
TE700

HWY APPROVAL	08/18/15	APP'D	Kristina Erickson
DESIGNED	B.A.H. DETAILED	R.W.B. QUANTITIES	TRACED
DESIGN CK.	DETAIL CK.	QUAN. CK.	TRACE CK.

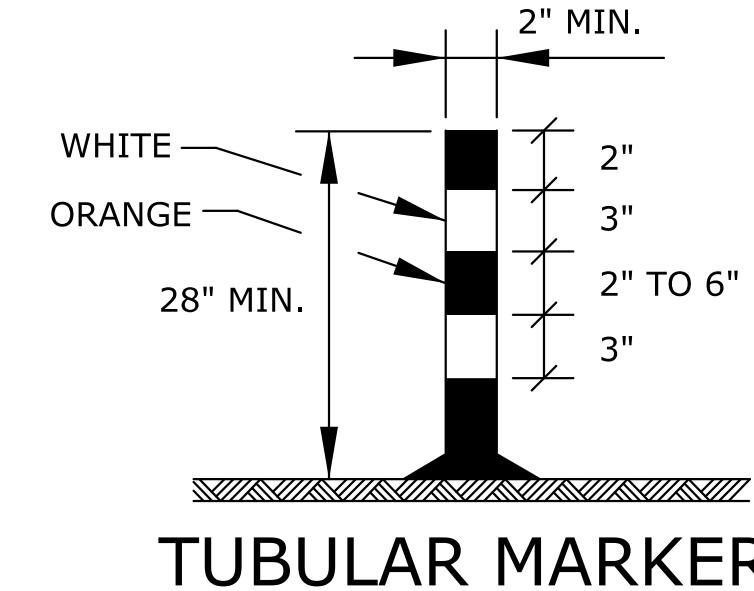
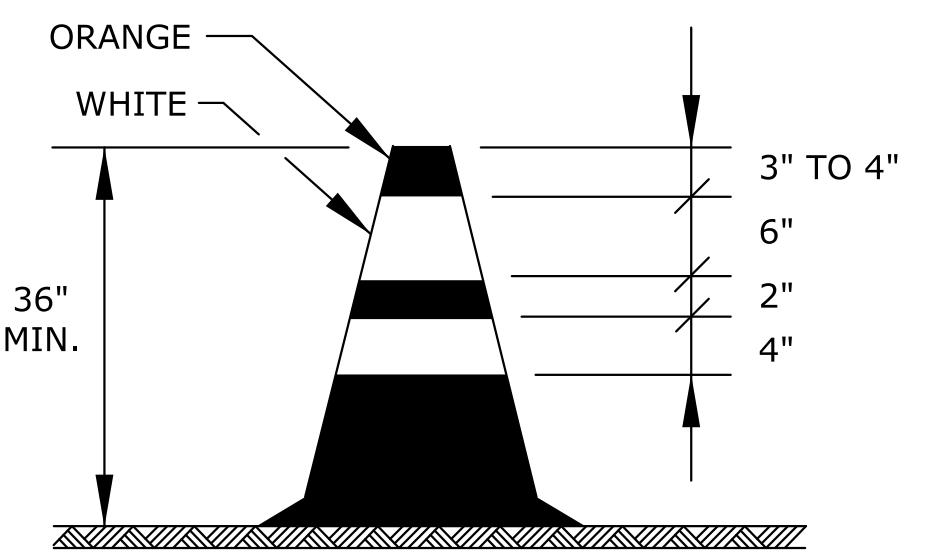
KDOT Graphics Certified 08-18-2015



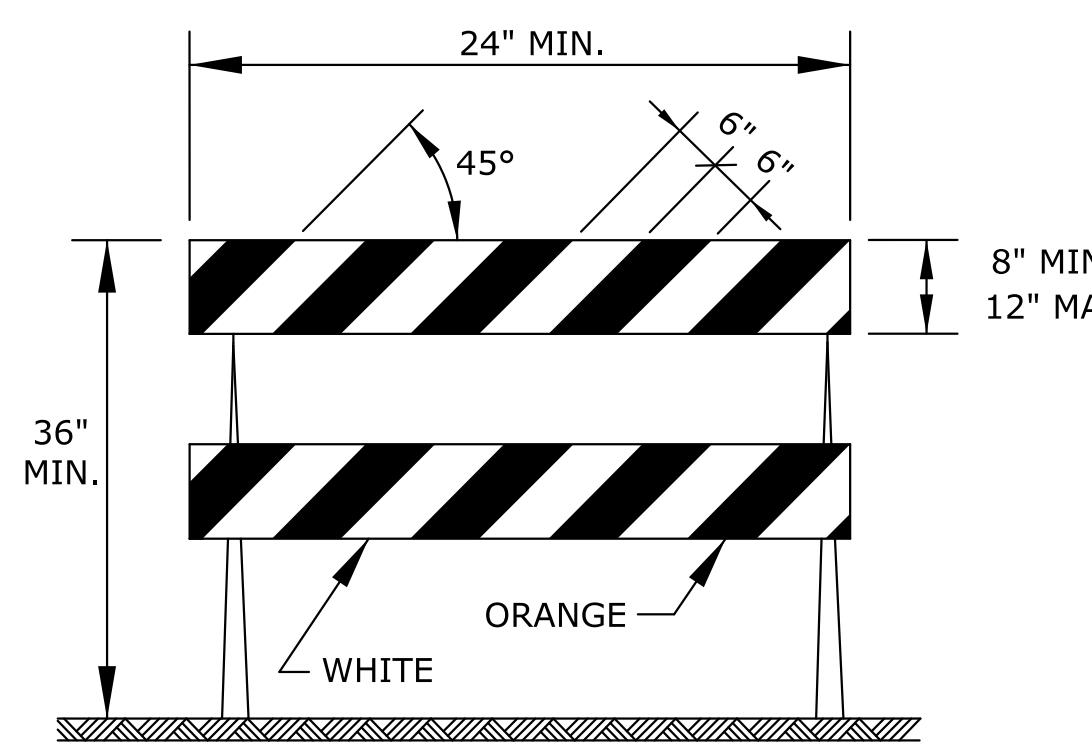
DRUM



CONICAL DELINEATOR

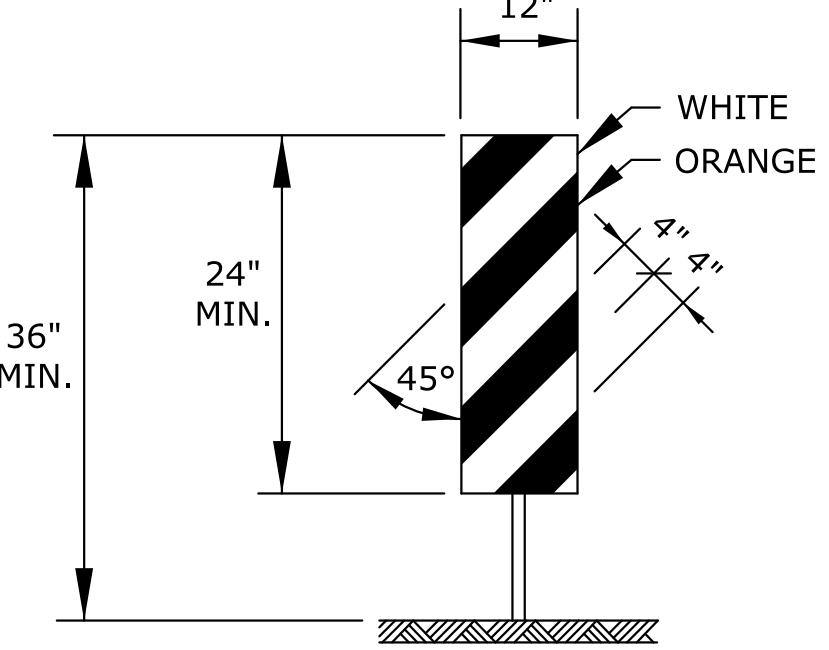
TUBULAR MARKER  
Striping as shown for up to 42".

TRAFFIC CONE



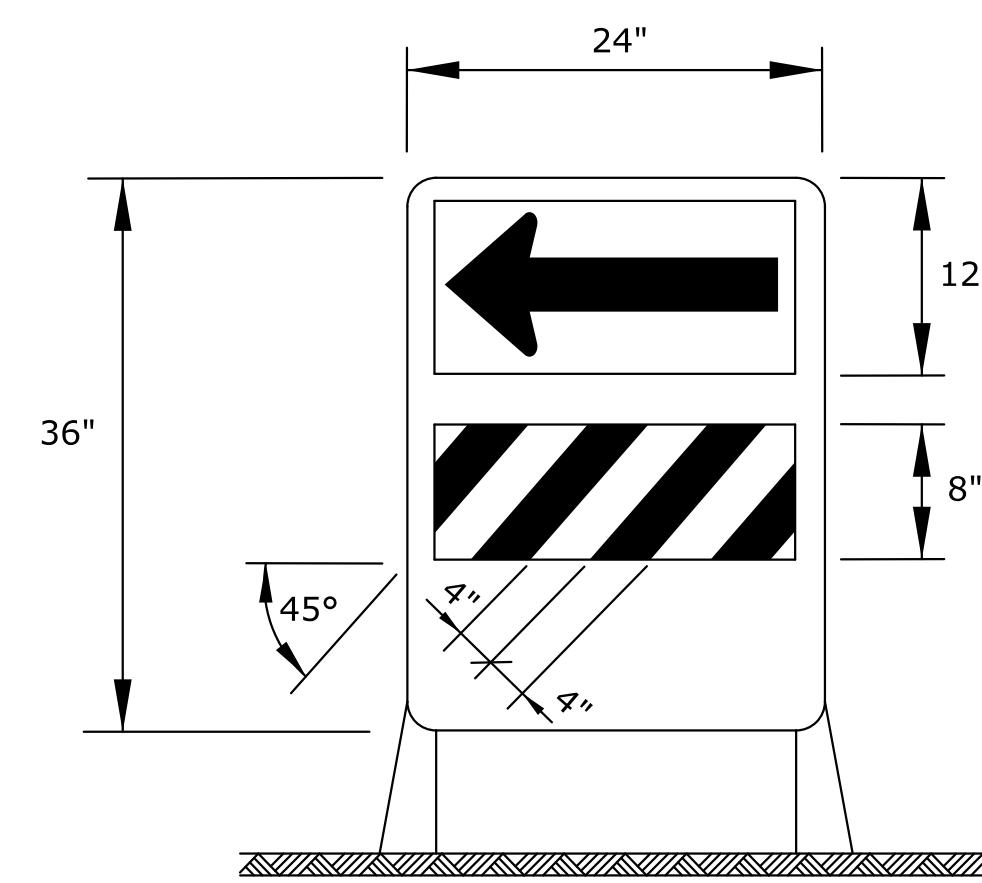
TYPE 2 BARRICADE

For rails less than 36" long, 4" wide stripes may be used.  
All stripes shall slope downward to the traffic side for channelization.



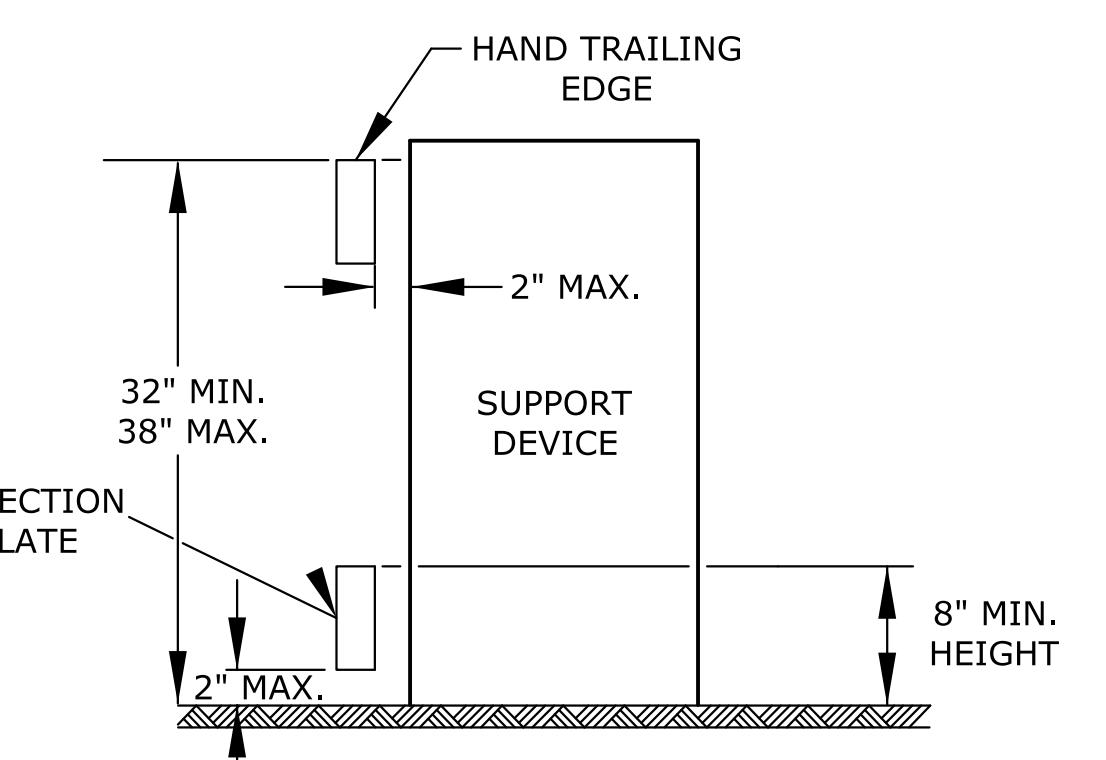
VERTICAL PANEL

The stripes shall slope downward to the traffic side for channelization.



DIRECTION INDICATOR BARRICADE

The stripes shall slope downward in the direction traffic is to pass.  
The direction indicator barricade shall be used in series to direct the motorist into the intended lane of travel.



PEDESTRIAN CHANNELIZER

1. Support device shall not project beyond the detection plate into the pathway.
2. Hand trailing edges and detection plates are optional for continuous walls.
3. Interconnect pedestrian channelizers to prevent displacement and to provide continuous guidance through or around work.
4. Alternate pathways shall be firm, stable, and slip resistant.
5. Treat height differentials  $> 1/2"$  in the surfaces of alternate paths with a firm, stable, and slip resistant temporary ramp having a slope of 12:1 or flatter and having a width equal to the alternate path.
6. Use alternating orange/white on interconnected devices.

ITEM	LOCATION	Cross-overs	Shoulder	Divergents	Tangents	Tapers	Ramps	Head to Head	Object Identifier	Lead-in Devices	Gores
PORTABLE	Drums	Yes	Yes	Yes	Yes	Yes	(1)	Yes	Yes	Yes	
	Conical Delineators	Yes	Yes	Yes	Yes	Yes	(1)	Yes	Yes	Yes	
	Vertical Panels	(2)	(2)	(2)	(2)	(2)	(1,2)	YES	(2)	(2)	
	Direction Indicator Barricade	NO	NO	NO	Yes	NO	NO	NO	NO	NO	
	Type 2 Barricade	(2)	(2)	(2)	(2)	NO	NO	Yes	NO	NO	
	Traffic Cones	NO	NO	(4)	(4)	(4)	NO	(4)	(4)	(4)	
FIXED	Tubular Markers	(3)	(3)	(3)	NO	(3)	Yes	NO	Yes	Yes	
	Vertical Panels	(3)	(3)	(3)	(3)	(3)	(3)	Yes	(2,3)	(2)	

- (1) Not allowed on centerline delineation along freeways or expressways.
- (2) The stripes shall slope downward to the traffic side for channelization.
- (3) May be used upon the approval of the engineer.
- (4) Daytime operations only.

3		
2		
1		

NO. DATE REVISIONS BY APP'D

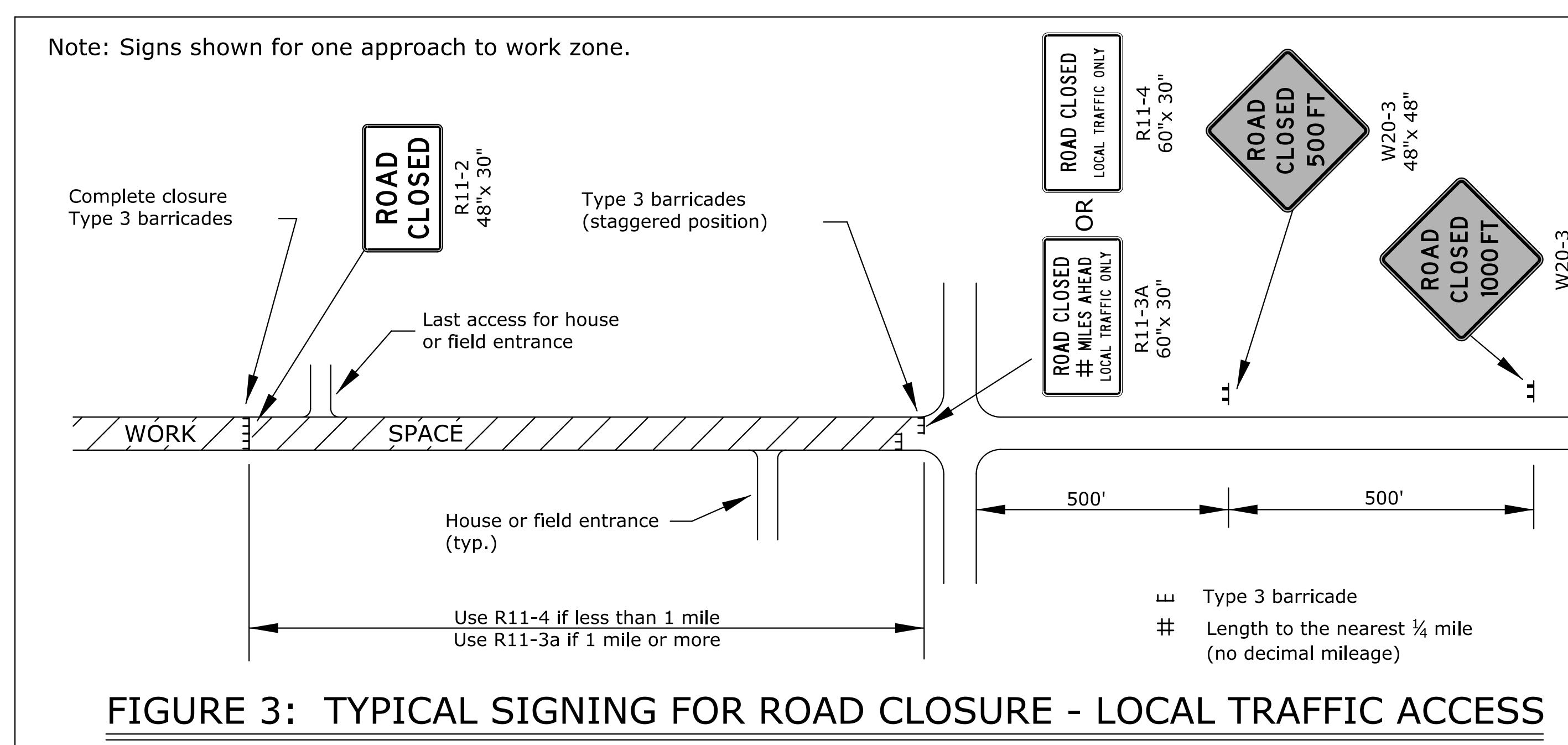
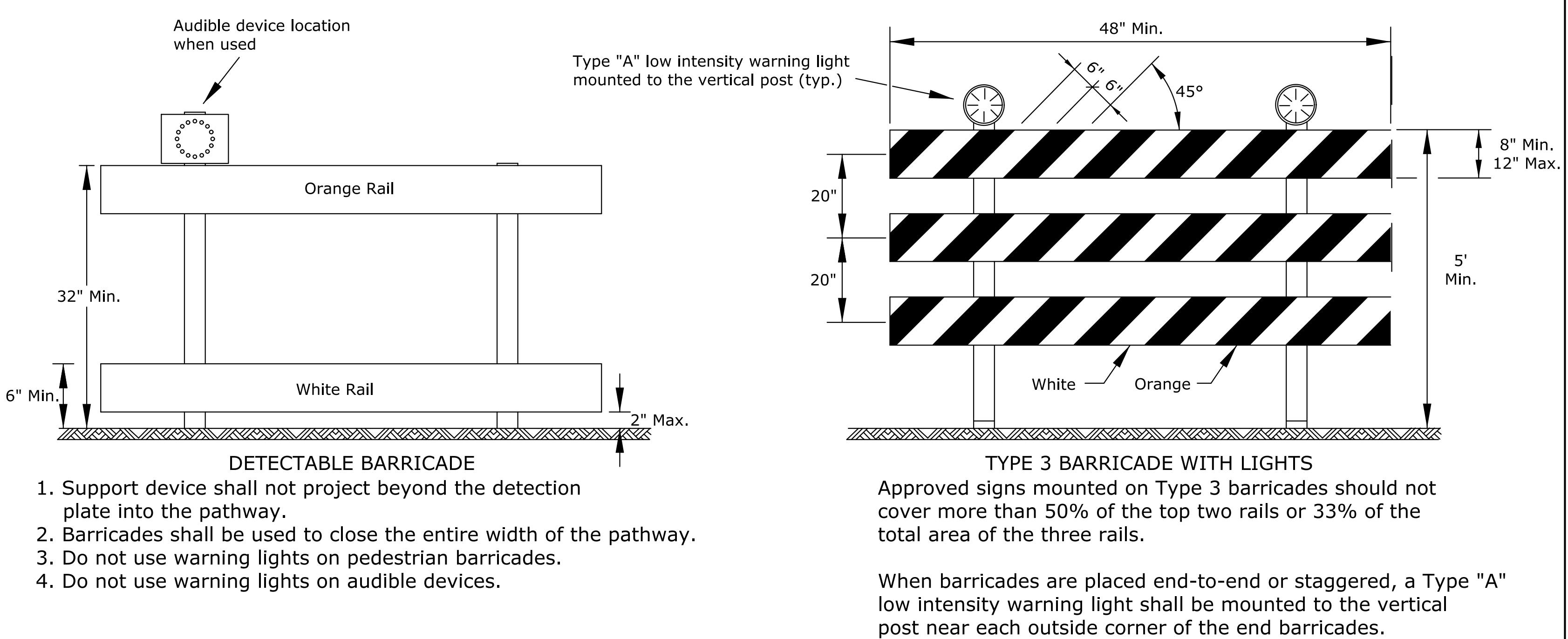
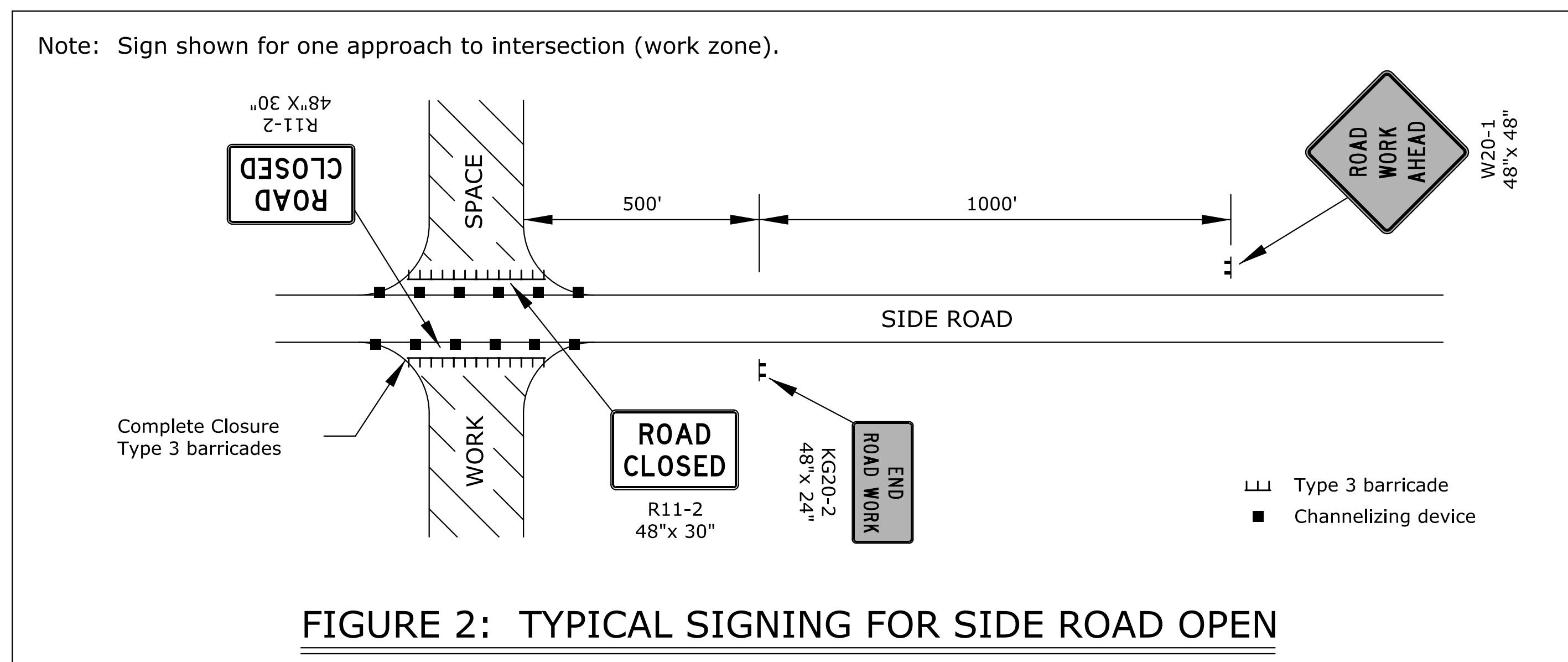
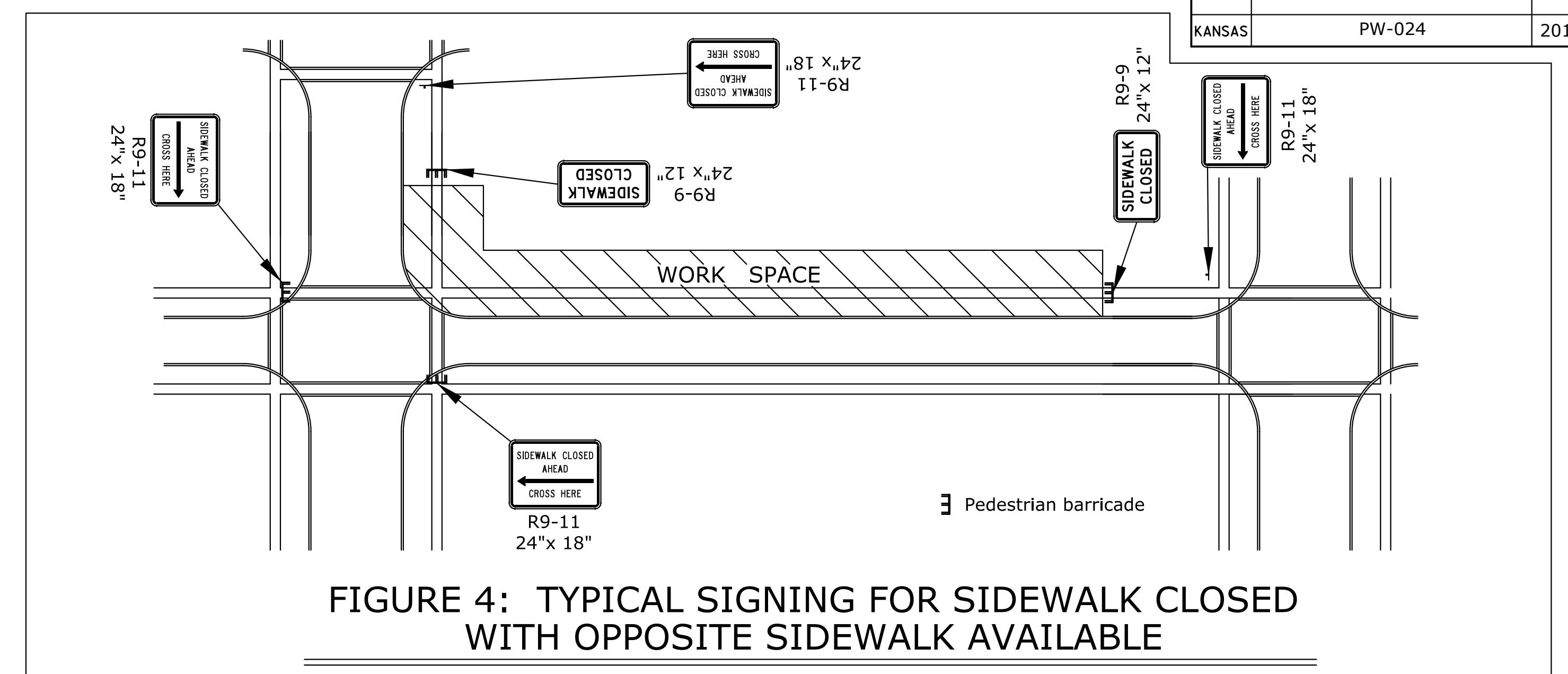
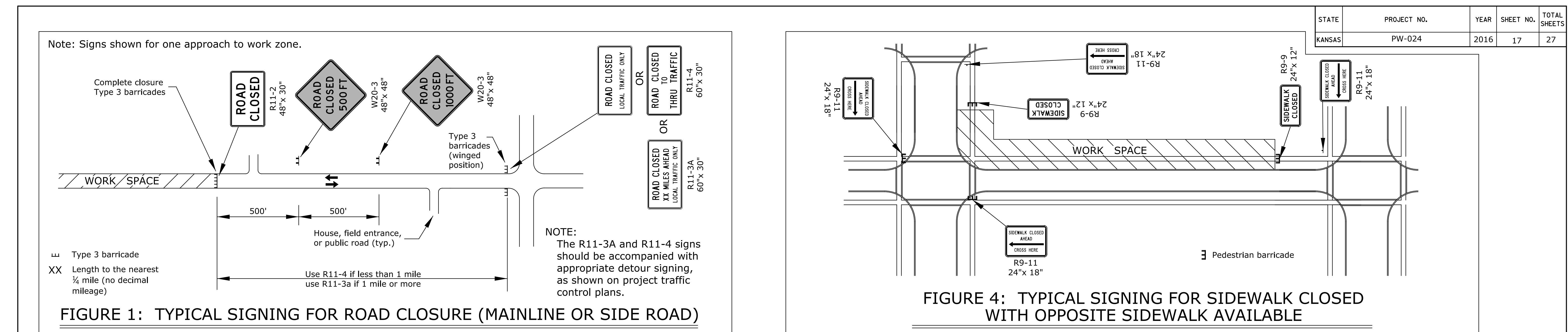
KANSAS DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL  
CHANNELIZING DEVICES

TE702

HWY APPROVAL	06/01/15	APP'D	Kristina Erickson
DESIGNED	L.E.R.	DETAILED	R.W.B.
DESIGN CK.	DETAIL CK.	QUAN. CK.	TRACE CK.

KDOT Graphics Certified 06-01-2015 Sh. No. 16



#### ROAD CLOSED GENERAL NOTES

As shown in Figure 1, at the point where thru traffic must detour and local traffic can proceed to the location where the roadway is completely closed, the R11-3a (ROAD CLOSED # MILES AHEAD LOCAL TRAFFIC ONLY) or R11-4 (ROAD CLOSED LOCAL TRAFFIC ONLY or ROAD CLOSED TO THRU TRAFFIC) sign shall be used with Type 3 barricades (winged position), placed on the shoulders of roadway.

As shown in Figure 3, when local traffic must be allowed access into the work zone, Type 3 barricades shall be longitudinally staggered to maintain the appearance of a closed roadway. A second line of end-to-end Type 3 barricades shall be placed just beyond the last access point in the work zone, to completely close the roadway.

The R11-4 (ROAD CLOSED TO THRU TRAFFIC or ROAD CLOSED LOCAL TRAFFIC ONLY) sign shall be used when the distance to the point of complete closure of the roadway is less than 1 mile.

The R11-3a (ROAD CLOSED # MILES AHEAD LOCAL TRAFFIC ONLY) sign shall be used when the distance to the point of complete closure of the roadway is 1 mile or greater.

The words "BRIDGE OUT" (or BRIDGE CLOSED) may be substituted for the words "ROAD CLOSED" on the R11-3a or R11-4 sign where applicable.

Plotted : 01-JUN-2015 13:54  
Traffic

Drawn By : mushock  
File : te704.dgn

3  
2  
1  
NO. DATE REVISIONS BY APP'D

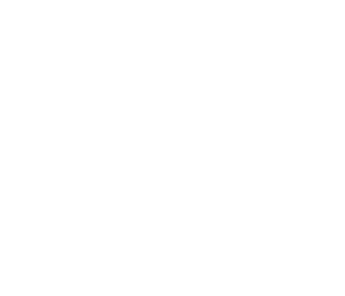
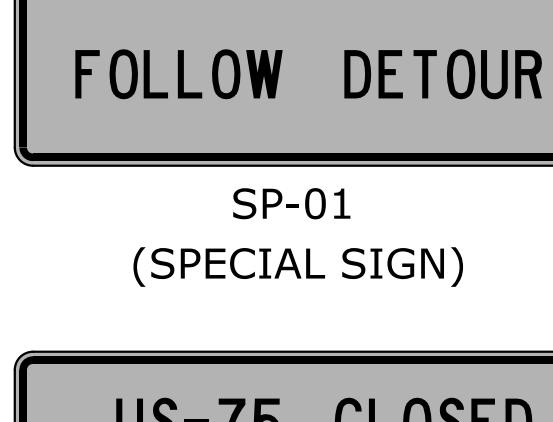
KANSAS DEPARTMENT OF TRANSPORTATION

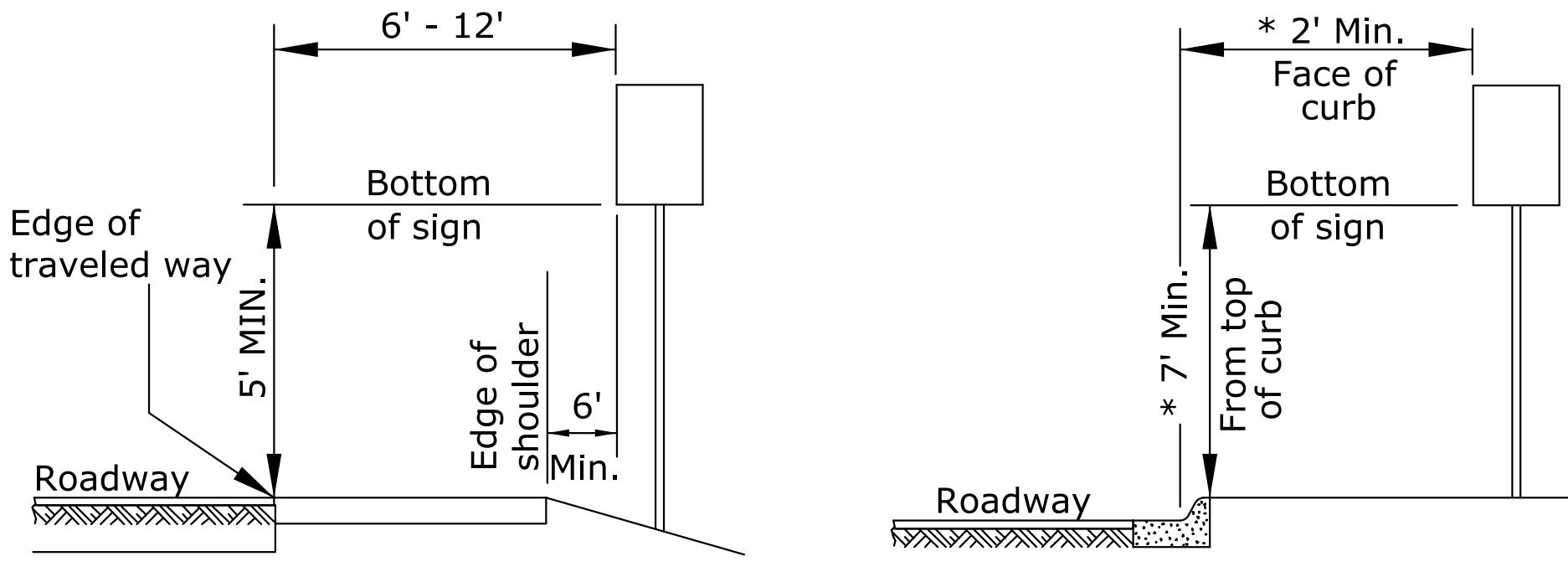
TRAFFIC CONTROL CLOSURES

TE704

HWY APPROVAL	06/01/15	APP'D	Kristina Erickson
DESIGNED	B.A.H. DETAILED	R.W.B.	QUANTITIES
DESIGN CK.	DETAIL CK.	QUAN. CK.	TRACE CK.

KDOT Graphics Certified 06-01-2015 Sh. No. 17

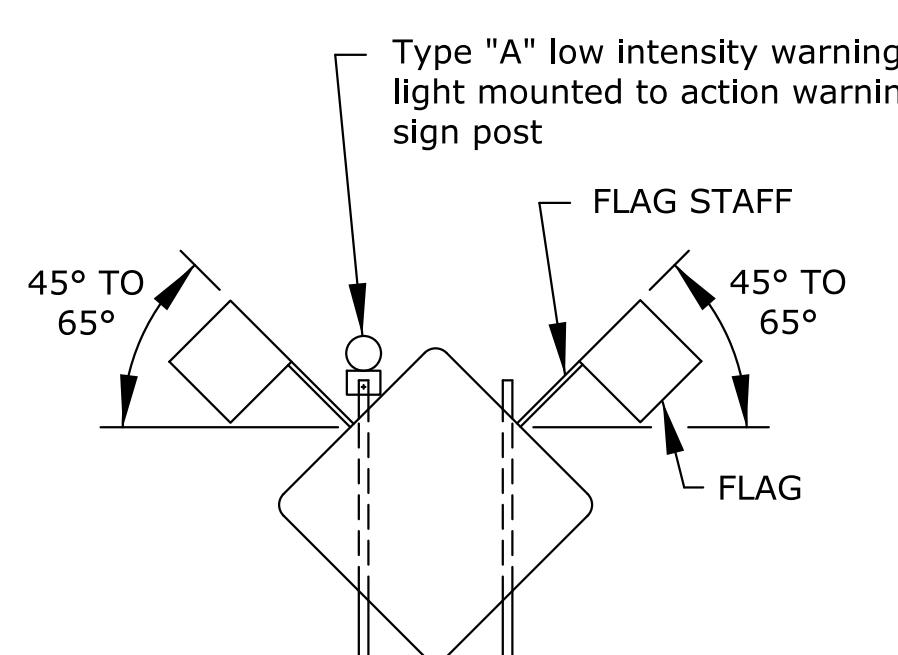
SIGN LAYOUT INFORMATION			
<b>END ROAD WORK</b> KG20-2	STD. SIZE EXPWY/FREEWAY 6" C 48"x 24"		STD. SIZE EXPWY/FREEWAY 8" D 48"x 48"
<b>WAIT FOR PILOT CAR</b> KG20-5	STD. SIZE EXPWY/FREEWAY 6" C 48"x 24"		STD. SIZE EXPWY/FREEWAY 8" D 48"x 48"
<b>WORK ZONE</b> KM4-20	STD. SIZE EXPWY/FREEWAY 3" C 24"x 6"      6" C 48"x 12"		STD. SIZE EXPWY/FREEWAY 30"x 24"
<b>NEXT X MILES</b> W7-3a	Mileage to be determined by the engineer.		STD. SIZE EXPWY/FREEWAY 8" D 48"x 48"
	STD. SIZE EXPWY/FREEWAY 48"x 48"		STD. SIZE EXPWY/FREEWAY 30"x 24"
<b>NB US-75 CLOSED FOLLOW DETOUR</b> SP-01 (SPECIAL SIGN)	STD. SIZE EXPWY/FREEWAY 6" C 10" D		STD. SIZE EXPWY/FREEWAY UPPERCASE: 6" C LOWER CASE: 4.5" C      UPPERCASE: 10" D LOWER CASE: 8" D
<b>US-75 CLOSED NORTH OF Topeka FOLLOW DETOUR</b> SP-02 (SPECIAL SIGN)	ALL CITY NAMES AND STREET NAMES ON SPECIAL SIGNS AND DESTINATION SIGNS MUST HAVE UPPER AND LOWER CASE LETTERS.		

**Rural**

- 1) Ground-mounted signs shall be mounted at a minimum height of 5' measured from the bottom of sign to the near edge of the pavement.
- 2) Large signs having an area exceeding 50 square feet installed on multiple breakaway posts shall be mounted a minimum of 7' above the ground.
- 3) The height of the secondary sign mounted below another sign may be 4' measured from the bottom of the sign to the near edge of the pavement. Signs shall not overlap each other.

**Urban**

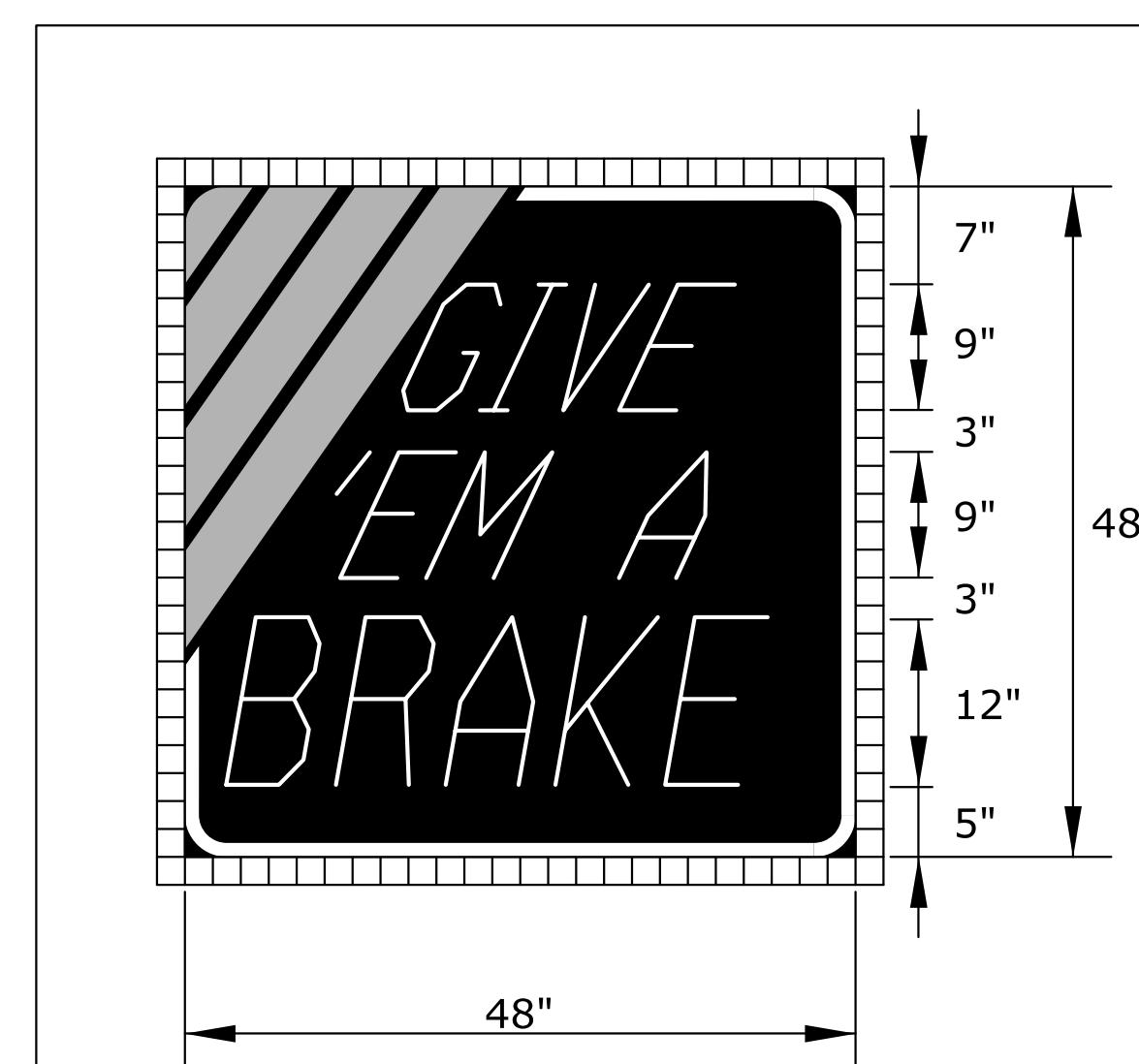
- 1) Signs shall be mounted at a minimum height of 7' measured from the bottom of sign to the near edge of the pavement.
- 2) Neither portable nor permanent sign supports should be located on sidewalks or areas designated for pedestrian or bicycle traffic.
- 3) Signs mounted lower than 7' should not project more than 4" into pedestrian facilities.
- 4) The height from of the secondary sign mounted below another sign may be 6' measured from the bottom of sign to the near edge of the pavement. Signs shall not overlap each other.
- 5) Large signs having an area exceeding 50 square feet installed on multiple breakaway posts shall be mounted a minimum of 7' above the ground.
- 6) Pedestrian detour signing shall be a minimum of 2' measured from the top of the pedestrian pathway to the bottom of the sign and shall not protrude into the walkway nor shall it project beyond the back of curb.



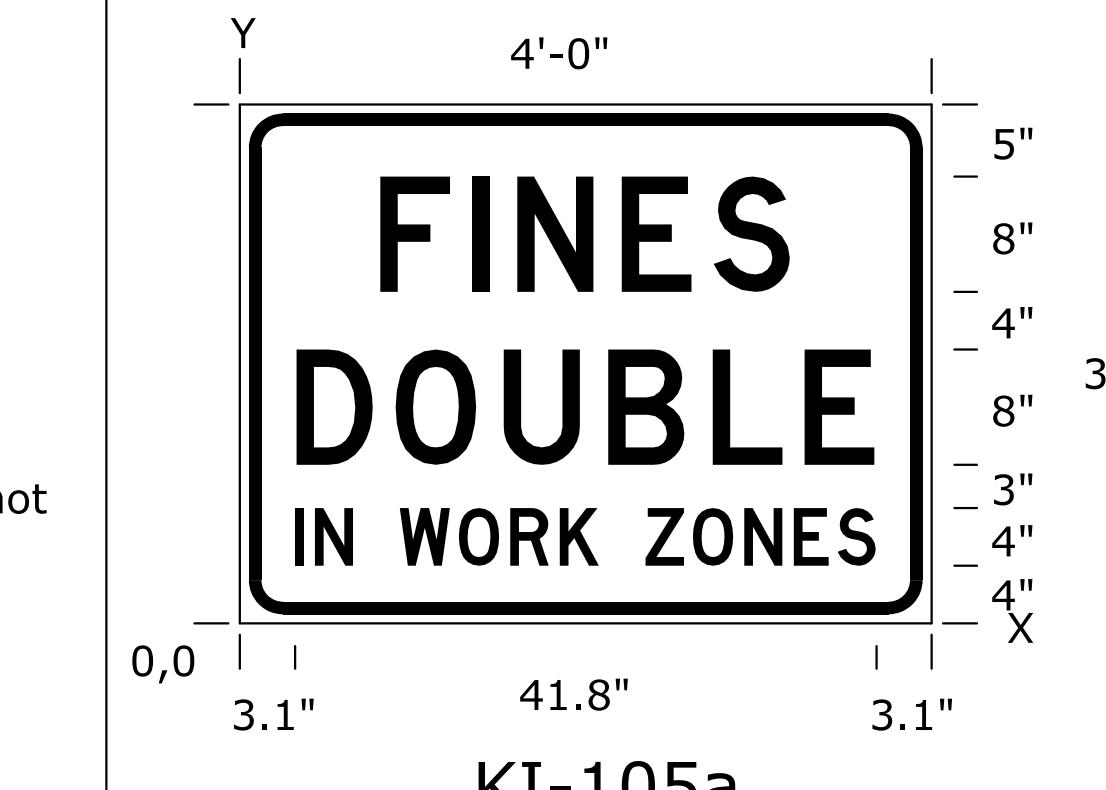
When the sign width is equal to or greater than 9', three or more wood posts may be used with a minimum of 4' between the centerline of each post. All signs less than 9' in width shall use a maximum of two wood posts.

In the case of hitting rock when driving posts

1. Shift the sign location. Do not violate minimum sign spacing.
2. With the engineer's approval, use acceptable alternative sign stands.



KI-104a



KI-105a

STATE	PROJECT NO.	YEAR	HEET NO.	TOTAL SHEETS
KANSAS	PW-024	2016	18	27

SIGN NUMBER	GIVE EM A BRAKE
WIDTH x HEIGHT	4'-0" x 4'-0"
BORDER WIDTH	1.0"
CORNER RADIUS	4.0"
STRIPE WIDTH	3.0"
MOUNTING	GROUND
BACKGROUND	TYPE: NON-REFLECTIVE COLOR: BLACK
LEGEND/BORDER	TYPE: REFLECTIVE COLOR: WHITE
LEGEND FONT	DUTCH 801 ROMAN SWC 25 DEGREE SLANT
STRIPES	TYPE: REFLECTIVE COLOR: ORANGE

SIGN NUMBER	FINES DOUBLE
WIDTH x HEIGHT	4'-0" x 3'-0"
BORDER WIDTH	0.9"
CORNER RADIUS	3.0"
MOUNTING	GROUND
BACKGROUND	TYPE: REFLECTIVE COLOR: WHITE
LEGEND/BORDER	TYPE: NON-REFLECTIVE COLOR: BLACK

## DIMENSIONS IN INCHES

Y	FONT	LETTER SPACINGS										HT LEN
23.0	F	I	N	E	S							8.0
D	9.7	6.4	3.2	7.3	6.4	5.4	9.7					28.6
11.0	D	O	U	B	L	E						8.0
	3.9	6.9	7.5	7.3	7.3	6.4	4.9	3.9				40.3
4.0	I	N	W	O	R	K	Z	O	N	E	S	4.0
D	3.1	1.6	2.7	3.2	4.3	3.8	3.6	2.8	3.2	3.4	3.8	41.8

## Notes:

Typically, there are two sets of informational signs installed per project: one for each direction of traffic.

Install signs a minimum of 500' in advance of the road work ahead sign. The engineer may designate a more appropriate location if conditions dictate.

The informational signs are not to interfere with the traffic control signs for the project.

3			
2			
1			
NO.	DATE	REVISIONS	BY APP'D

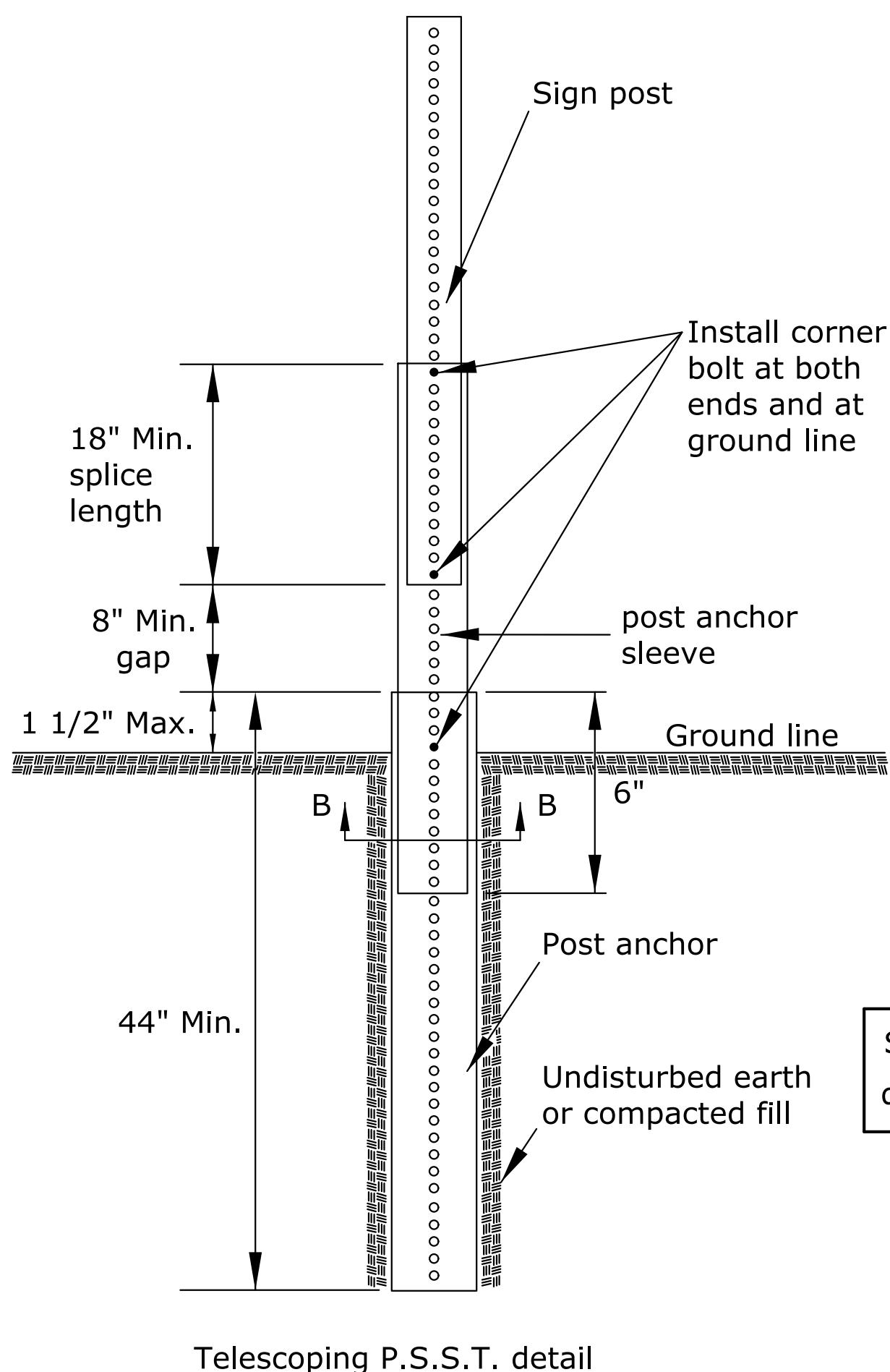
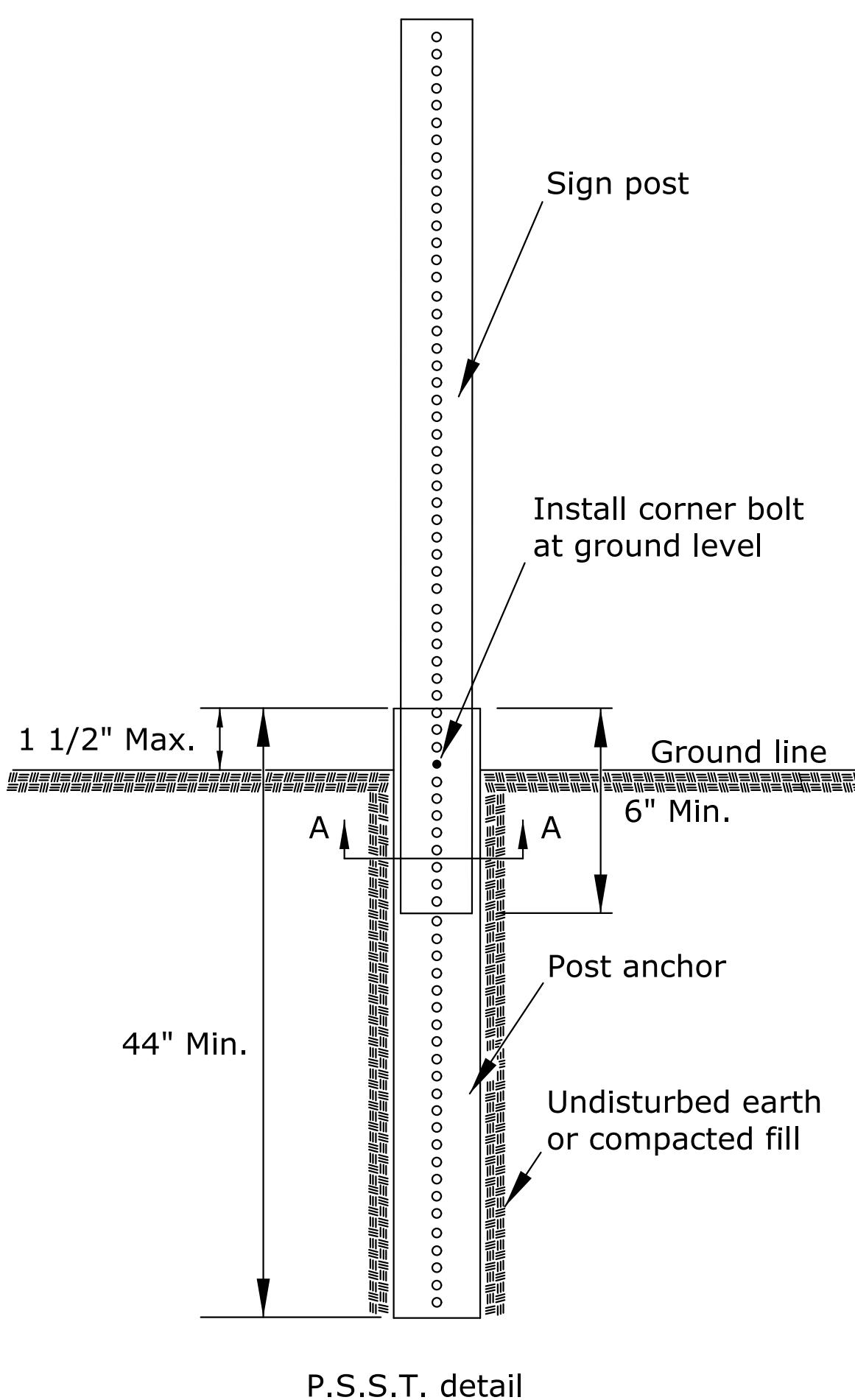
KANSAS DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL SIGN INFORMATION

TE710

HWY APPROVAL	06/01/15	APP'D	Kristina Pyle
DESIGNED	R.W.B.	DETAILED	R.W.B.
DESIGN CK.	DETAIL CK.	QUAN. CK.	TRACE CK.

# Perforated square steel tube (P.S.S.T.) post setup



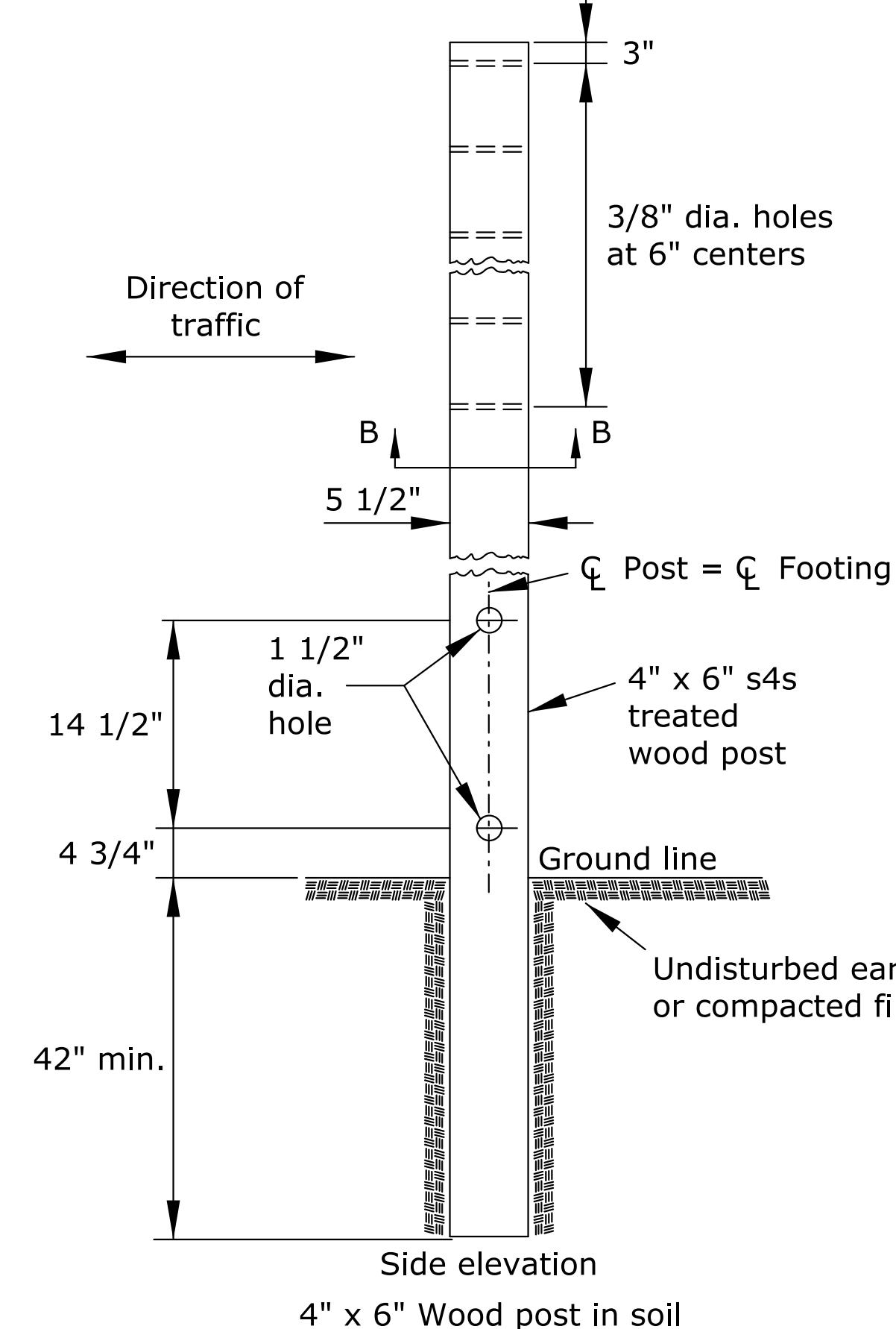
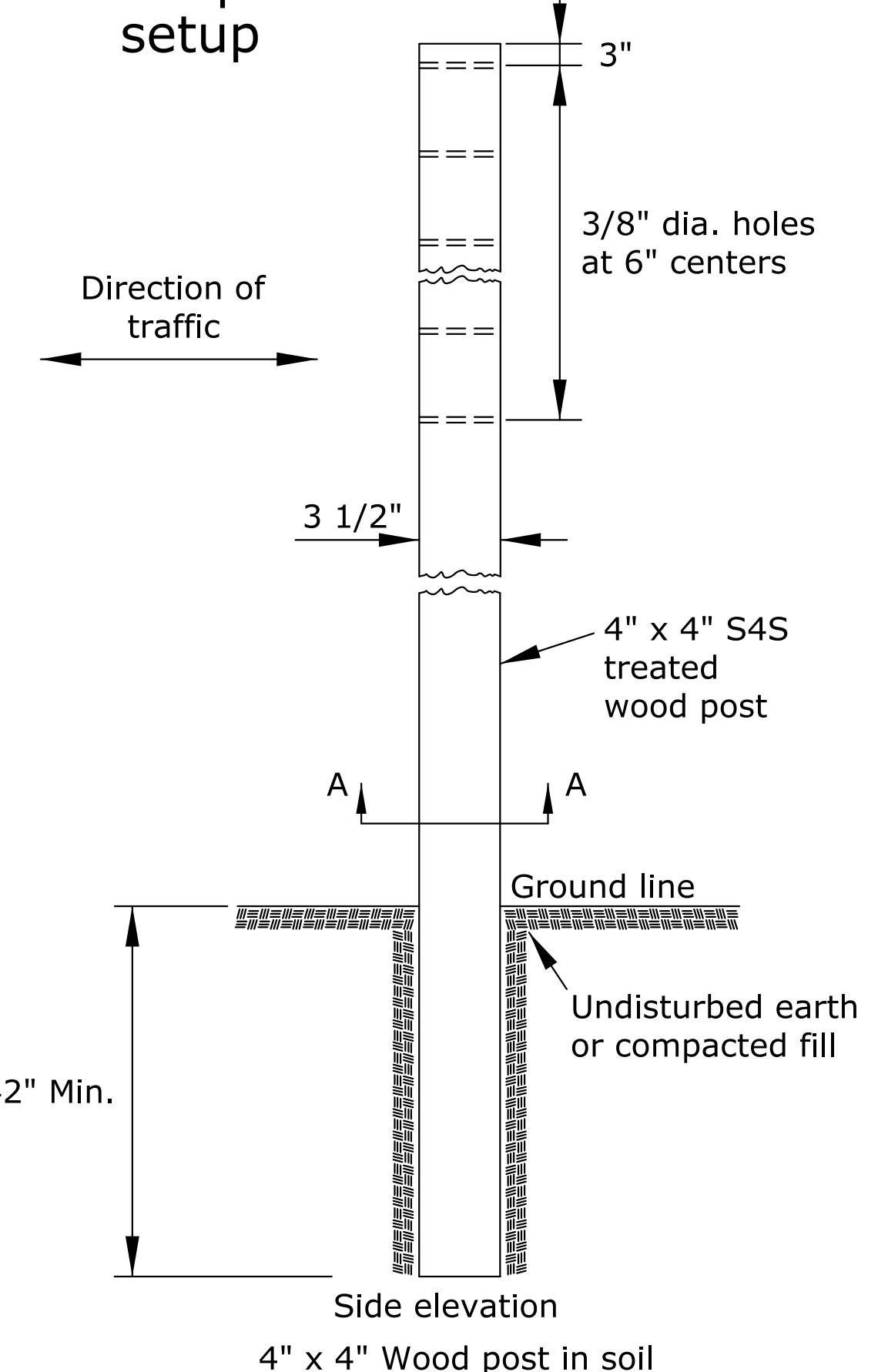
Plotted : 01-JUN-2015 13:54  
Traffic

Drawn By : mushock  
File : te712.dwg

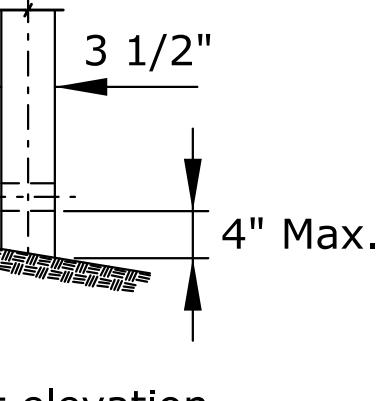
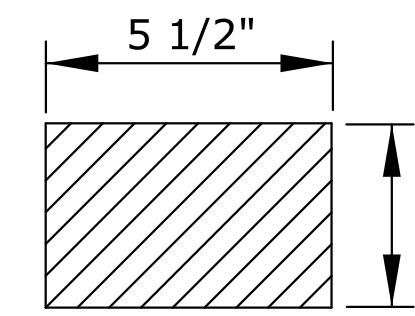
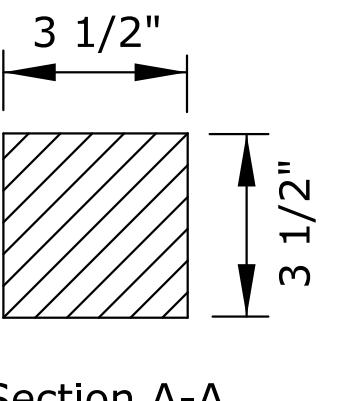
Details for 2", 2 1/4", or 2 1/2" sign posts

Place bolts in the same corner along each sign post.

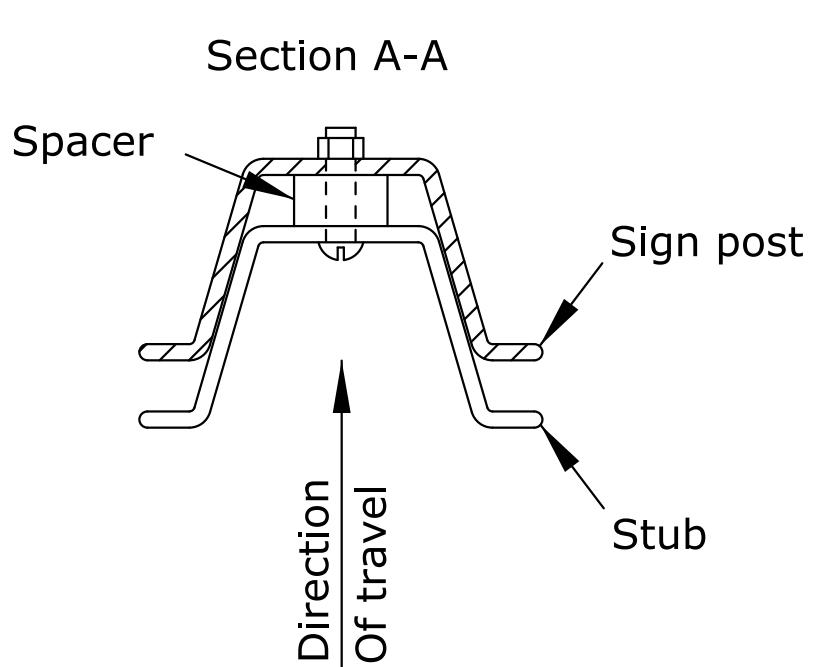
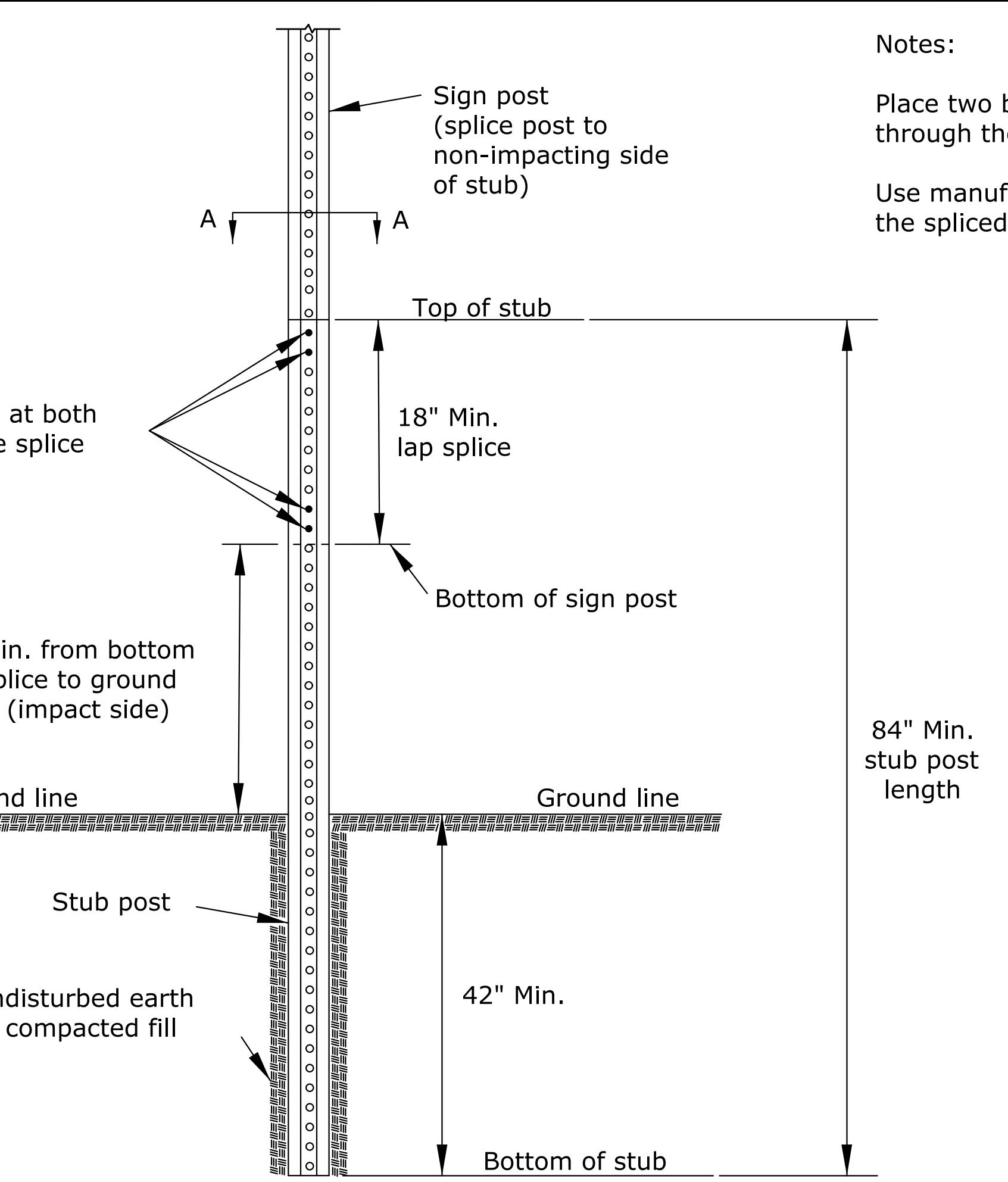
# Wood post setup



STATE	PROJECT NO.	YEAR	HEET NO.	TOTAL SHEETS
KANSAS	PW-024	2016	19	27



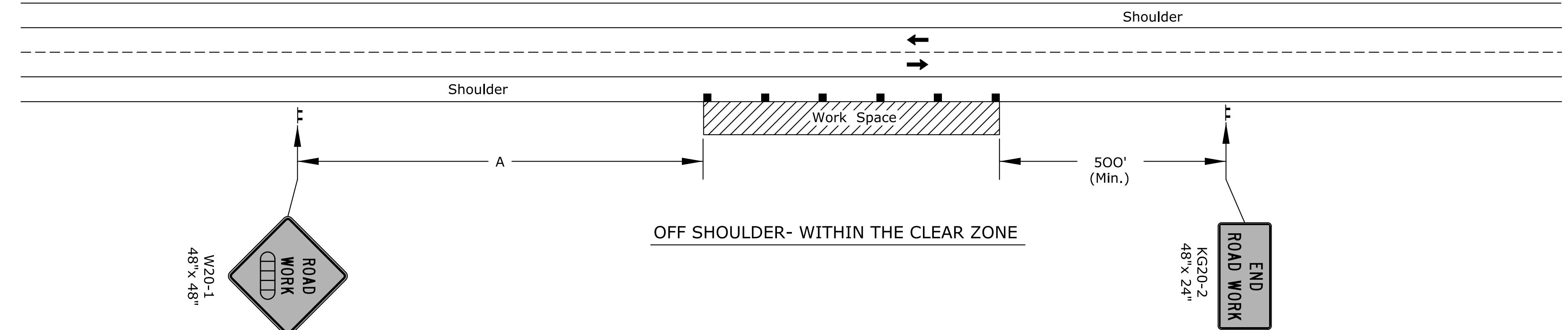
# 3 lb/f U-Channel setup



3			
2			
1			
NO.	DATE	REVISIONS	BY APP'D

KANSAS DEPARTMENT OF TRANSPORTATION  
TRAFFIC CONTROL  
SIGN POSTS  
TE712

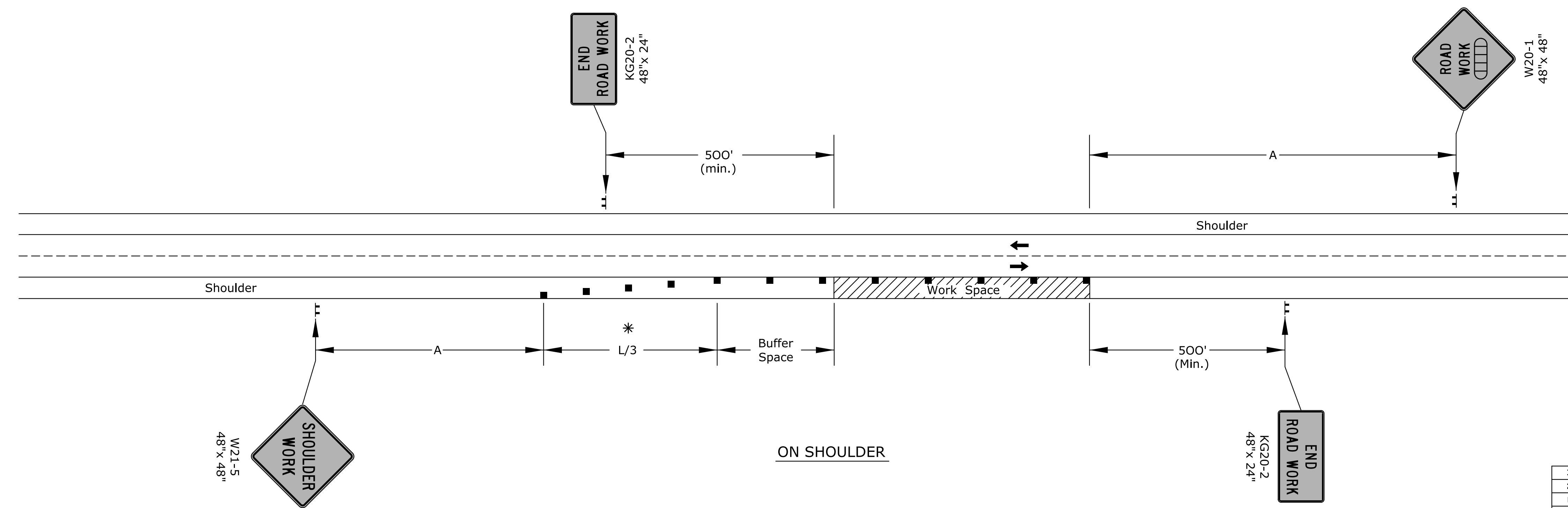
HWY APPROVAL 06/01/15 APP'D Kristina Pyle  
DESIGNED B.A.H. DETAILED R.W.B. QUANTITIES TRACED  
DESIGN CK. DETAIL CK. QUAN. CK. TRACE CK.



Notes:

No traffic control is required if the work space is located outside of the clear zone.

For operations of 60 minutes or less, all signs and channelizing devices may be eliminated if a vehicle with high-intensity rotating, flashing, oscillating, or strobe lights is used.



\* Omit taper if paved shoulder is less than 8' wide.

# Summary Of Traffic Control Devices (Each)

# Summary Of Traffic Control Devices (Each Per Day)

\* Quantity Most Used On The Project At Any One Time

Barricades *		Channelizing Devices *		
Type 3 (4' To 12')	Pedestrian	Fixed	Portable	Pedestrian
27	16		50	

Lighted Devices *	
Work Zone Warning Light (Type "A" Low Intensity)	22
Work Zone Warning Light (Red Type "B" High Intensity)	
Arrow Display	
Portable Changeable Message Sign	

3				
2				
1				
NO.	DATE	REVISIONS	BY	APP'D

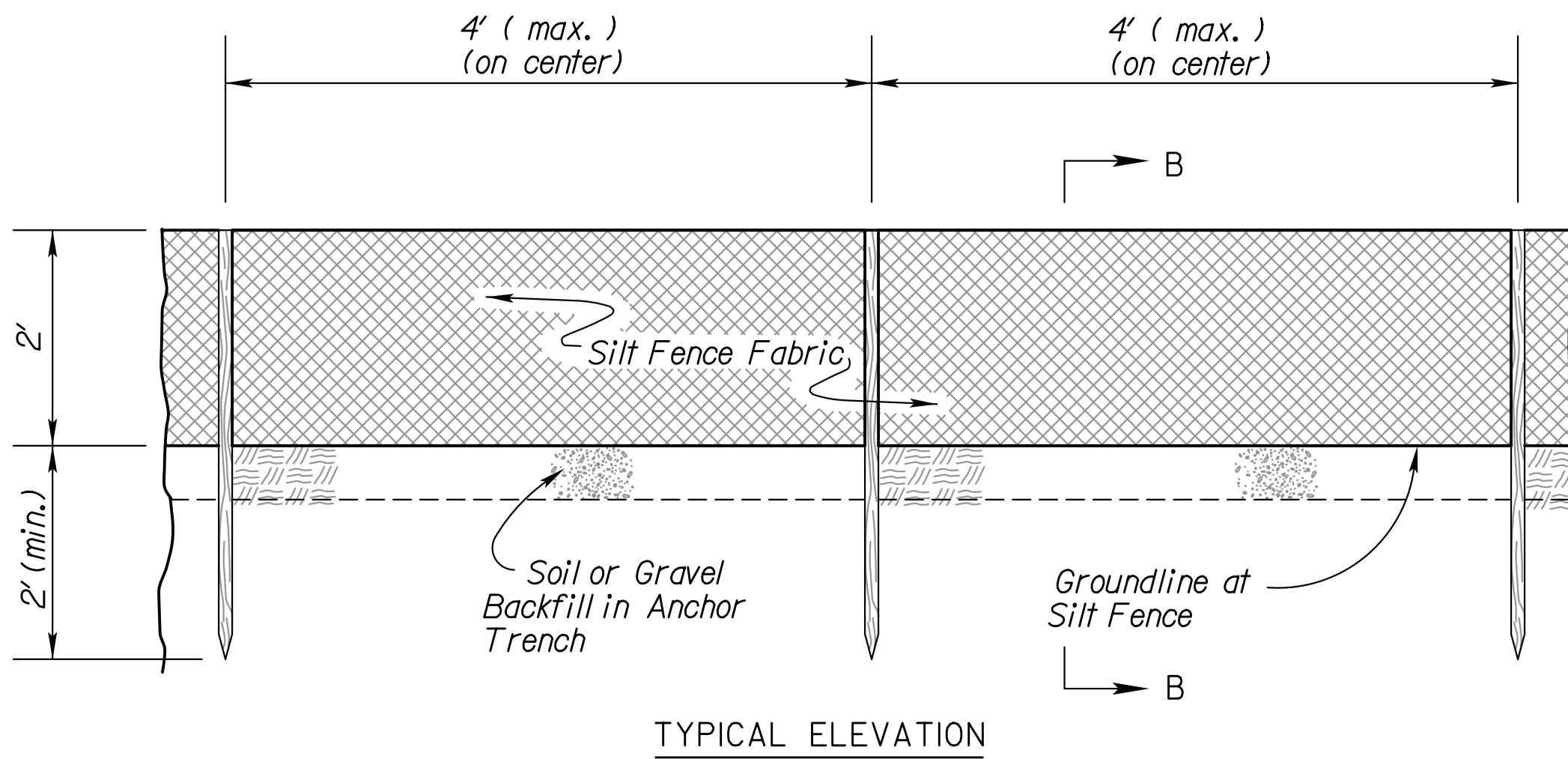
KANSAS DEPARTMENT OF TRANSPORTATION

# TRAFFIC CONTROL SUMMARY OF DEVICES E CAPITULATION OF QUANTITIES

WA APPROVAL 06/01/15 APP'D Kristina Erickson  
SIGNED B.A.H. DETAILED R.W.B. QUANTITIES TRACED  
SIGN CK. DETAIL CK. QUAN. CK. TRACE CK.



STATE	PROJECT NO.	YEAR	HEET NO.	TOTAL SHEETS
KANSAS	PW-024	2016	23	27



#### INSTALLATION NOTES

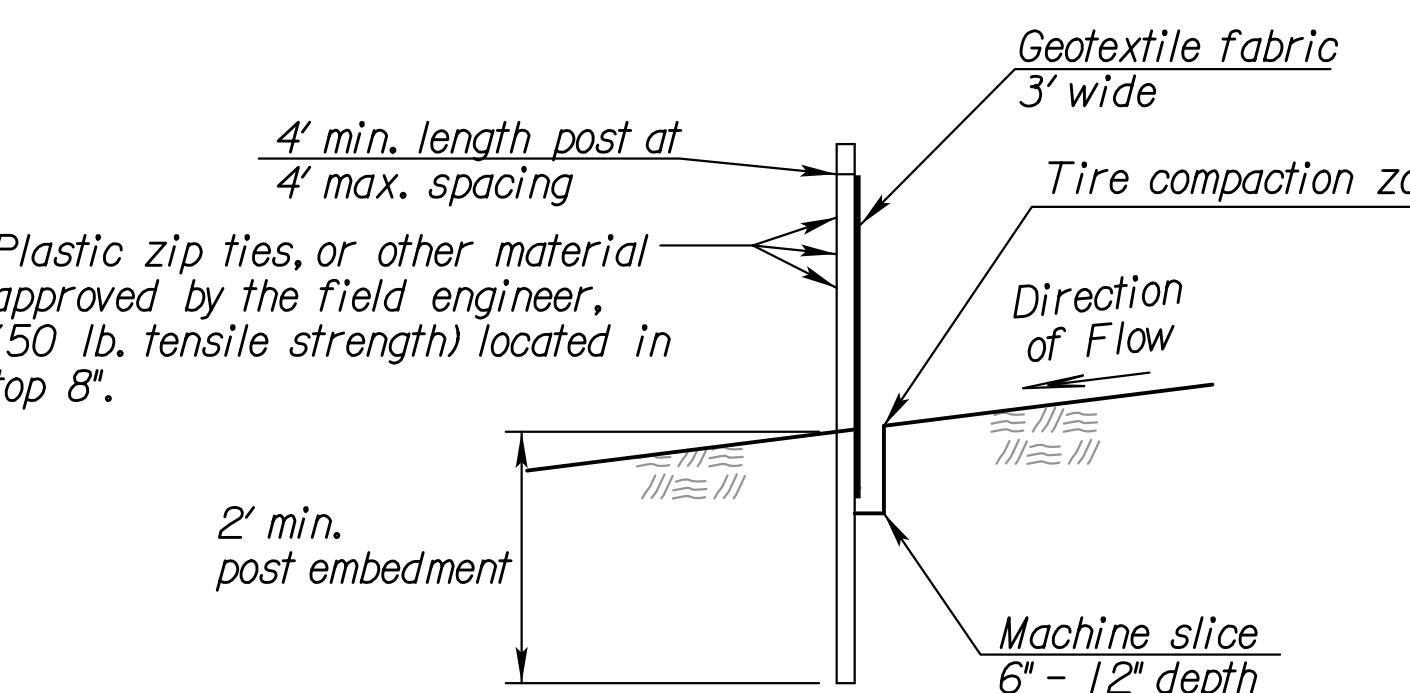
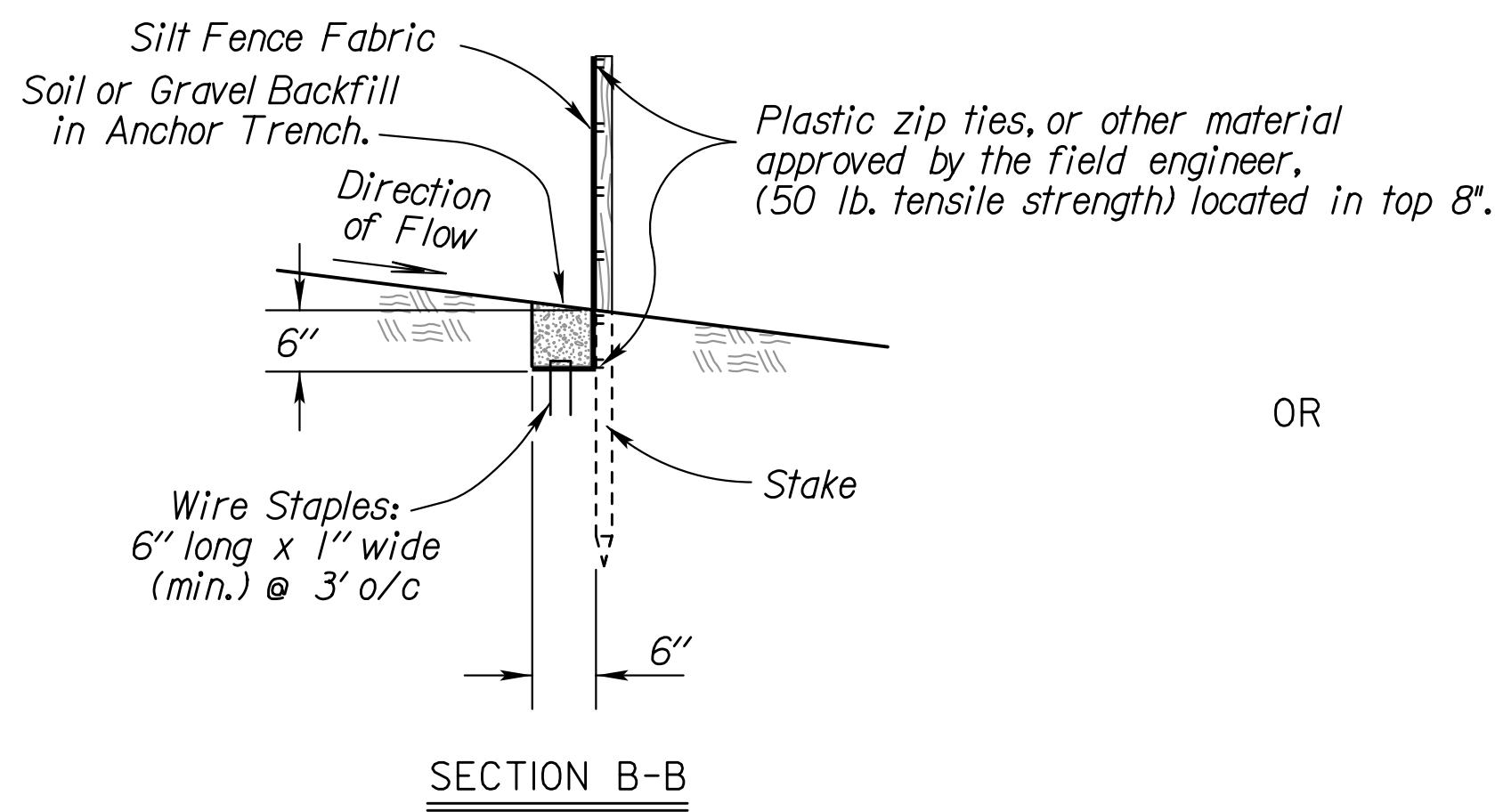
##### SILT FENCE:

1. Stakes shall be 4' (min.) long and of one of the following materials:
  - a. Hardwood - 1 3/16" x 1 3/16";
  - b. Southern Pine (No. 2) - 2 5/8" x 2 5/8";
  - c. Steel U, T, L, or C Section - .95 lbs. per 1'-0"; or
  - d. Synthetic - same strength as wood stakes.
2. Cross pieces shall be of same material as stakes.
3. Attach fence fabric securely on 6" centers (max.).
4. Use of high flow material is acceptable.
5. Refer to plan sheets to estimate the length of silt fence required.

##### BIODEGRADABLE LOG BARRIERS

1. Place biodegradable logs tightly together.
2. Wood stakes shall be 2" x 2" (nom.).
3. Wire staples shall be 6" long x 1" wide (min.) and placed on 4' (max.) centers.
4. Refer to plan sheets to estimate length of biodegradable log barriers required.
5. Logs should be keyed into the ground at a minimum of 25% of its height.
6. Length of stakes should be 2 times the height of the log at a minimum.

**SILT FENCE SLOPE BARRIER**  
NO SCALE

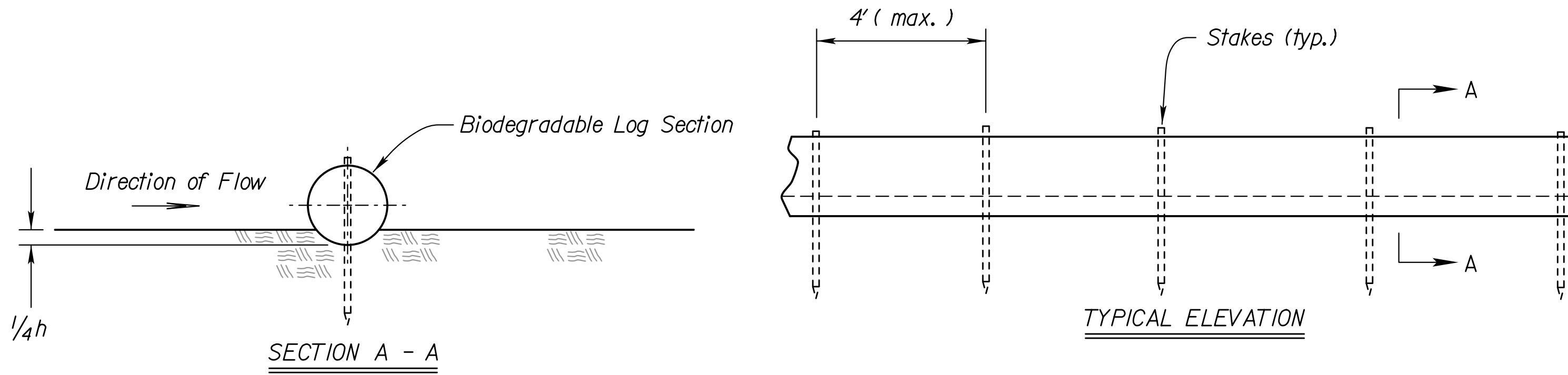


#### Biodegradable Logs, Straw Wattles & Sediment Logs

PRODUCT			
	9" Sediment Log & 9" Straw Wattle (ft)	12" Sediment Log & 12" Straw Wattle (ft)	20" Sediment Log & 20" Straw Wattle (ft)
≤4H:IV	40	60	80
3H:IV	30	45	60
2H:IV	20	30	40
1H:IV	10	15	20

BIODEGRADABLE LOG MATERIAL		
	LOW FLOW	HIGH FLOW
9" Straw/Compost	Excelsior / Wood Chips / Coconut Fiber	
12" Straw/Compost	Excelsior / Wood Chips / Coconut Fiber	
18"-20" Straw/Compost	Excelsior / Wood Chips / Coconut Fiber	

9" and 12" material should only be used in areas which have been seeded and mulched. 20" material should be used in all other areas. Deviations should be approved by the Field Engineer.



**BIODEGRADABLE LOG SLOPE BARRIER**  
NO SCALE

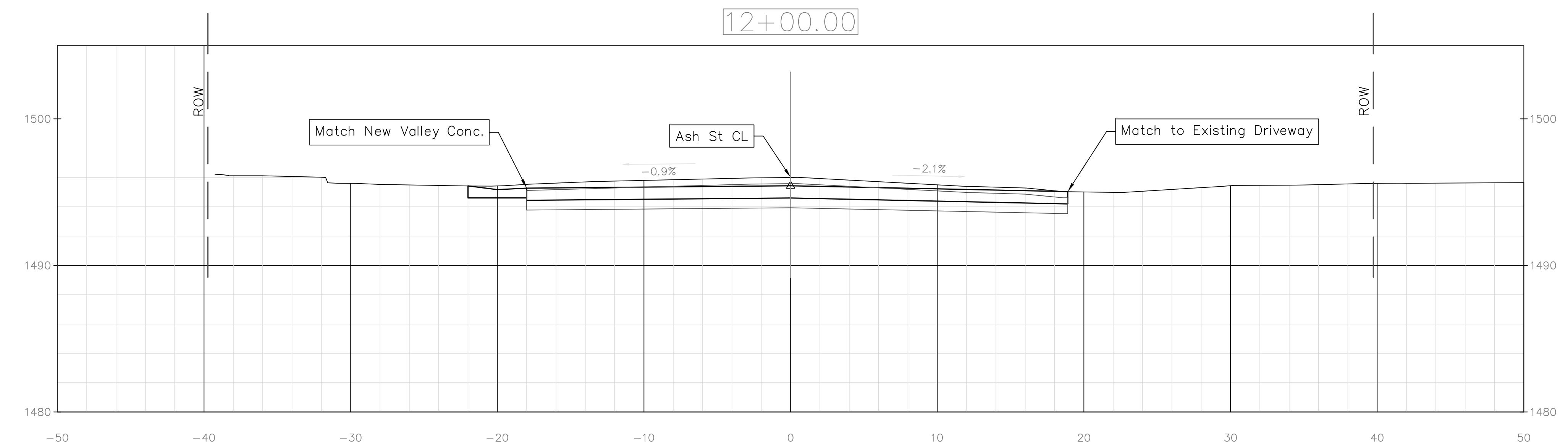
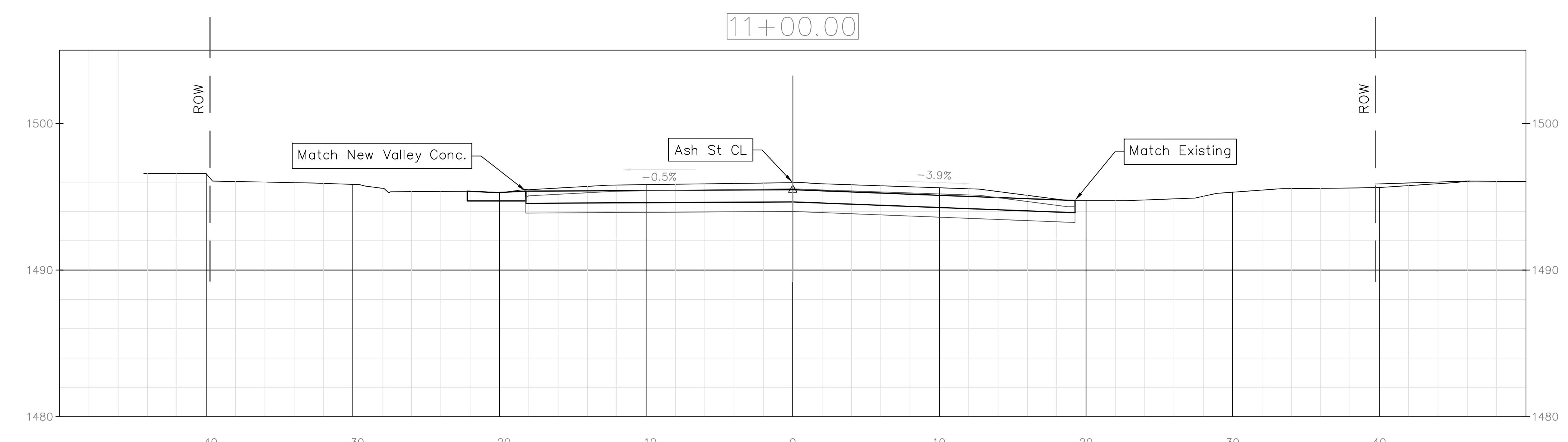
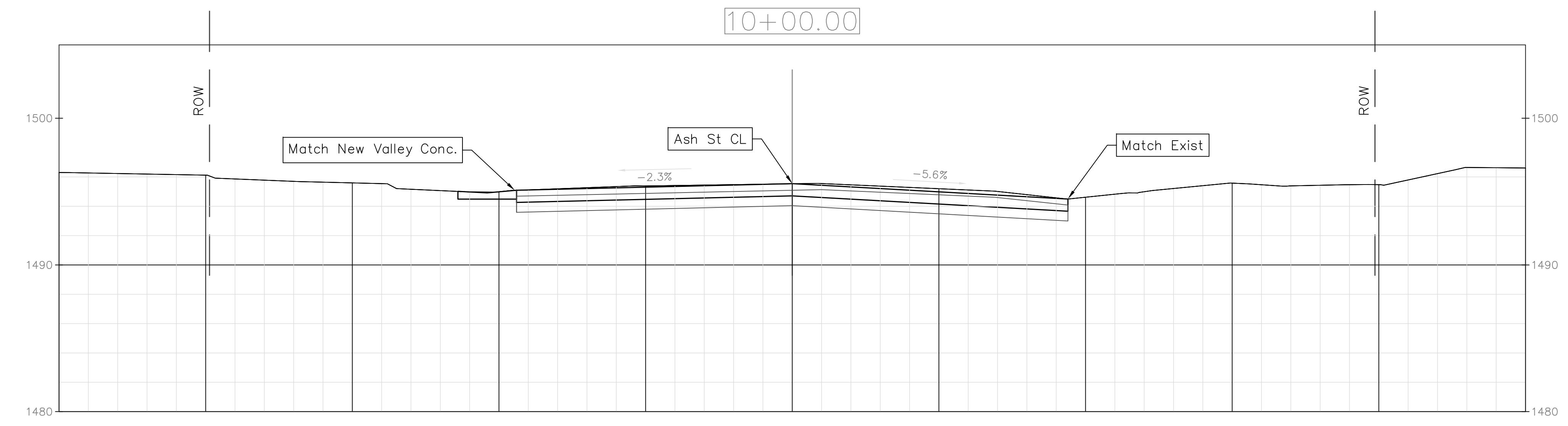
#### GENERAL NOTES

- 1) The slope barriers shall be placed along contour lines, with a short section turned upgrade at each end of the barrier. The maximum length of the slope barrier shall not exceed 250 feet, and the barrier ends need to be staggered.
- 2) At culverts, the Silt Fence shall be placed over the culvert, not through the streambed flowline.
- 3) Barriers damaged by Contractor's negligence, including improper maintenance or lack of maintenance, shall be repaired immediately by Contractor at no additional cost to KDOT.
- 4) Agricultural products, such as native prairie hay, used for mulching and erosion control practices, excluding wood based mulch, shall meet the North American Weed Free Forage Standards.

3	3/01/15	Revised Standard	RA	SHS
2	6/01/13	Revised Standard	MRM	SHS
I	3/01/13	Revised Standard	MRM	SHS
NO.	DATE	REVISIONS	BY	APP'D

**KANSAS DEPARTMENT OF TRANSPORTATION**  
**TEMPORARY EROSION AND POLLUTION CONTROL**  
**SILT FENCE SLOPE BARRIERS**  
**BIODEGRADABLE LOG SLOPE BARRIERS**  
LA852D

FHWA APPROVAL 3/10/2015 APP'D Scott H. Shields  
DESIGNED RA DETAILED RA QUANTITIES CADD  
DESIGN CK. SHS DETAIL CK. QUAN.CK. CADD CK.



DATE	REVISIONS	BY	APP'D

CROSS SECTIONS	McPHERSON	PLANS
10+00 - 12+00	CITY OF MCPHERSON, KANSAS	BY
	PUBLIC WORKS DEPT.	DATE
VERTICAL		
ENG 0.05" 1/32" ARCH	SCALE 5 10 15 20	
	2 1 3 4 1/16"	
HORIZONTAL		

REVISIONS

BY

APP'D

DATE

CROSS SECTIONS

10+00 - 12+00

VERTICAL

ENG 0.05" 1/32" ARCH

SCALE 5 10 15 20

2 1 3 4 1/16"

HORIZONTAL

McPHERSON

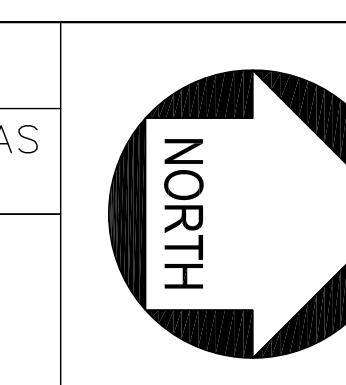
CITY OF MCPHERSON, KANSAS

PUBLIC WORKS DEPT.

FINAL PLANS

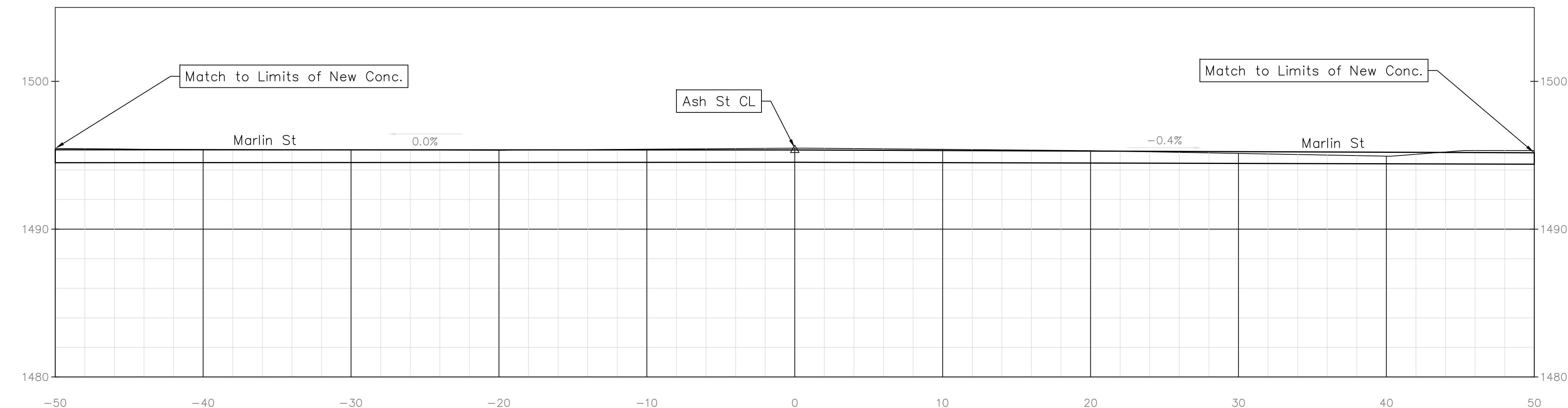
APPROVED FOR

CONSTRUCTION

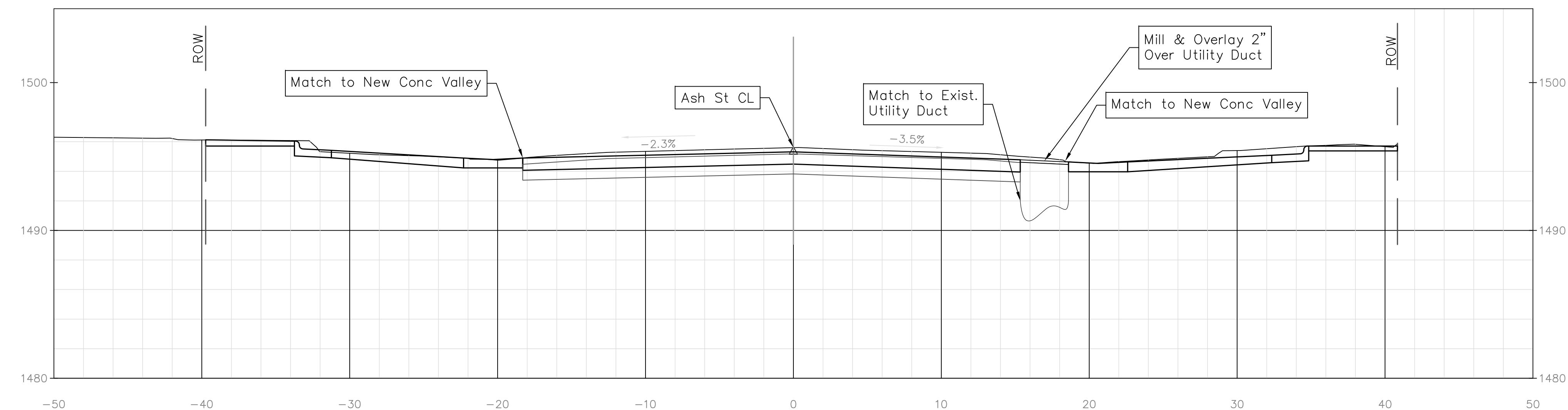


PLANS	BY	DATE
SURVEY:		
DESIGNED:	JustinW	3/21/2016
DRAWN:	JustinW	3/21/2016
CHECKED:	JeffW	2016-05-06
PLOT BY:	JustinW	2016-05-06
FILE:	2016-05-03_AshStDesign.dwg	
PROJECT NO:	PW-024	SHEET 24 OF 27

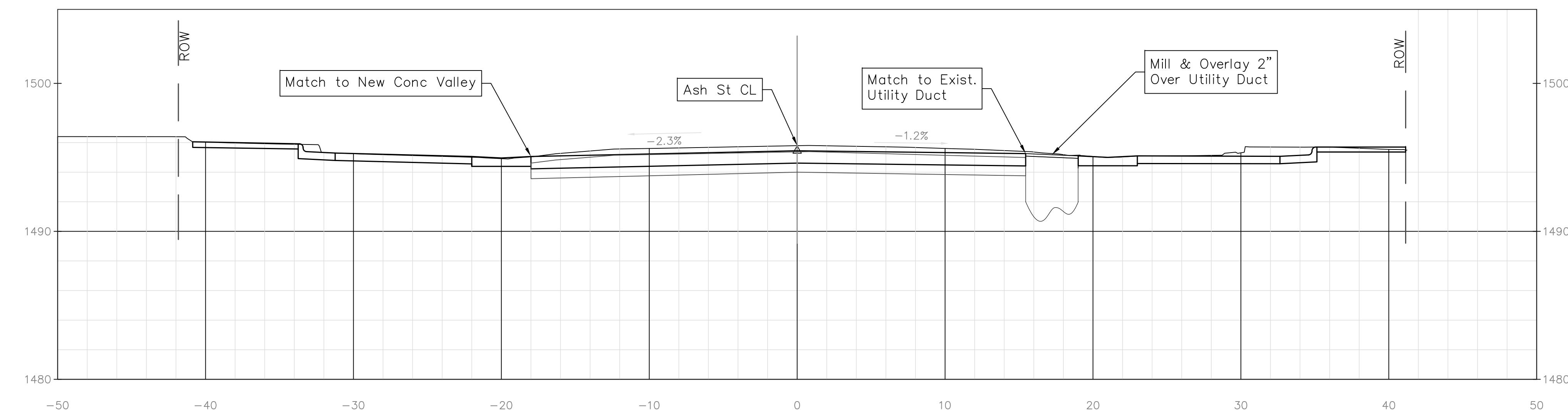
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14+00.00



15+00.00



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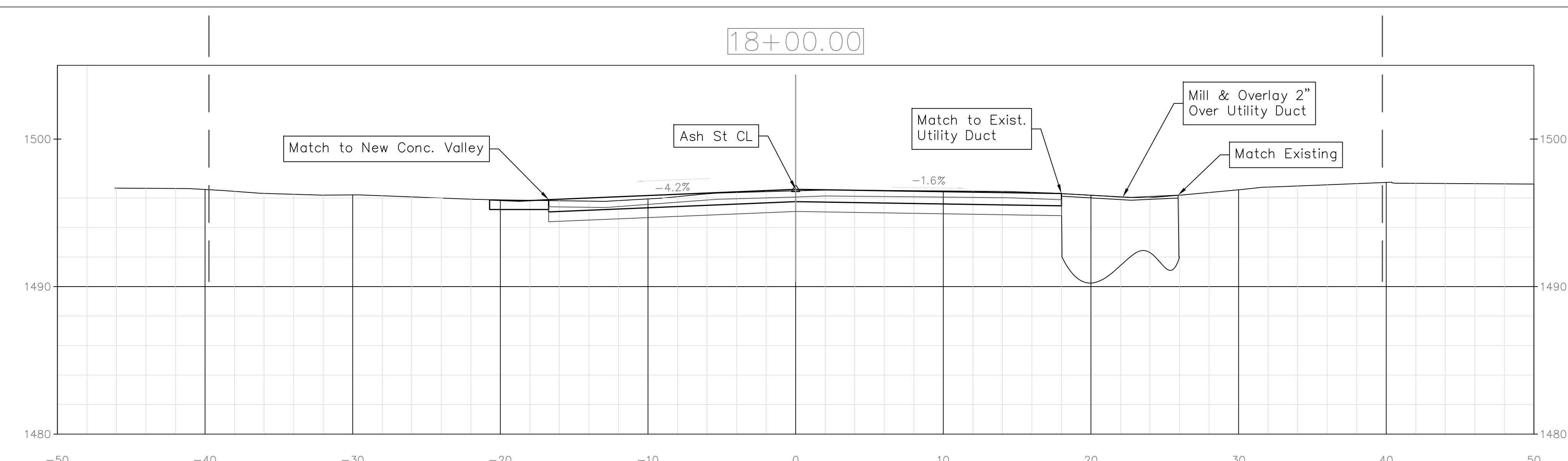
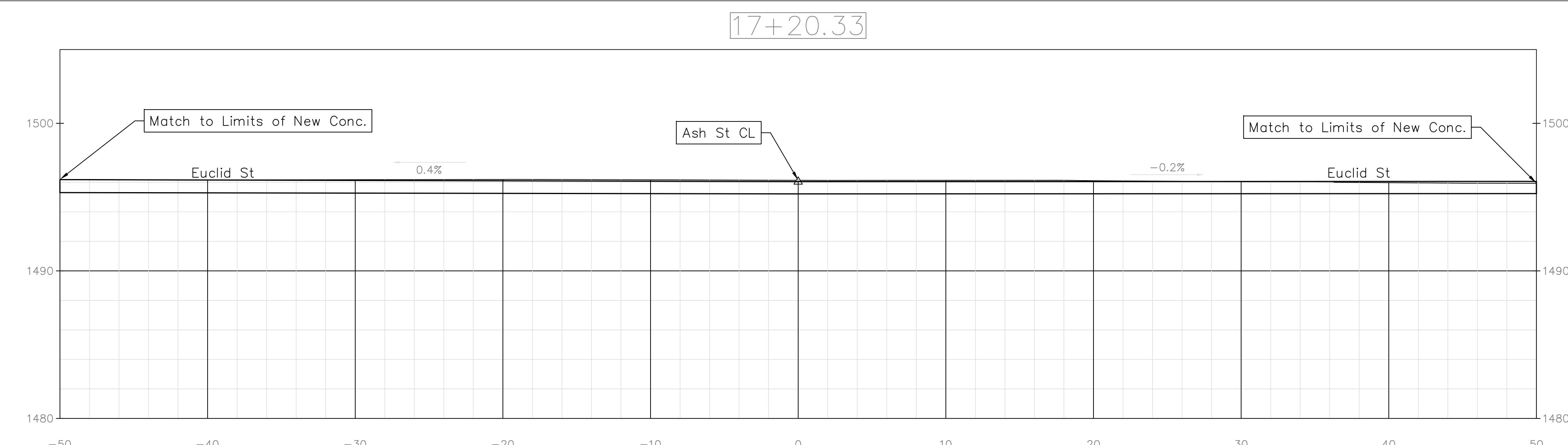
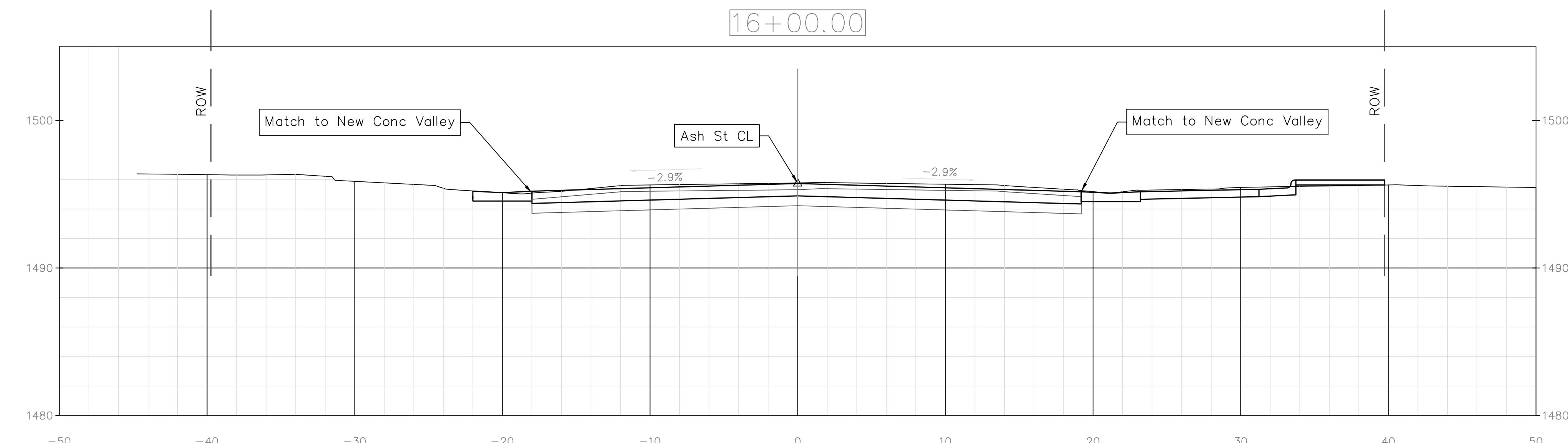
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