

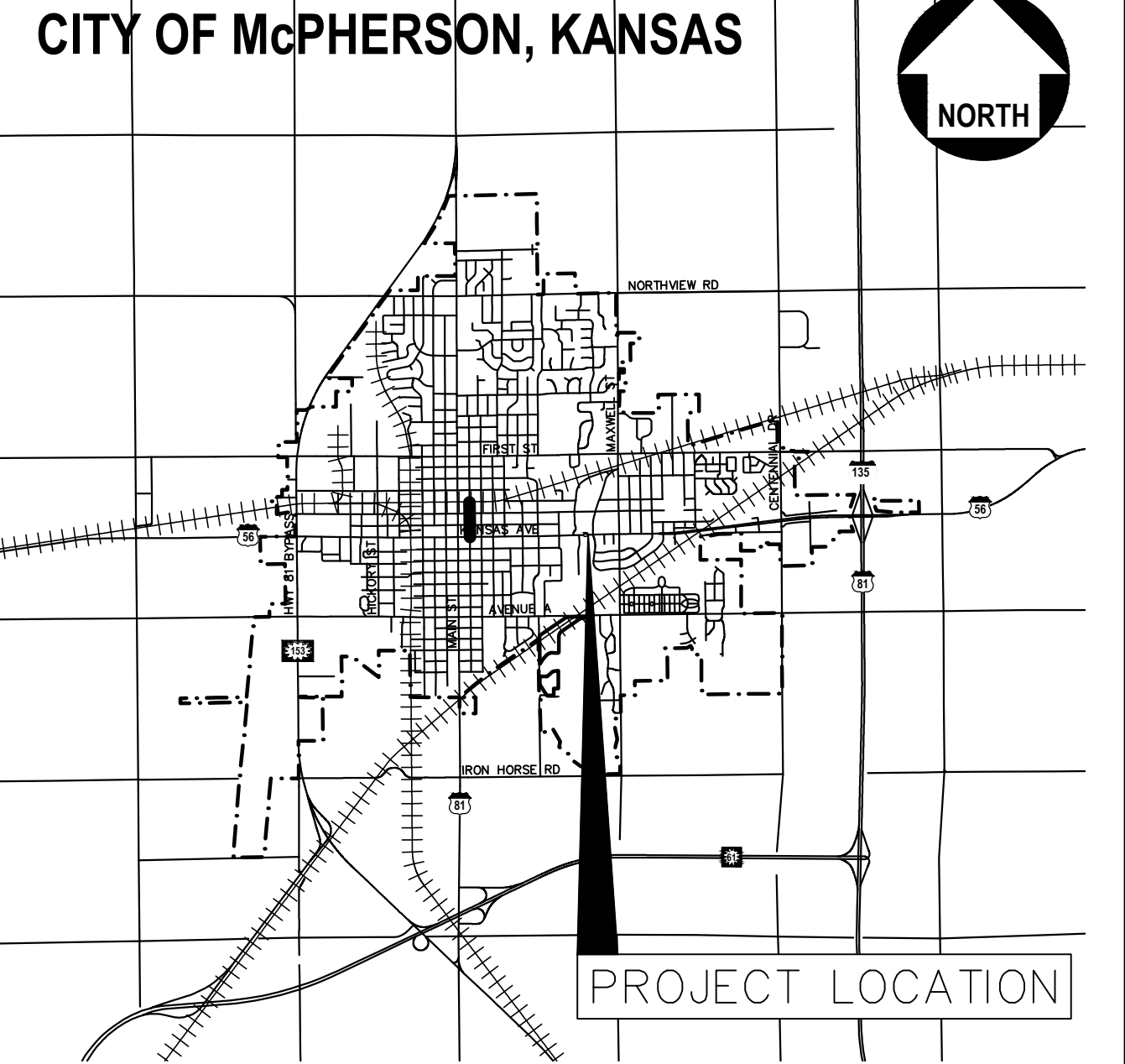
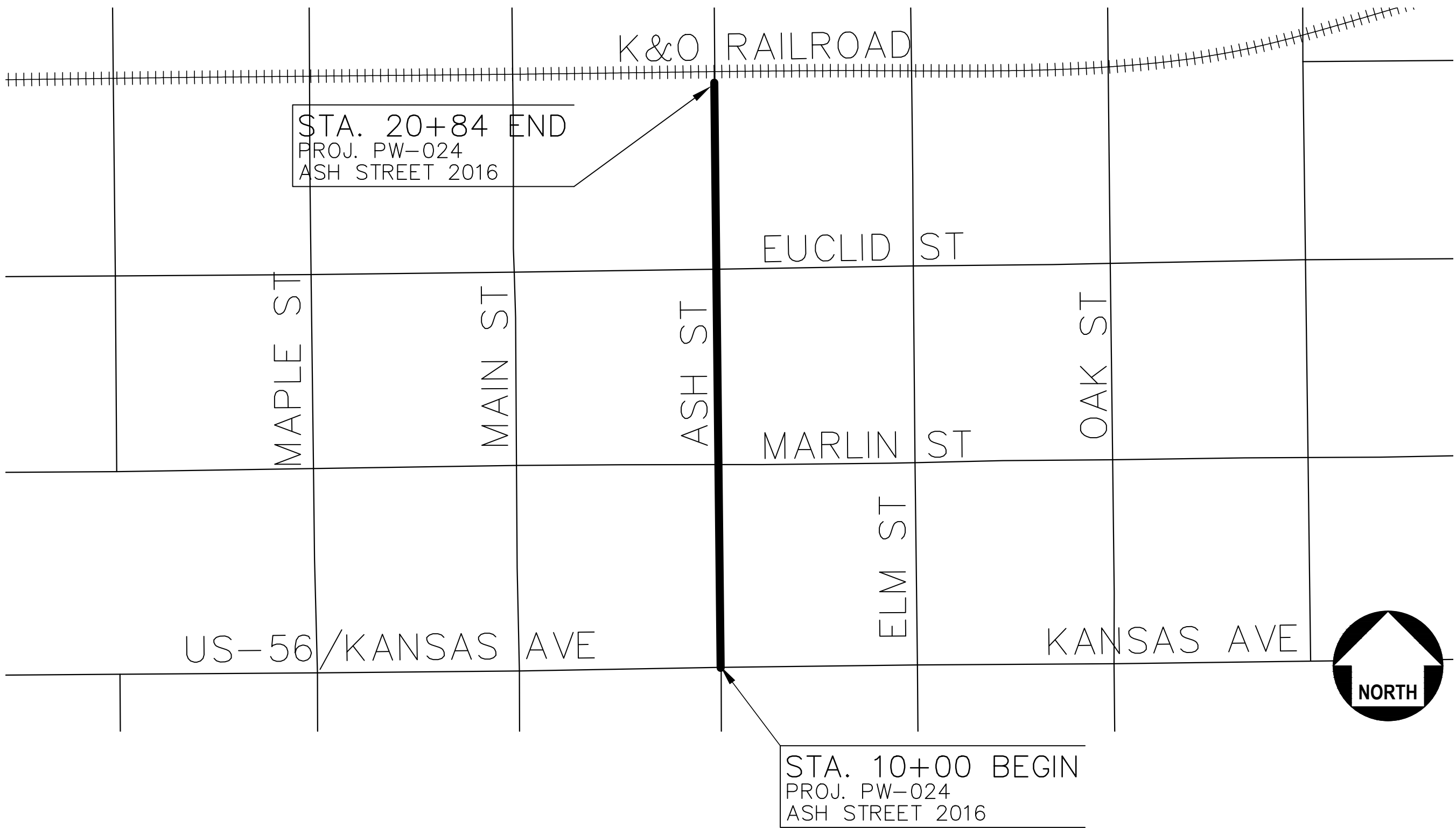
INDEX OF SHEETS

SHEET	DESCRIPTION
1	TITLE SHEET
2	TYPICAL SECTIONS/GENERAL NOTES
3-4	PLAN/PROFILE
5-8	INTERSECTION DETAILS
9	PARKING LOT DETAIL
10-12	PAVING/SIDEWALK DETAILS
13	PAVEMENT MARKING DETAILS
15-21	TRAFFIC CONTROL
22-23	EROSION CONTROL
24-27	CROSS SECTIONS

CITY OF McPHERSON, KANSAS
DEPARTMENT OF PUBLIC WORKS

2016 STREET IMPROVEMENTS

ASH ST
KANSAS AVE TO K&O RAILROAD
P.W. PROJECT - 024



- MILLING**
GRADING (8" GeoGrid/Aggregate Base)
BITUMINOUS PAVEMENT SURFACE (10")
BITUMINOUS PAVEMENT OVERLAY (2")
ADA RAMPS (By City Forces)
SIDEWALK (By City Forces)
OFF-STREET PARKING (By City Forces)
VALLEY GUTTER (By City Forces)

UTILITY OWNERS

- Electric & Water –
Board of Public Utilities
Mark Wurm, PE
401 W Kansas Ave
McPherson,Ks 67460
620–755–5665

Storm and Sanitary Sewer –
City of McPherson
Jeff Woodward, PE
400 E Kansas Ave
McPherson,Ks 67460
620–245–2545

Gas Service –
Kansas Gas Service
Dennis Alexander
1644 W Kansas Ave
McPherson,Ks 67460
620–241–0837 ext 224

Telephone –
AT&T
Scott Dunlap
137 S 7th
Salina,Ks 67401
620–665–1939
- Cable –
Cox Communications
Noel Tolbert
901 S George Washington Blvd
Wichita,Ks 67211
316–214–4488

Fiber Optic –
Zayo Group
Tommy Bunce
102 N Main St
Buhler,Ks 67522
620–200–3621

Fiber Optic –
Mutual Telephone Company
John Teitjens
Little River,Ks 67457
620–897–6200

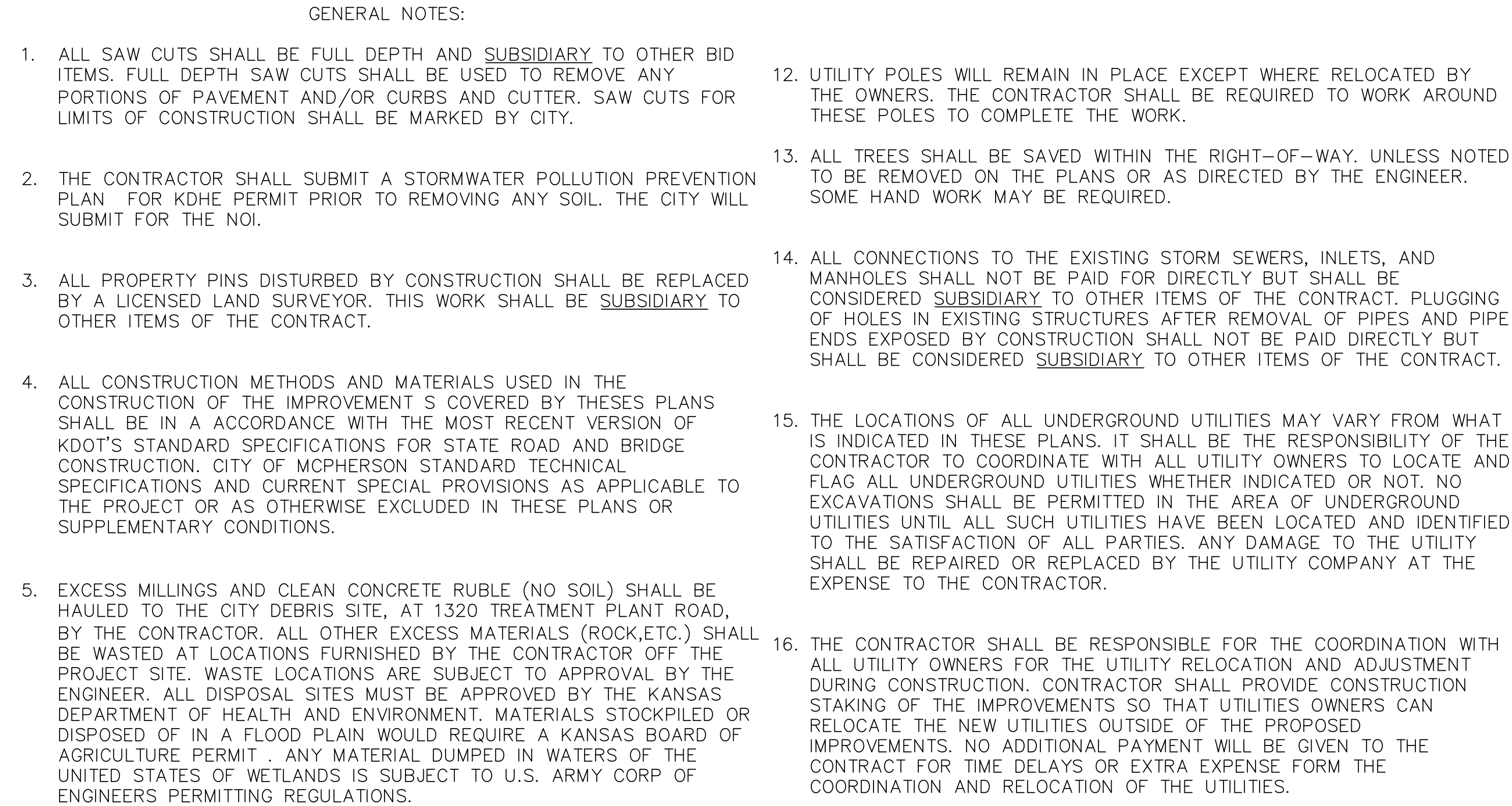
TOTAL LENGTH: 1084 FT
0.205 Miles

LANE MILES: 0.410 Miles

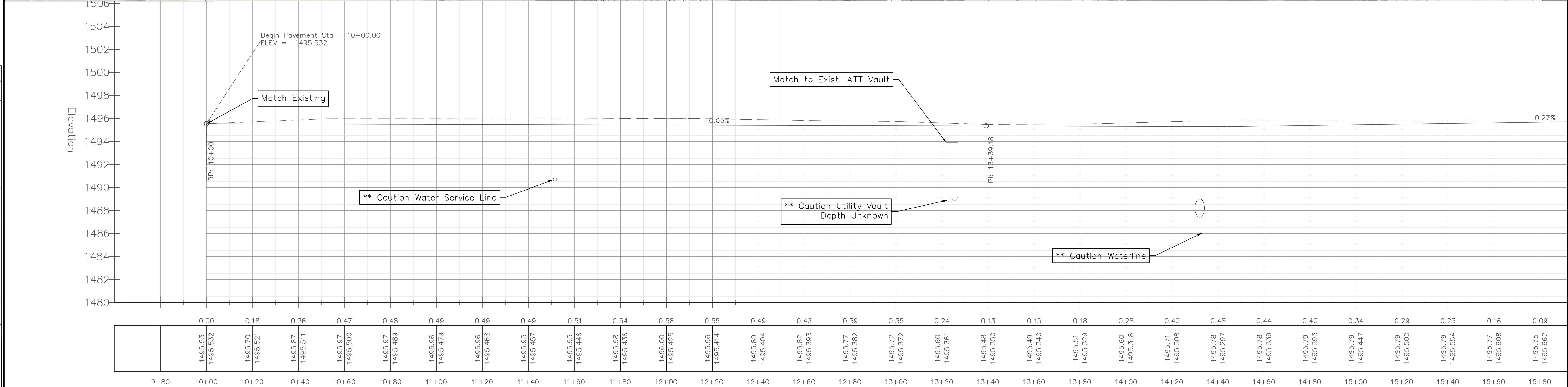
STREET DESIGN CRITERIA					
STREET	LENGTH Ft.	LANES	AADT 2013	AADT 2033	% TRUCKS
ASH STREET	1084	2	9590	14250	6%

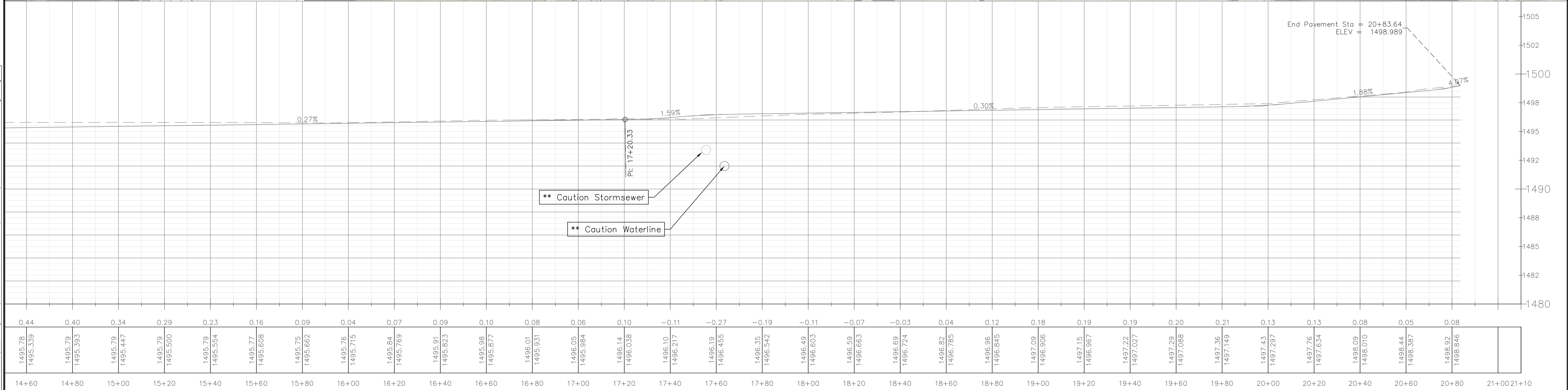
PLANS REVIEWED BY	DATE	APP'D	DATE	REVISION	BY	APP'D

PROJECT NO: P.W. PROJECT – 024	FILE: PW–024_TitleSheet.dwg
WORK ORDER NO: -----	PLOT BY: JustinW
CONSTR COMPL:	PLOT DATE: 2016–05–06
FIELD REVISIONS:	SHEET 1 OF 27

[illegible]

PLANS	BY	DATE
SURVEY:	RichardH/JustinW	1/18/2016
DESIGNED:	JustinW	2/3/2016
DRAWN:	JustinW	3/17/2016
CHECKED:	JeffW	2016-05-10
PLOT BY:	JustinW	2016-05-10
FILE:	2016-05-03_AshStDesign.dwg	
PROJECT NO: PW-024 SHEET 2 OF 27		

[illegible]

[illegible]

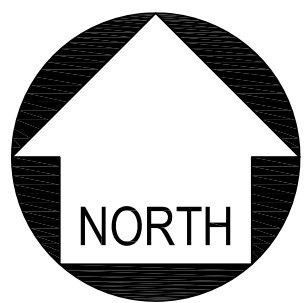
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PLANS	BY	DATE
SURVEY:	RichardH/JustinW	1/18/2016
DESIGNED:	JustinW	2/3/2016
DRAWN:	JustinW	3/17/2016
CHECKED:	JeffW	2016-05-06
PLOT BY:	JustinW	2016-05-06
FILE:	2016-05-03_AshStDesign.dwg	

PROJECT NO:	PW-024	SHEET	5	OF	27
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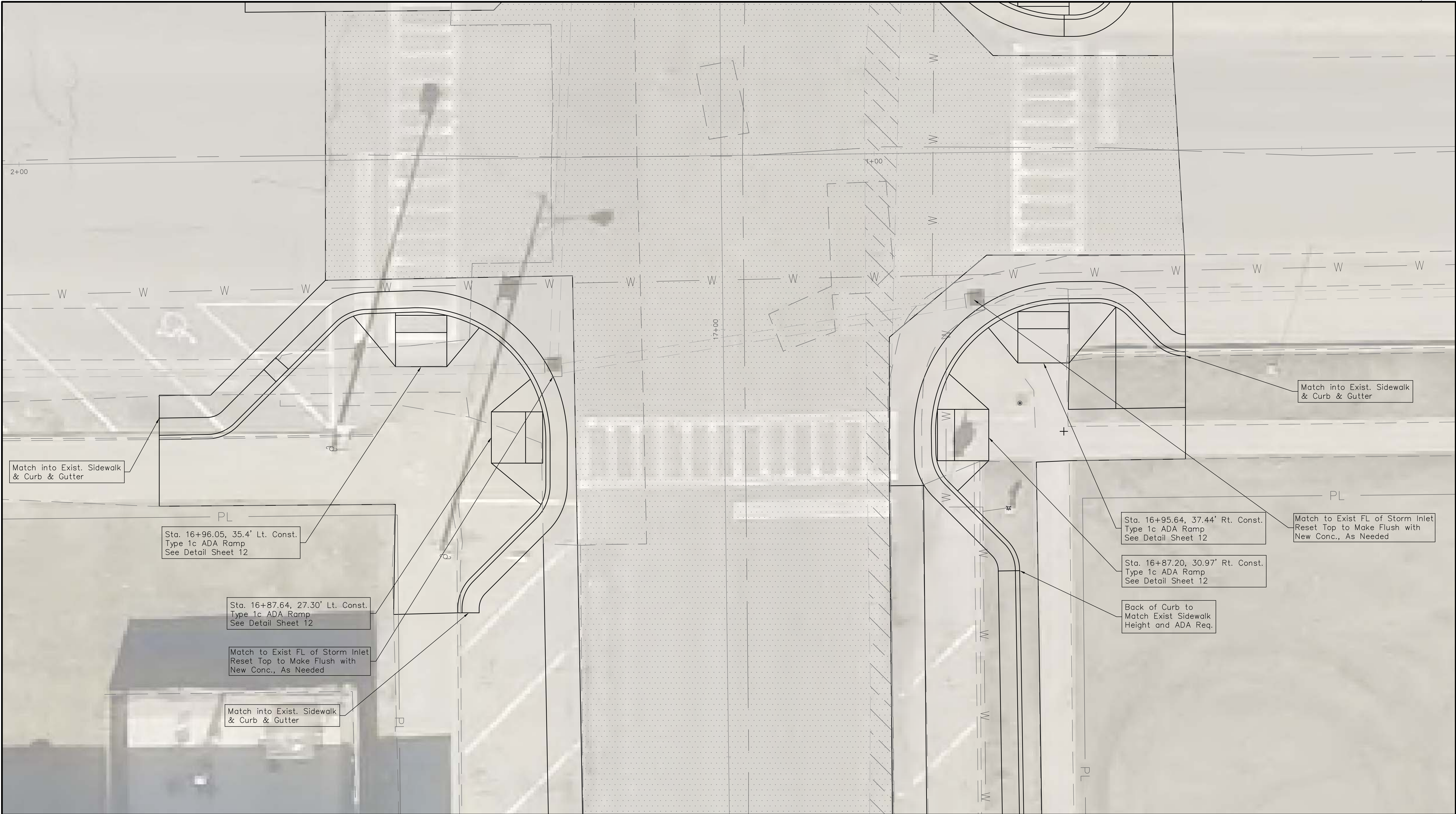
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FINAL PLANS
APPROVED FOR
CONSTRUCTION



PLANS	BY	DATE
SURVEY:	RichardH/JustinW	1/18/2016
DESIGNED:	JustinW	2/3/2016
DRAWN:	JustinW	3/17/2016
CHECKED:	JeffW	2016-05-06
PLOT BY:	JustinW	2016-05-06
FILE:	2016-05-03_AshStDesign.dwg	

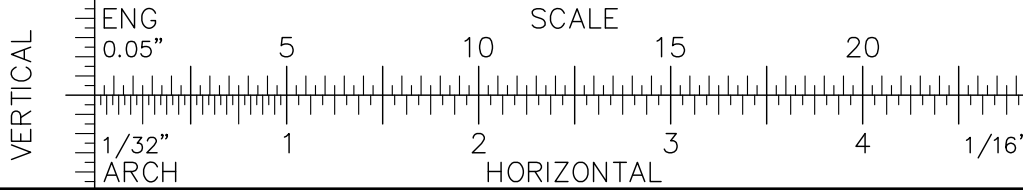
PROJECT NO:	PW-024	SHEET	6	OF	27
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Design File: I:\PublicWorks\PW_Projects\PW-024_2016_AshSt\AutoCAD\2016-05-03_AshStDesign.dwg

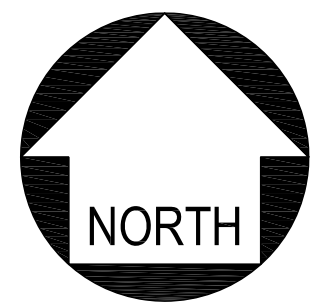
DATE	REVISIONS	BY	APP'D

Ash Street Partial Reconstruction
Ash St/Euclid St South Intersection Details



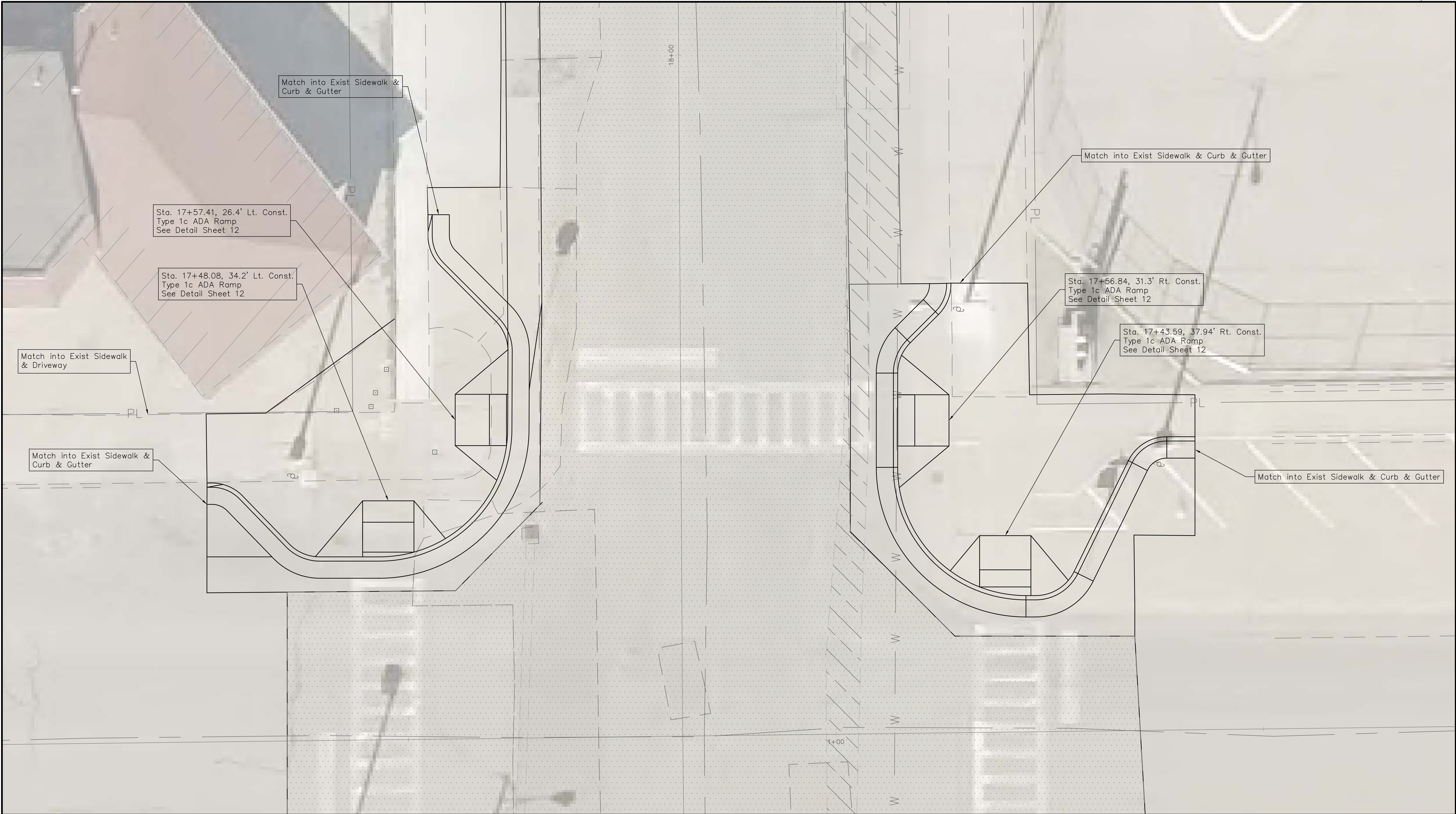
McPHERSON
CITY OF McPHERSON, KANSAS
PUBLIC WORKS DEPT.

FINAL PLANS
APPROVED FOR
CONSTRUCTION



PLANS	BY	DATE
SURVEY:	RichardH/JustinW	1/18/2016
DESIGNED:	JustinW	2/3/2016
DRAWN:	JustinW	3/17/2016
CHECKED:	JeffW	2016-05-06
PLOT BY:	JustinW	2016-05-06
FILE:	2016-05-03_AshStDesign.dwg	

PROJECT NO:	PW-024	SHEET	7	OF	27
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Design File: I:\PublicWorks\PW_Projects\PW-024_2016_AshSt\AutoCAD\2016-05-03_AshStDesign.dwg

DATE	REVISIONS	BY	APP'D

Ash Street Partial Reconstruction

Ash St/Euclid St North Intersection Details

VERTICAL

ENG 0.05"
ARCH 1 1/32"

SCALE
5 10 15 20
1 2 3 4 1/16"

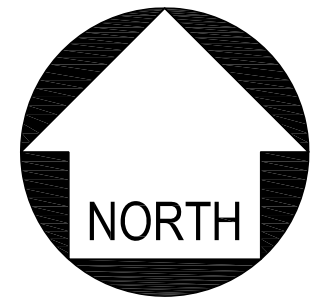
HORIZONTAL

MCPHERSON

CITY OF MCPHERSON, KANSAS
PUBLIC WORKS DEPT.

FINAL PLANS

APPROVED FOR
CONSTRUCTION

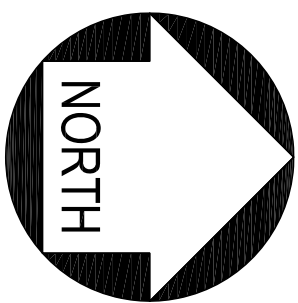


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DRAWN:	JustinW		3/17/2016
CHECKED:	JeffW		2016-05-06
PLOT BY:	JustinW		2016-05-06
FILE:	2016-05-03_AshStDesign.dwg		
PROJECT NO: PW-024		SHEET 8	OF 27

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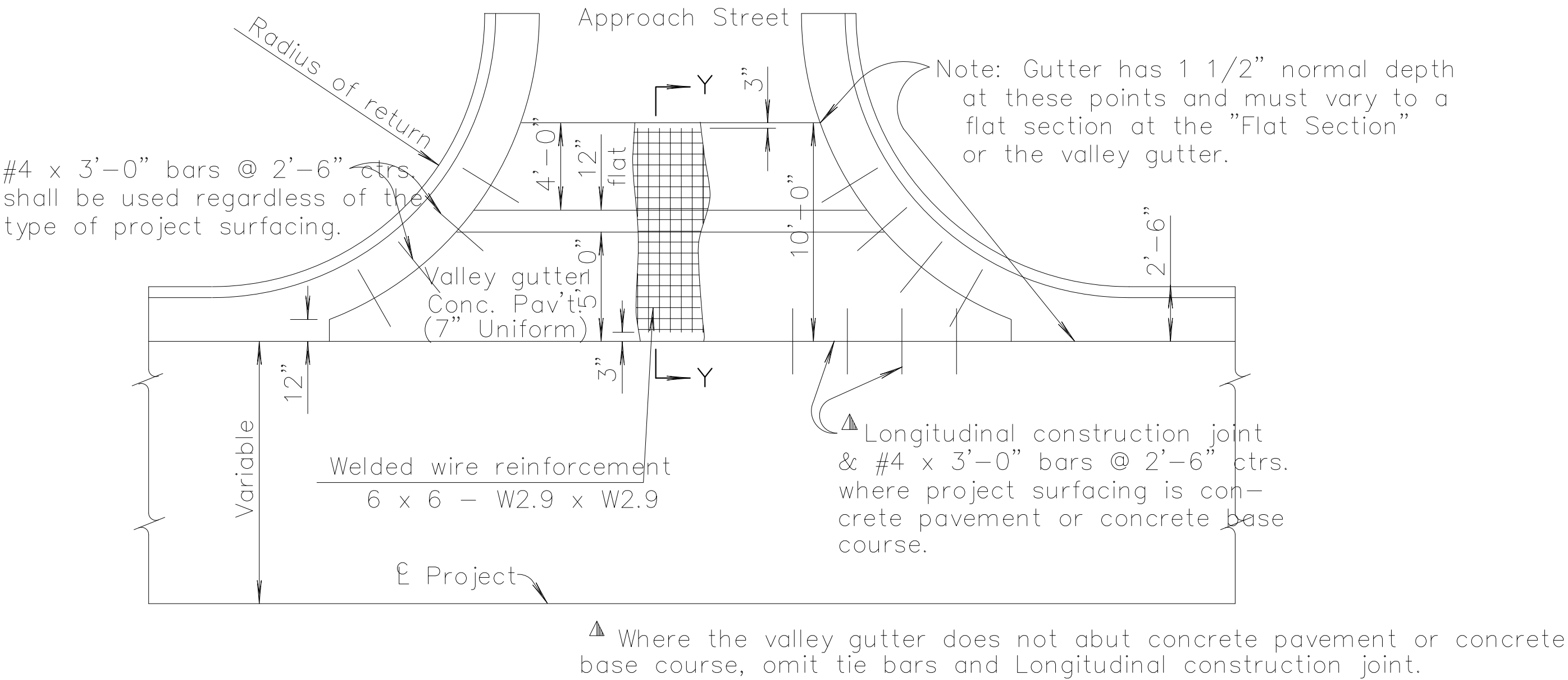
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FINAL PLANS
APPROVED FOR
CONSTRUCTION



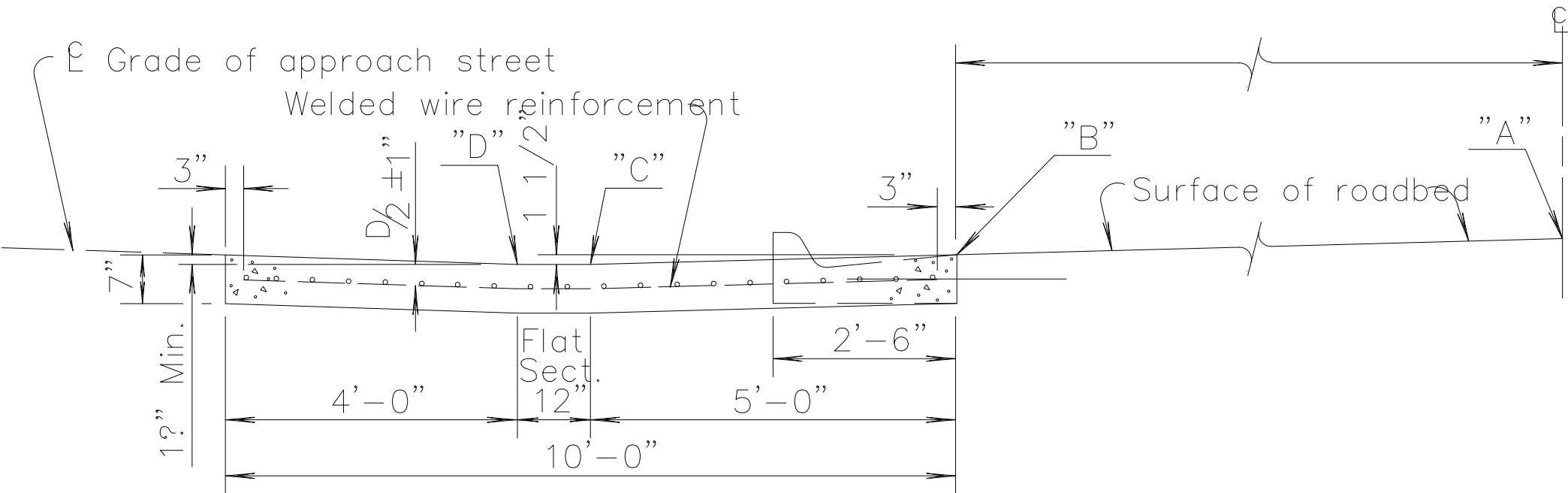
PLANS	BY	DATE
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DESIGNED:	JustinW	2/3/2016
DRAWN:	JustinW	3/17/2016
CHECKED:	JeffW	2016-05-06
PLOT BY:	JustinW	2016-05-06
FILE:	2016-05-03_AshStDesign.dwg	

PROJECT NO:	PW-024	SHEET	9	OF	27
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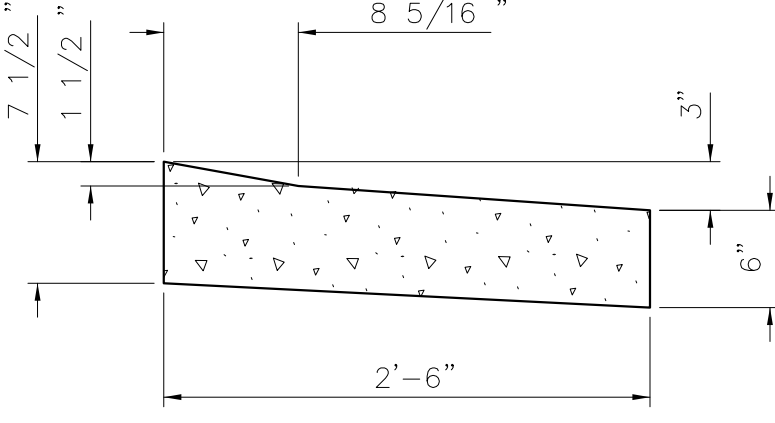
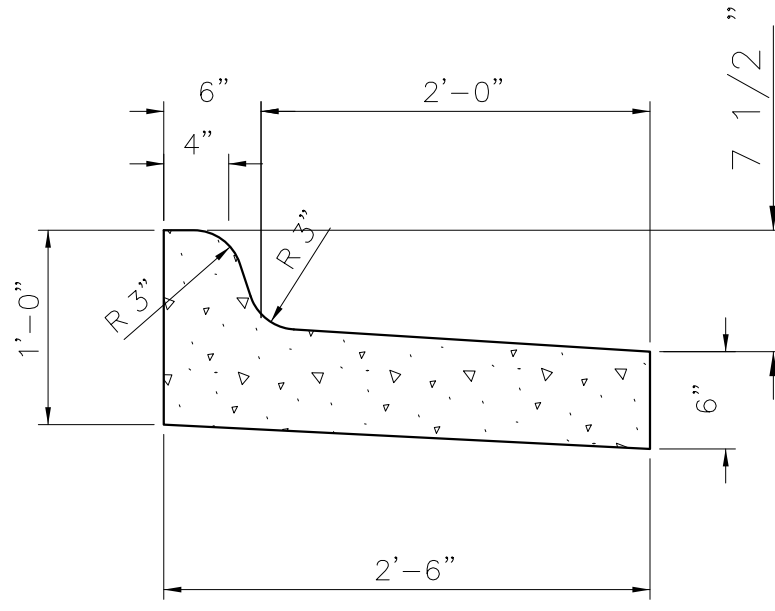
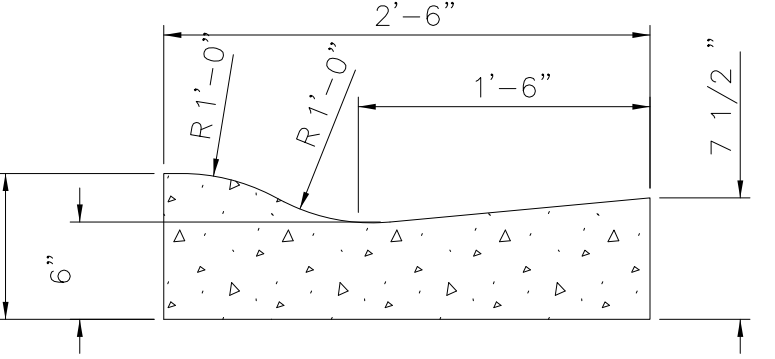
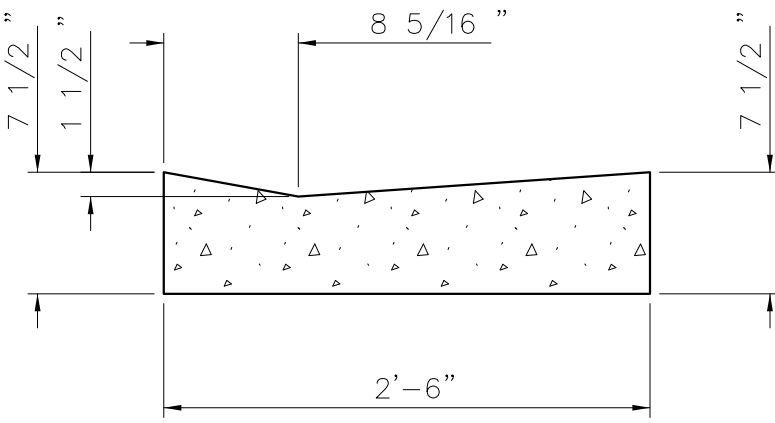
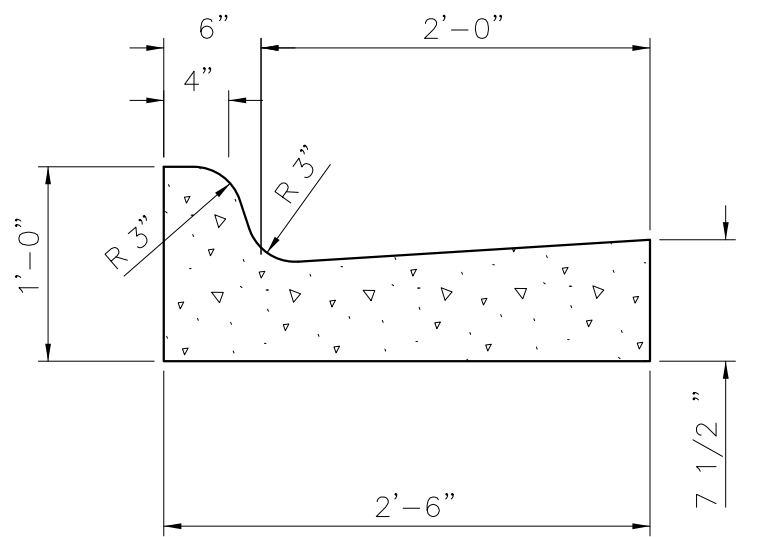
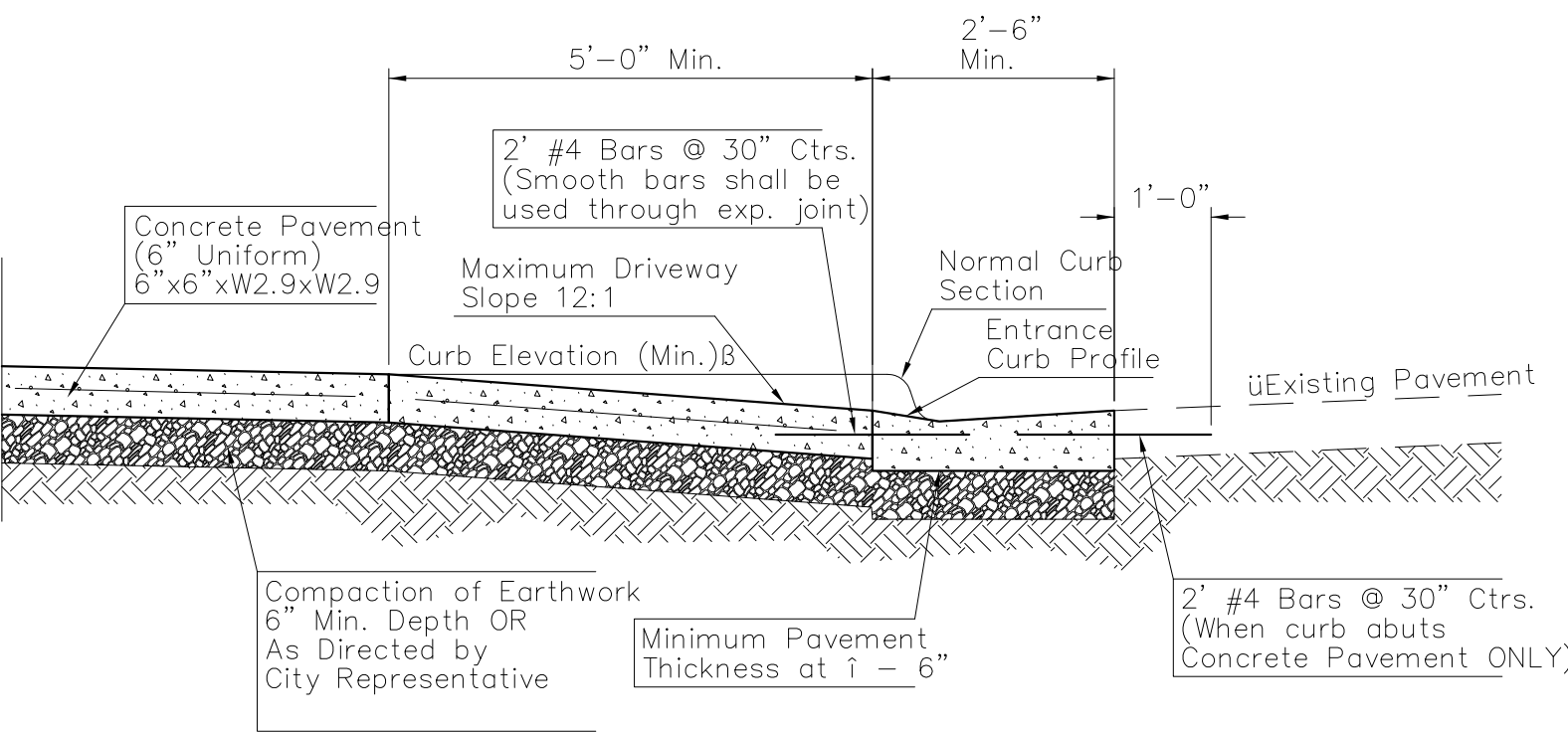
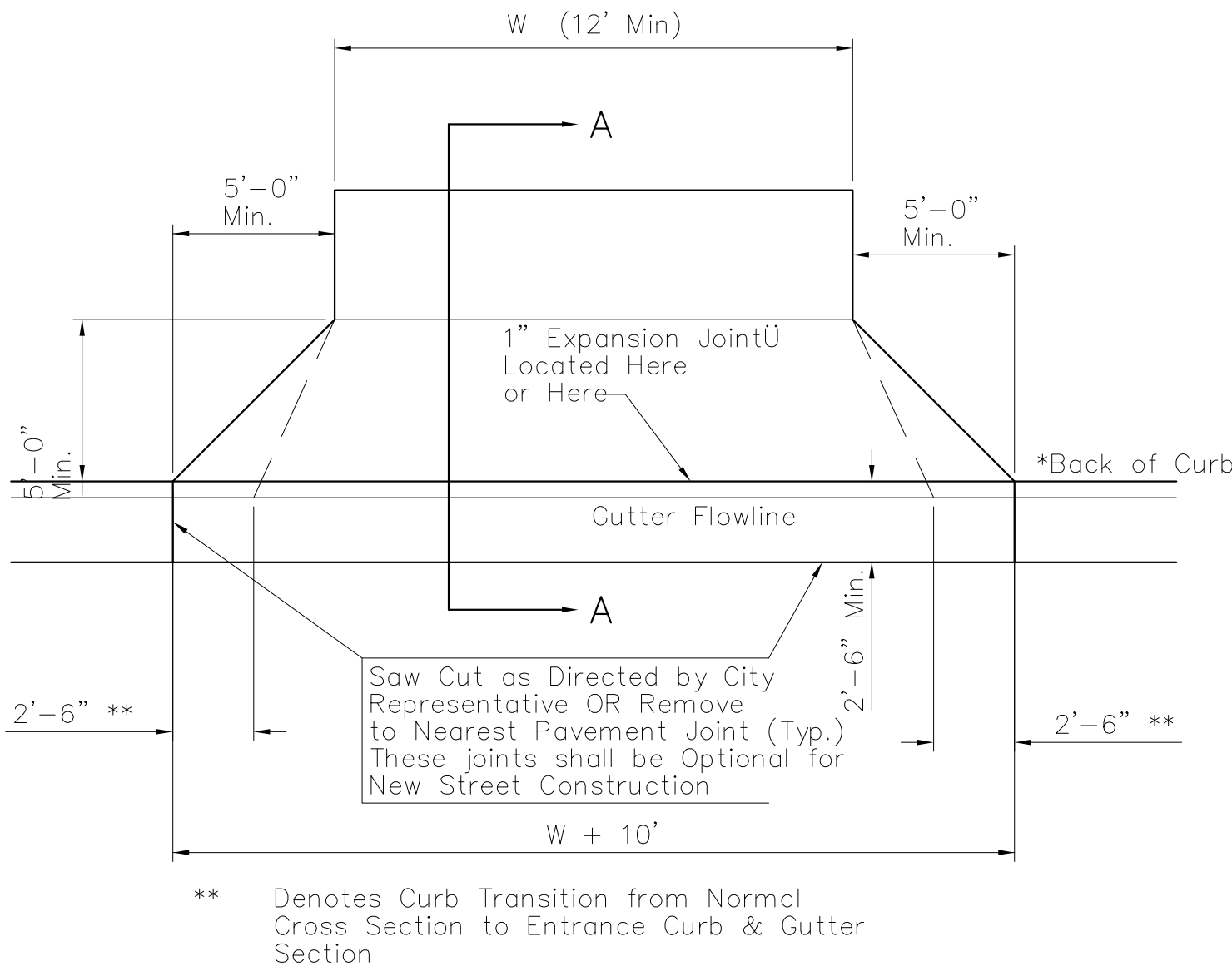
NOTE: Valley gutter concrete pavement shall be of 7" uniform thickness, with welded wire reinforcement. Approximate weight of welded wire reinforcement = 58 lbs. per 100 sq. ft. .

Where valley gutter, alley, and/or entrance pavement is the only pavement on the project, Concrete Grade 3.0 (AE) may be used.



VALLEY GUTTER SUMMARY							
Street	Station	Side	Elev. Pt. "A"	Elev. on $\bar{\imath}$ of Approach Str. Pt. "B" "C" & "D"	Appr. Str. Grade	Sq. Yds. Conc. Pvmt. (7" Uniform)	
		$\bar{\imath}$	--		VARIES		
		$\bar{\imath}$					
		$\bar{\imath}$					
		$\bar{\imath}$					
		$\bar{\imath}$					
		$\bar{\imath}$					
		$\bar{\imath}$					
		$\bar{\imath}$					
		$\bar{\imath}$					

VALLEY GUTTER



DATE	REVISIONS	BY	APP'D
2-18-2016	Added Minimum width to Entrances	JRW	JCW

PAVING DETAILS	
TYPICAL DRIVEWAY, VALLEY GUTTER, CURB	
ENG 0.05"	SCALE
1/32" ARCH	1 2 3 4 1/16"
VERTICAL	HORIZONTAL

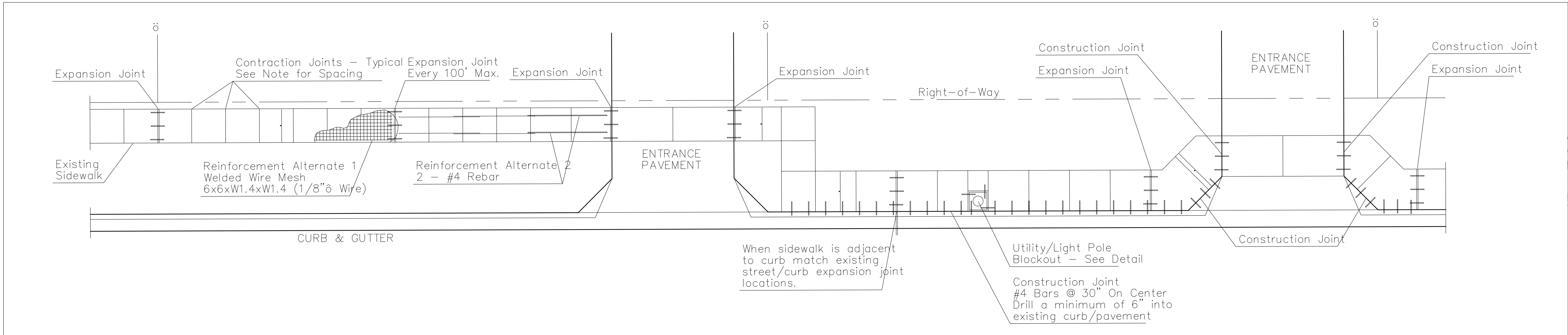
McPHERSON
CITY OF McPHERSON, KANSAS
PUBLIC WORKS DEPT.

FINAL PLANS
APPROVED FOR
CONSTRUCTION



PLANS		BY	DATE
SURVEY:			
DESIGNED:	RichardH		2/08/2015
DRAWN:	JustinW		2/18/2016
CHECKED:	JeffW		2/18/2016
PLOT BY:	JustinW		2016-05-06
FILE:	PavingDetails.dwg		
PROJECT NO:	PW-024	SHEET	10 OF 27

Design File: I:\PublicWorks\Standards\Sidewalk\SidewalkStandardDetails.dwg

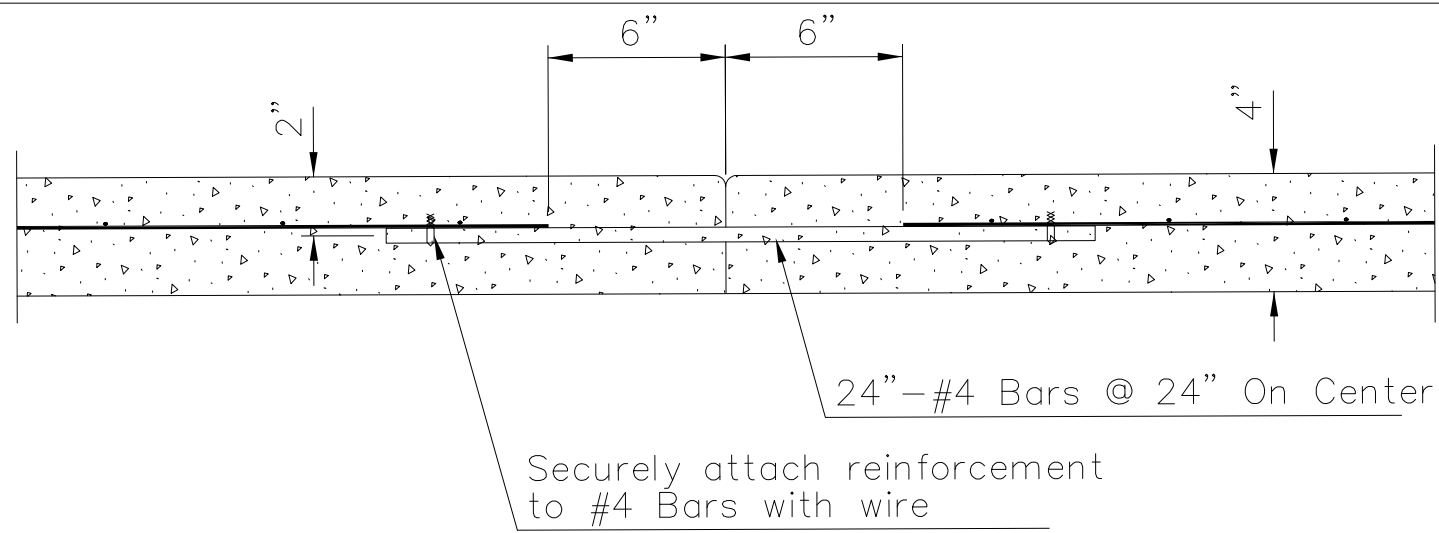


GENERAL NOTES

MATERIALS
Concrete used for sidewalk construction shall be "City Paving Mix", 6 Sack, 900#-1" Rock (FA)(AE) unless otherwise specified on the plans or by the Engineer.

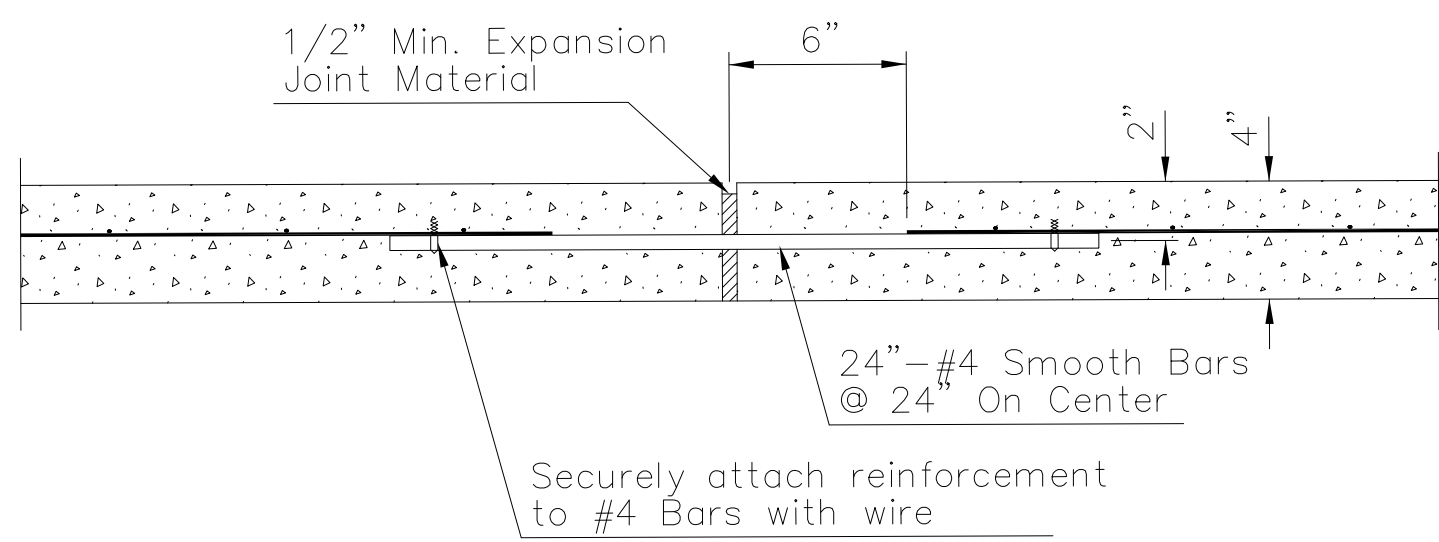
Welded wire reinforcement, if used, shall have a six inch (6") square pattern with 1/8" diameter wire or greater (6"x6"xWV.4xW1.4, 14# per 100 Sq. Ft.).

CONSTRUCTION
Excavation should be made as close to the desired lines and grades as possible so that concrete may be placed on undisturbed and compacted soil. Filler materials such as pea gravel or sand may be used to fill voids or low areas. Filler materials should be placed in lifts and compacted using vibratory equipment to remove any voids. All deleterious materials (roots, trash, etc.) should be removed from the base prior to placement of filler material or concrete.



CONSTRUCTION JOINT

Construction Joints shall be constructed at locations shown on the plan or as directed by the Engineer. Construction joints transfer movement between adjacent sections of concrete and prevent the concrete surfaces from shifting.



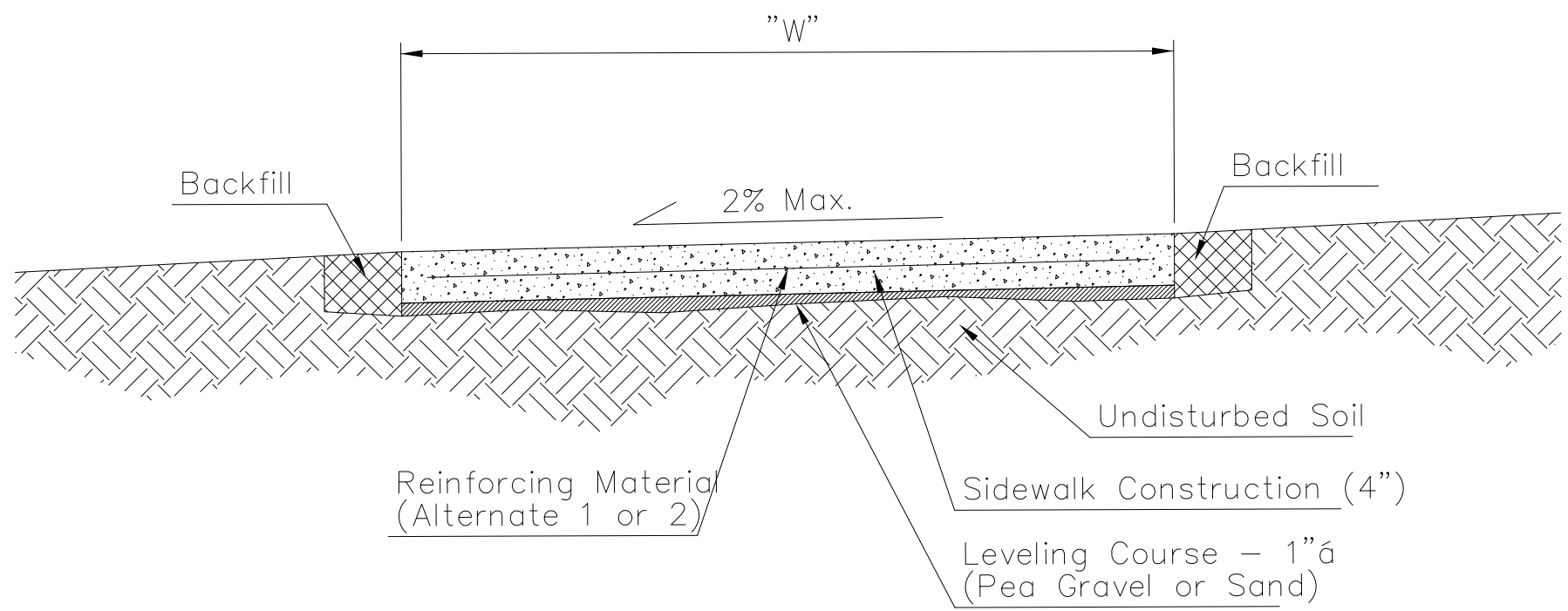
EXPANSION JOINT

Expansion Joints shall be constructed at locations shown on the plan or as directed by the Engineer. Expansion joints isolate movement between adjacent sections of concrete as the concrete expands with increases in temperature.

Expansion joint material may be foam, cedar or redwood board, cut to match the thickness of the finished concrete.

Reinforcing steel, or wire mesh should be cut 6" from the expansion joint to completely isolate the joint. Caps and grease are not required for sidewalk expansion joints.

TYPICAL SIDEWALK PLAN

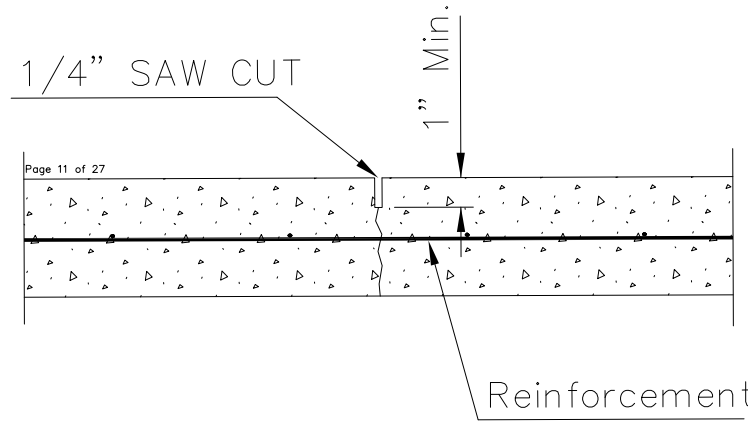


TYPICAL SECTION

Sidewalk width "W" shall match the existing sidewalk width for all maintenance work. For New Construction, minimum sidewalk width "W" shall be 5'-0" for sidewalk adjacent to the right-of-way and 6'-0" for sidewalk adjacent to the curb or street.

Sidewalks constructed shall conform to the latest edition of the Americans with Disabilities Act Accessibility Guidelines, ADAAG, as required by the Department of Justice.

Any features adjacent to the planned construction that are not compliant with the ADAAG guidelines MUST be brought into compliance unless the cost of the additional construction is more than 20% of the TOTAL cost of the project.



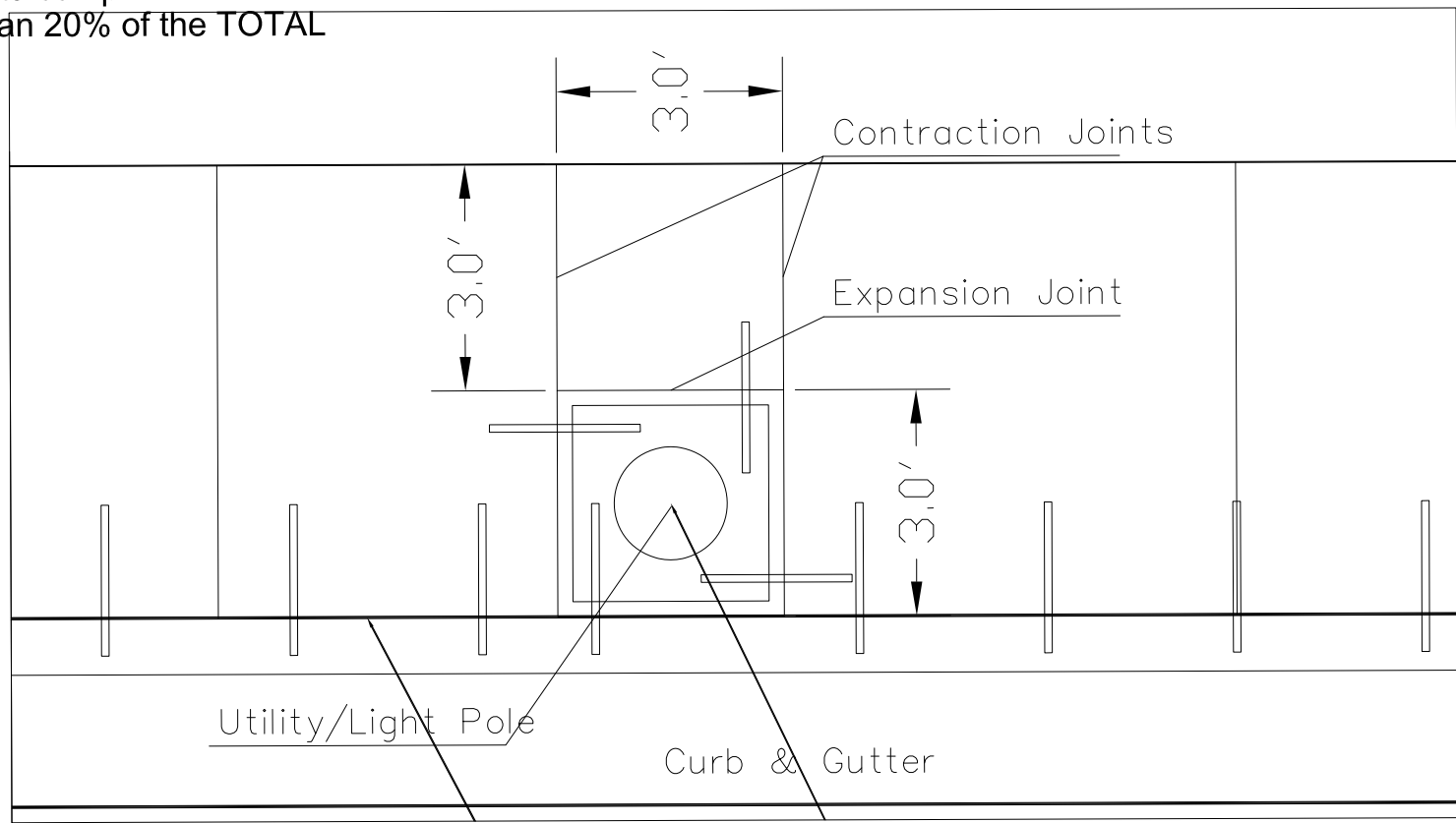
CONTRACTION JOINT

Contraction joints shall be placed in the sidewalk at intervals equal to the width of the sidewalk up to a maximum spacing of seven feet (7'). For example, with a four foot wide sidewalk, joints should be spaced on average every 4 feet.

Contraction joints may be formed in the concrete with a center edger of sufficient depth. Saw cutting may still be required if tooled joints "close up" as the concrete hardens.



Saw cut joints should be made as soon as the concrete is hard enough to prevent spalling and before shrinkage occurs. Typically saw cut joints should be made the same day as the concrete is placed unless otherwise directed by the Engineer.

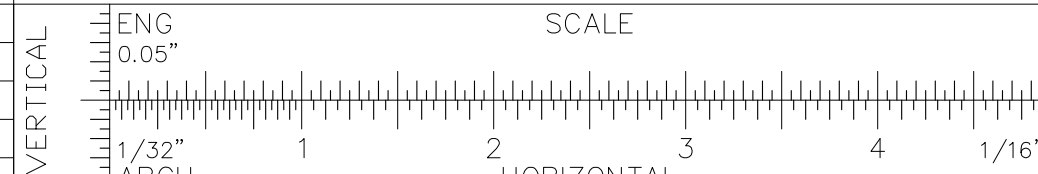
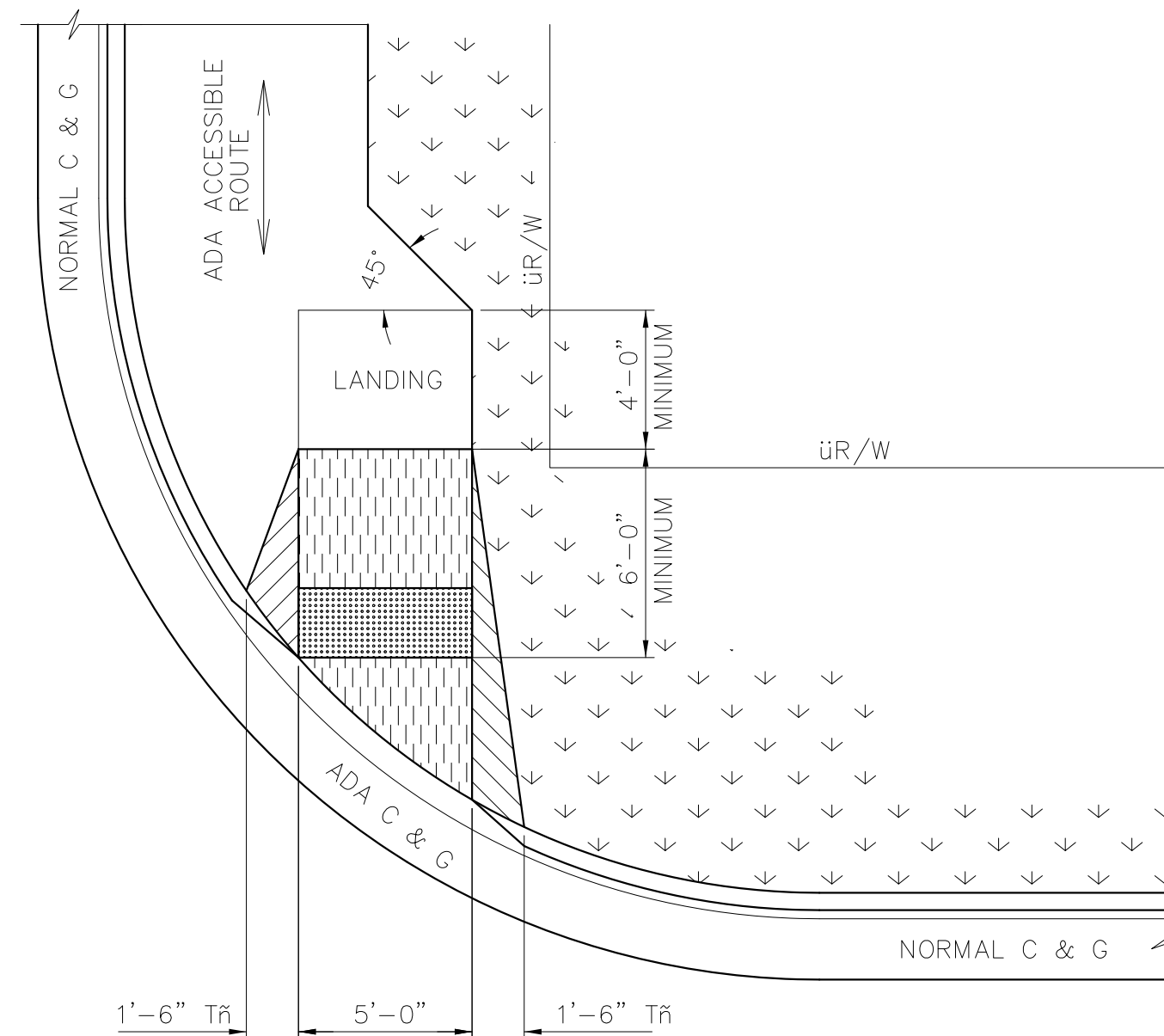
All reinforcing materials should extend through the contraction joint.



UTILITY/LIGHT POLE BLOCKOUT DETAIL

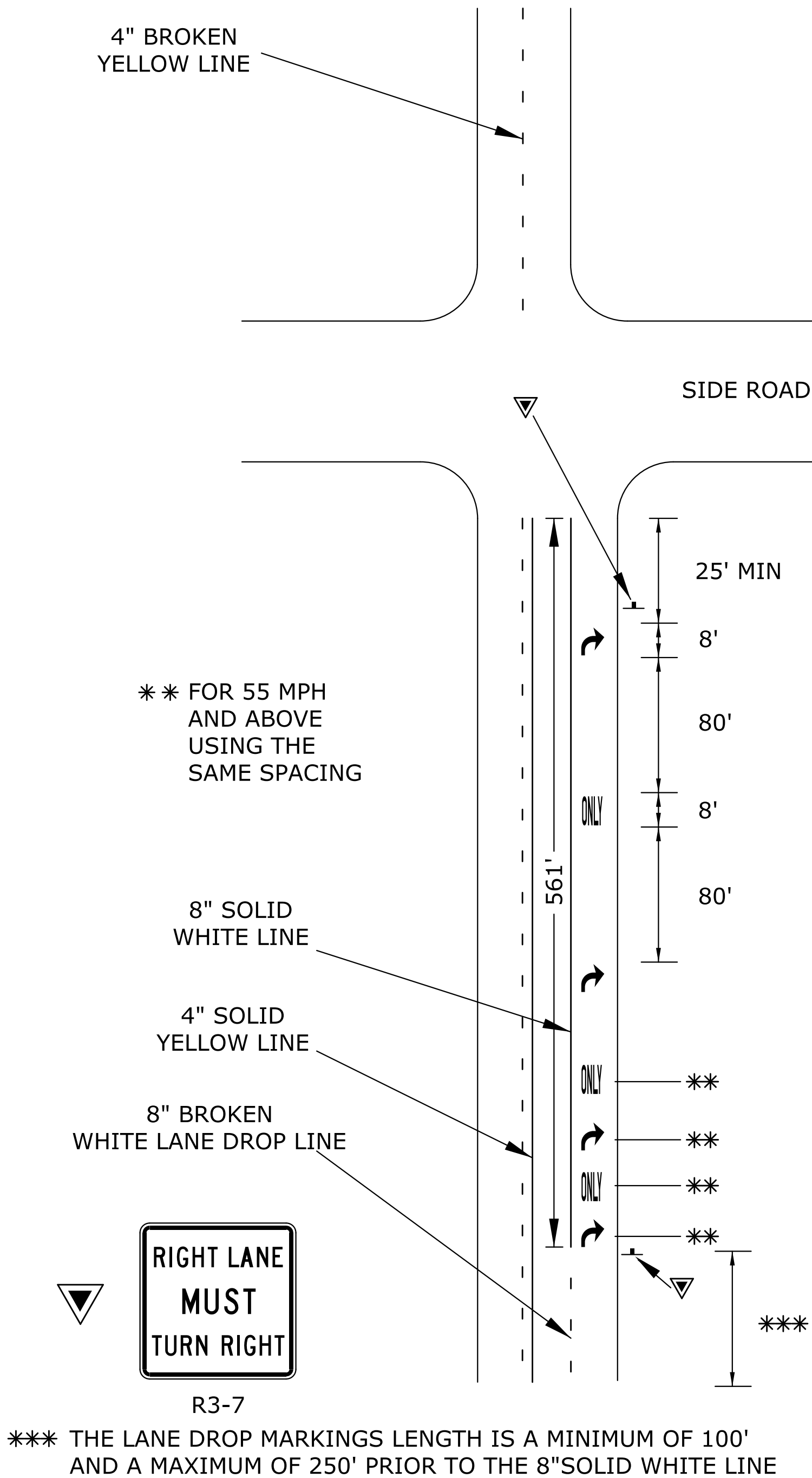
3'-0" Min. is required for ADA access.

Design File: I:\PublicWorks\Standards\Sidewalk\SidewalkStandardDetails.dwg	DATE				REVISIONS				BY				APP'D						
Sidewalk Details												McPHERSON							
Details for the Typical Construction of Sidewalk												CITY OF McPHERSON, KANSAS PUBLIC WORKS DEPT.							
SCALE												FINAL PLANS APPROVED FOR CONSTRUCTION							
																			



STATE	PROJECT NO.	YEAR	SHEET NO.	TOTAL SHEETS
KANSAS	PW-024	2016	13	27

TYPICAL SIGNING AND MARKING
FOR RIGHT LANE MUST TURN RIGHT



RAILROAD CROSSING MARKING

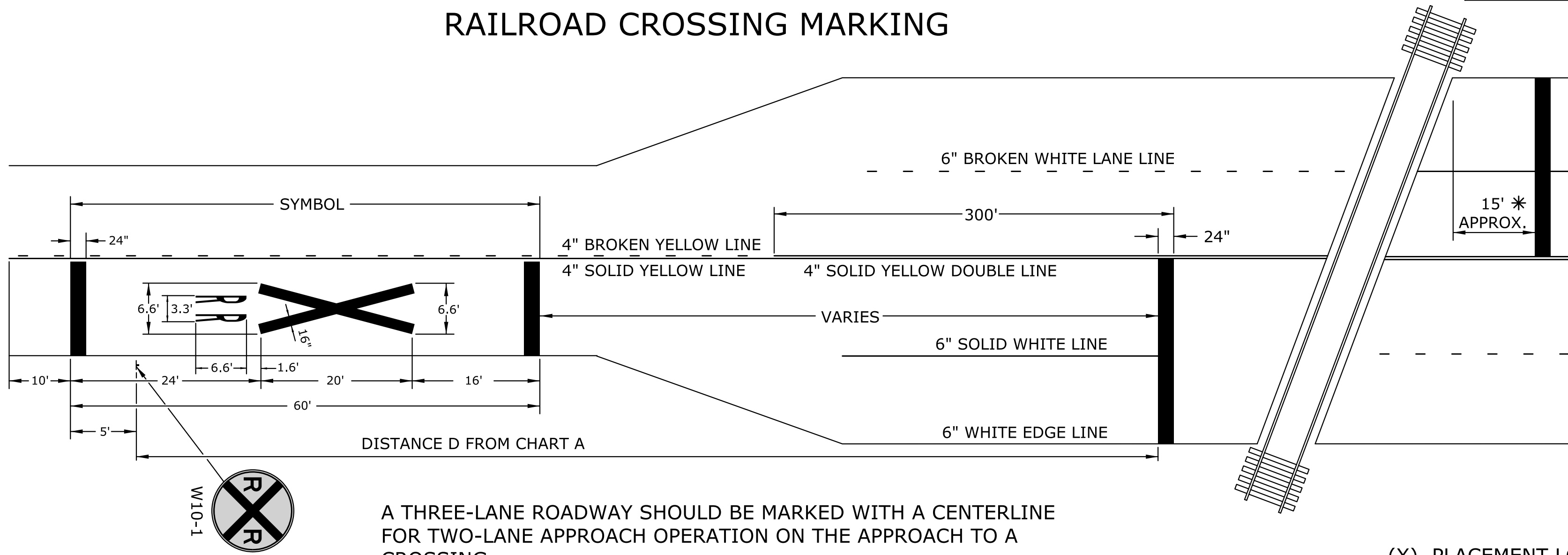


CHART "A"

SPEED MPH	DISTANCE D (feet)
75	850
70	750
65	650
60	550
55	450
50	375
45	300
40	225
35	150
30	(X)
25	(X)
20	(X)

ALL DISTANCES ARE MINIMUM.

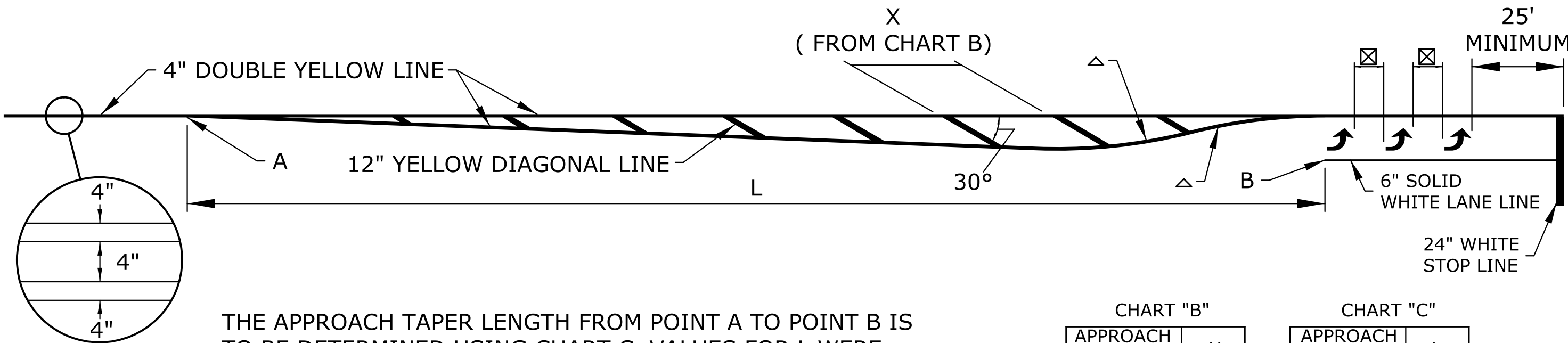
(X) PLACEMENT LOCATION IS DEPENDENT ON SITE CONDITIONS AND OTHER SIGNING TO PROVIDE ADEQUATE ADVANCE WARNING TO THE DRIVER

A THREE-LANE ROADWAY SHOULD BE MARKED WITH A CENTERLINE FOR TWO-LANE APPROACH OPERATION ON THE APPROACH TO A CROSSING. ON MULTI-LANE ROADS THE TRANSVERSE BANDS SHOULD EXTEND ACROSS ALL APPROACH LANES, AND INDIVIDUAL R X R SYMBOLS SHOULD BE USED IN EACH APPROACH LANE. REFER TO STANDARD ALPHABET FOR HIGHWAY SIGNS AND MARKINGS FOR R X R SYMBOLS DETAILS.

*STOP LINE 8' FROM NEAR EDGE OF GATE OR CANTILEVER, IF PRESENT.

NOTE:
ON NON I, US, AND K ROUTES, 4" EDGE LINES MAY BE INSTALLED.
6" EDGE LINES ARE NOT REQUIRED ON NON I, US, AND K ROUTES.

TYPICAL
APPROACH TAPER DETAIL



THE APPROACH TAPER LENGTH FROM POINT A TO POINT B IS TO BE DETERMINED USING CHART C. VALUES FOR L WERE CALCULATED USING THE EQUATIONS BELOW AND INCREASED TO THE NEXT HIGHER 5 MPH INCREMENT.

- SPEEDS < 45 MPH $L = \frac{W \cdot S^2}{60}$

- SPEEDS = 45 MPH $L = W \cdot S$

IF ARROWS ARE USED AND UNLESS OTHERWISE SPECIFIED THE SPACE BETWEEN LINES SHOULD BE AT LEAST FOUR TIMES THE HEIGHT OF THE CHARACTERS FOR LOW SPEED ROADS BUT NOT MORE THAN TEN TIMES THE HEIGHT OF THE CHARACTERS, UNDER ANY CONDITIONS.

FOR SPEEDS LESS THAN OR EQUAL TO 40 MPH, R=150'.
FOR SPEEDS GREATER THAN OR EQUAL TO 45 MPH, R=300'.

CHART "B"

APPROACH SPEED	X
20 MPH	20'
25 MPH	25'
30 MPH	30'
35 MPH	35'
40 MPH	40'
45 MPH	45'
50 MPH	50'
55 MPH	55'
60 MPH	60'
65 MPH	65'
70 MPH	70'

CHART "C"

APPROACH SPEED	L
20 MPH	80'
25 MPH	125'
30 MPH	180'
35 MPH	245'
40 MPH	320'
45 MPH	540'
50 MPH	600'
55 MPH	660'
60 MPH	720'
65 MPH	780'
70 MPH	840'

3	5/25/12	Updated Chart B and Lane Drop Lines	B.A.H.	B.D.G.
2	10/20/06	RR Xing Symbol Changed from 18" to 16"	T.L.H.	B.D.G.
1	9/20/05	Added 4" Solid Yellow Double Line to RR Xing	J.F.F.	B.D.G.
NO.	DATE	REVISIONS	BY	APP'D

KANSAS DEPARTMENT OF TRANSPORTATION				
TYPICAL MISCELLANEOUS PAVEMENT MARKING DETAIL SHEET				
TE309				
FHWA APPROVAL	7/26/2005	APP'D	Bryan D. Gower	
DESIGNED	J.F.F.	DETAILED	J.F.F.	QUANTITIES
DESIGN CK.	B.D.G.	DETAIL CK.	B.D.G.	QUAN. CK.
			TRACED	TRACE CK.

KDOT Graphics Certified 06-26-2012

Sh. No. 13

Plotted : 26-JUN-2012 08:41

Drawn By : connie
File : te309.dgn (te309)

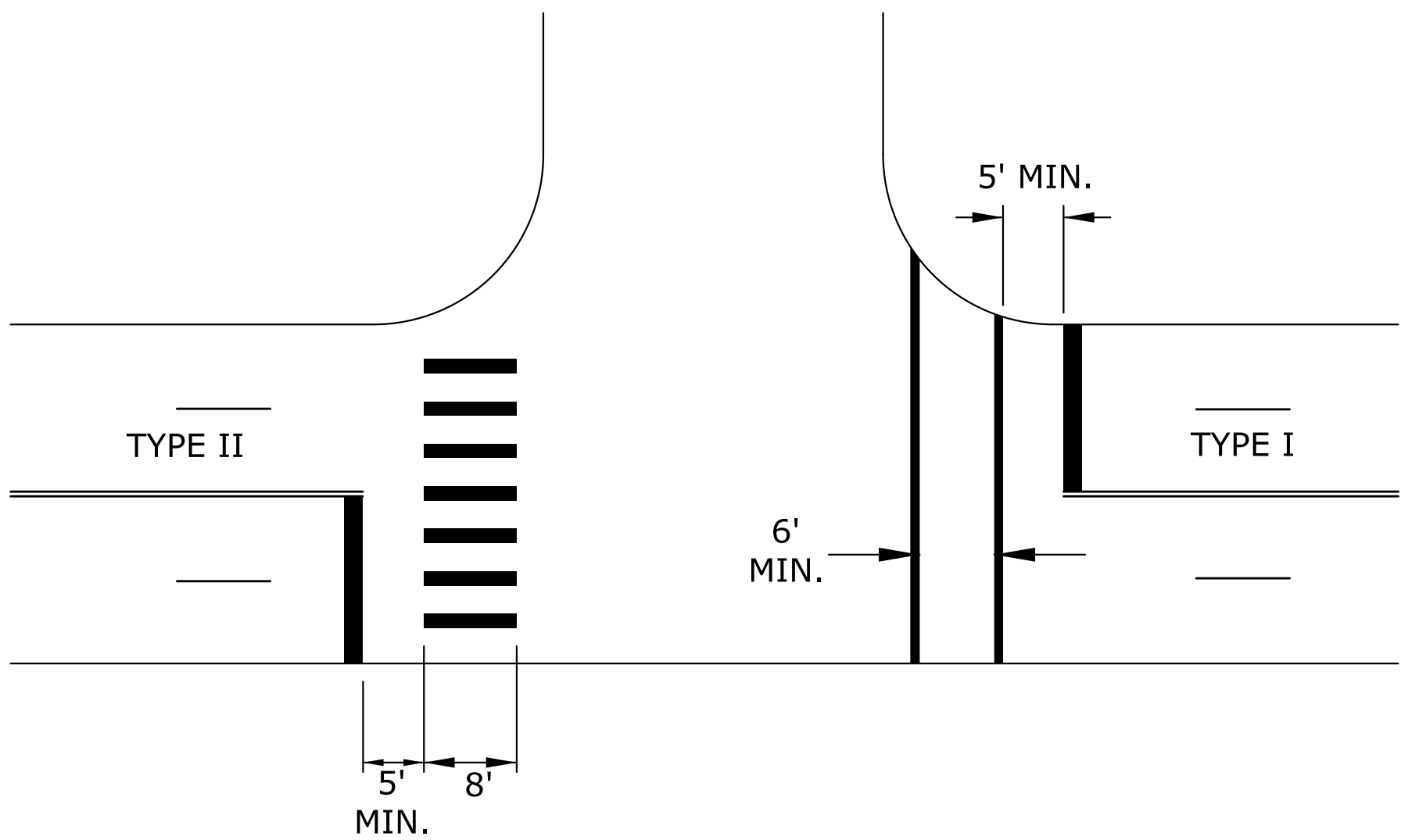
Traffic

TYPICAL CROSSWALKS

TYPE I: CROSSWALK LINES SHALL BE 12" SOLID WHITE LINES. THEY SHALL BE SPACED A MINIMUM OF 6' APART FROM INSIDE EDGE TO INSIDE EDGE.

TYPE II: THESE LINES SHOULD BE SOLID WHITE 24" WIDE PLACED PARALLEL TO THE DIRECTION OF TRAFFIC FLOW. THE LINE PLACEMENT IS DETERMINED BY LANE LINE, CENTER LINE, AND WHEEL PATH IN SUCH A MANNER AS TO MINIMIZE TRAFFIC WEAR. THE CROSSWALK WIDTH SHOULD BE NOT LESS THAN 8'. THE TRANSVERSE CROSSWALK LINES MAY BE ADDED.

WHEN REQUIRED, STOP LINES SHALL BE INSTALLED A MINIMUM OF 5' FROM CROSSWALKS.



STATE	PROJECT NO.	YEAR	SHEET NO.	TOTAL SHEETS
KANSAS	PW-024	2016	14	27

SUMMARY OF PAVEMENT MARKINGS

[illegible]

RECAPITULATION OF QUANTITIES

ITEMS	TOTAL	UNITS
PAVEMENT MARKING (MULTI-COMPONENT)(WHITE)(4")		FT
PAVEMENT MARKING (MULTI-COMPONENT)(WHITE)(6")		FT
PAVEMENT MARKING (MULTI-COMPONENT)(WHITE)(8")		FT
PAVEMENT MARKING (MULTI-COMPONENT)(WHITE)(12")		FT
PAVEMENT MARKING (MULTI-COMPONENT)(YELLOW)(4")		FT
PAVEMENT MARKING (MULTI-COMPONENT)(YELLOW)(6")		FT
PAVEMENT MARKING (MULTI-COMPONENT)(YELLOW)(12")		FT
PAVEMENT MARKING (THERMOPLASTIC)(WHITE)(4")		FT
PAVEMENT MARKING (THERMOPLASTIC)(WHITE)(6")		FT
PAVEMENT MARKING (THERMOPLASTIC)(WHITE)(8")		FT
PAVEMENT MARKING (THERMOPLASTIC)(WHITE)(12")		FT
PAVEMENT MARKING (THERMOPLASTIC)(YELLOW)(4")		FT
PAVEMENT MARKING (THERMOPLASTIC)(YELLOW)(6")		FT
PAVEMENT MARKING (THERMOPLASTIC)(YELLOW)(12")		FT
PAVEMENT MARKING (EPOXY)(WHITE)(4")		FT
PAVEMENT MARKING (EPOXY)(WHITE)(6")	2620	FT
PAVEMENT MARKING (EPOXY)(WHITE)(8")		FT
PAVEMENT MARKING (EPOXY)(WHITE)(12")		FT
PAVEMENT MARKING (EPOXY)(YELLOW)(4")		FT
PAVEMENT MARKING (EPOXY)(YELLOW)(6")		FT
PAVEMENT MARKING (EPOXY)(YELLOW)(12")		FT
PAVEMENT MARKING (INTERSECTION GRADE)(WHITE)(12")	622	FT
PAVEMENT MARKING (INTERSECTION GRADE)(WHITE)(24")	144	FT
PAVEMENT MARKING (INTERSECTION GRADE)(YELLOW)(12")		FT
PAVEMENT MARKING SYMBOL (INTERSECTION GRADE)(WHITE)(Railroad X)	1	EACH
PAVEMENT MARKING SYMBOL (INTERSECTION GRADE)(WHITE)()		EACH
PAVEMENT MARKING SYMBOL (INTERSECTION GRADE)(WHITE)()		EACH
PAVEMENT MARKING SYMBOL (INTERSECTION GRADE)(WHITE)()		EACH
PAVEMENT MARKING SYMBOL (INTERSECTION GRADE)(WHITE)()		EACH
PAVEMENT MARKING SYMBOL (INTERSECTION GRADE)(US-SHIELD)()		EACH
PAVEMENT MARKING SYMBOL (INTERSECTION GRADE)(K-SHIELD)()		EACH
PAVEMENT MARKING SYMBOL (INTERSECTION GRADE)(I-SHIELD)()		EACH
PAVEMENT MARKING (PATTERNED COLD PLASTIC)(WHITE)(6")		FT
PAVEMENT MARKING (PATTERNED COLD PLASTIC)(WHITE)(8")		FT
PAVEMENT MARKING (PATTERNED COLD PLASTIC)(WHITE)(12")		FT
PAVEMENT MARKING REMOVAL		FT

SUMMARY OF WORD & SYMBOL MARKINGS

[illegible]

NOTE:
WORDS & SYMBOLS SHALL CONFORM TO THE LATEST EDITION OF
"STANDARD ALPHABETS FOR HIGHWAY SIGNS AND PAVEMENT
MARKINGS" PRINTED BY THE U.S. DEPARTMENT OF TRANSPORTATION,
FEDERAL HIGHWAY ADMINISTRATION.

PRIOR TO COMMENCEMENT OF PAVEMENT MARKING WORK THE ENGINEER WILL ESTABLISH THE LIMITS FOR "NO PASSING" ZONES. THESE LIMITS SHALL BE USED FOR THE LOCATION OF "NO PASSING" LINES AND FOR THE COMPUTATION OF ACTUAL MARKING QUANTITIES FOR THIS LINE TYPE.

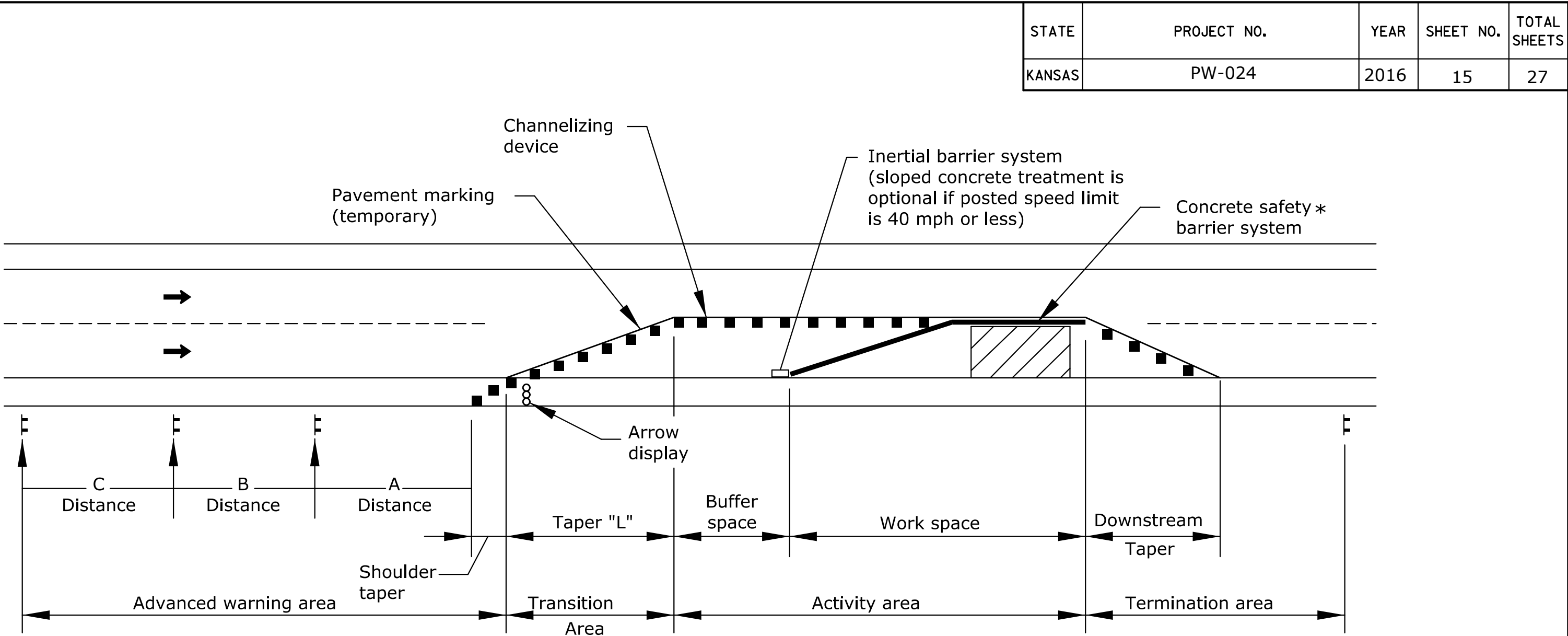
NOTE: FOR SPECIFIC PAVEMENT MARKING DETAILS AND DIMENSIONS SEE PLAN SHEETS

NOTE: ALL TOTALS REFLECT ACTUAL QUANTITY OF PAVEMENT MARKING MATERIALS REQUIRED.

2	5/25/12	Added Line Types, Symbols, and Shields						B.A.H.	B.D.G.
1	7/26/05	New FHWA Approval Date						J.F.F.	B.D.G.
NO.	DATE	REVISIONS						BY	APP'D
<p style="text-align:center;">KANSAS DEPARTMENT OF TRANSPORTATION SUMMARY AND RECAPITULATION OF PAVEMENT MARKING QUANTITIES</p>									
TE3II									
FHWA APPROVAL		5/25/2012		APP'D		Brian D. Gower			
DESIGNED	J.F.F.	DETAILED	J.F.F.	QUANTITIES		TRACED			
DESIGN CK.	B.D.G.	DETAIL CK.	B.D.G.	QUAN. CK.		TRACE CK.			

Drawn By : mushock
File : te700.dgn
Plotted : 18-AUG-2015 14:02
Traffic

- 1) Design Speed: Those items delegated to temporary traffic control should be designed and installed using the posted/legal speed of the roadway prior to work starting.
- 2) Minimum lane width: Lane widths shall be a minimum of 11' (measured between centerlines of pavement markings) or as shown on the plans, or as directed by the engineer. A lane width less than 11' may require restricted roadway width signing.
- 3) Consideration should be made to seperate pedestrian and, if needed, bicycle movements from both work site activity and vehicular traffic. Unless a reasonable safe route that does not involve crossing the roadway can be provided, pedestrians should be appropriately directed with advance signing that encourages them to cross to the opposite side of the roadway. In urban and suburban areas with high vehicular traffic volumes, these signs should be placed at intersections (rather than midblock locations) so that pedestrians are not confronted with midblock work sites that will induce them to attempt skirting the work site or making a midblock crossing.
- 4) When existing pedestrian facilities are disrupted, closed, or relocated, the temporary facilities shall be detectable and include accessibility features consistent with the features present in the existing pedestrian facility.
- 5) When the driving surface open to traffic is milled, is a temporary surface made of loose material, or when directed by the engineer use the W8-15 (Grooved Pavement) or W8-7(Loose Gravel) a "C" distance after the W20-1 (Road Work Ahead) on mainline approaches. Signs may be used with the W8-15p motorcycle plaque as directed by the engineer. Display signs in advance of the condition as long as the condition is present.
- 6) Alternative temporary rumble strip options may be available. Please contact the Temporary Traffic Control Unit for more information at 785-296-0355 or 785-296-1183.



TYPICAL WORK ZONE COMPONENTS

* When concrete barrier system is used, portable channelizing devices are not needed along the tangent barrier section.

Minimum advance warning sign spacing (in feet):

SPEED (MPH) *	A	B	C
URBAN (40 MPH OR LOWER)	100	100	100
URBAN (45 MPH OR HIGHER)	350	350	350
RURAL (55 MPH OR LOWER)	500	500	500
RURAL (60 MPH OR HIGHER)	750	750	750
EXPRESSWAY/FREEWAY	1000	1500	2640

* Posted speed prior to work starting
The minimum spacing between signs shall be no less than 100', unless directed by the engineer.
The spacing between any signs may be increased beyond the minimum values in the table above as approved by the engineer in order to maximize visibility.

Taper Formulas:

L = WS for speeds of 45 MPH or more

L = WS²/60 for speeds of 40 MPH or less

Where: L =Minimum length of taper in feet
S =Numerical value of posted speed prior to work starting in MPH
W =Width in offset feet

Shifting taper=1/2 L
Shoulder taper=1/3 L

Channelizer placement:

- (1) The spacing between devices in transition area (taper) should not exceed a distance in feet equal to 1/2 the posted speed limit in mph prior to work starting.
- (2) The spacing between devices in the advanced warning area and the activity area should not exceed a distance in feet equal to two times the posted speed limit in mph prior to work starting.
- (3) Channelizing devices shall be placed for optimum visibility, normally at right angles to the traffic flow.
- (4) Place directional indicator barricades in series to direct traffic onto the new path. The arrow sign should not be visible to opposing traffic.
- (5) Alternating diagonal orange and white striping must slope downward in the direction traffic is expected to pass.

Buffer Space

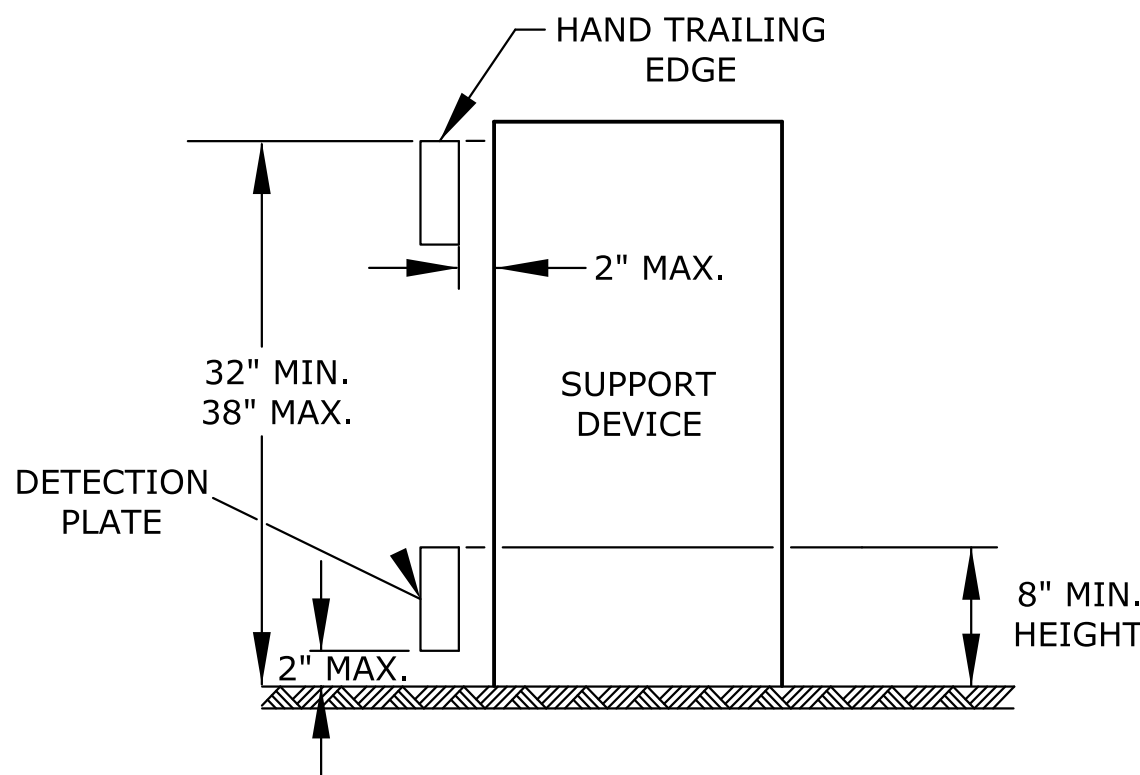
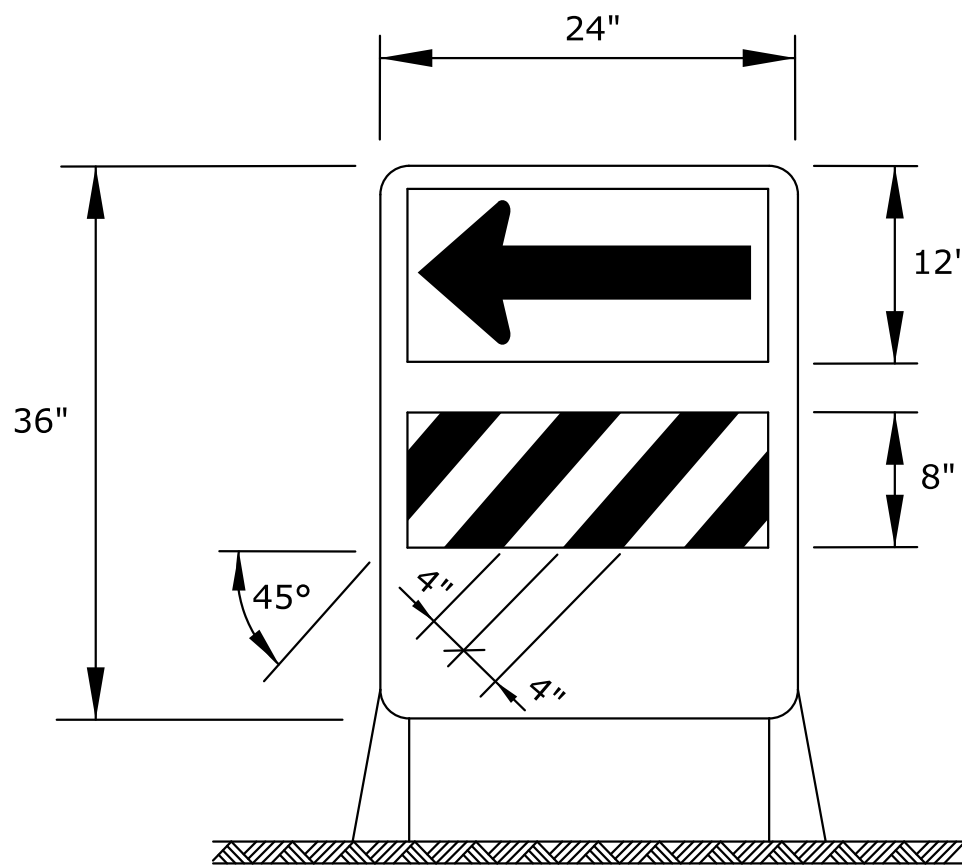
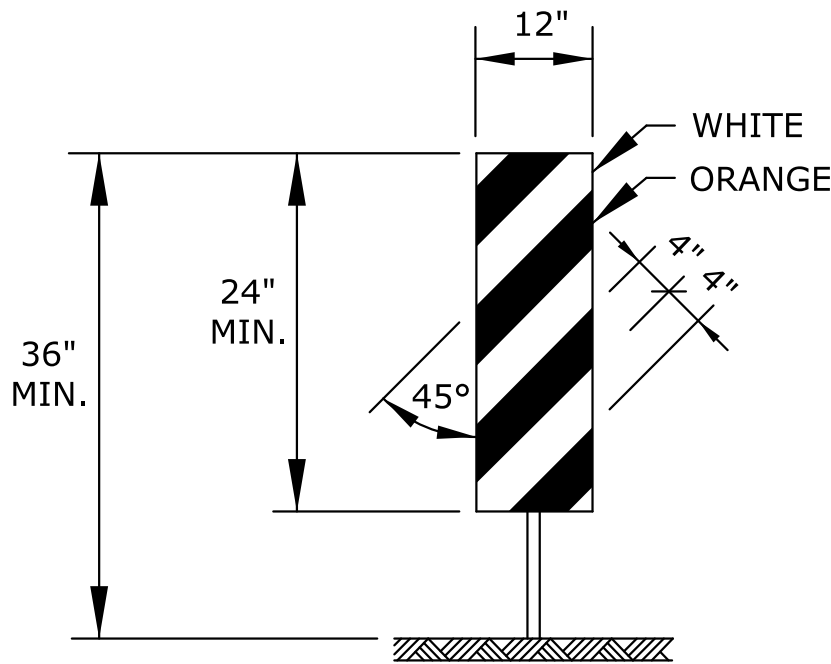
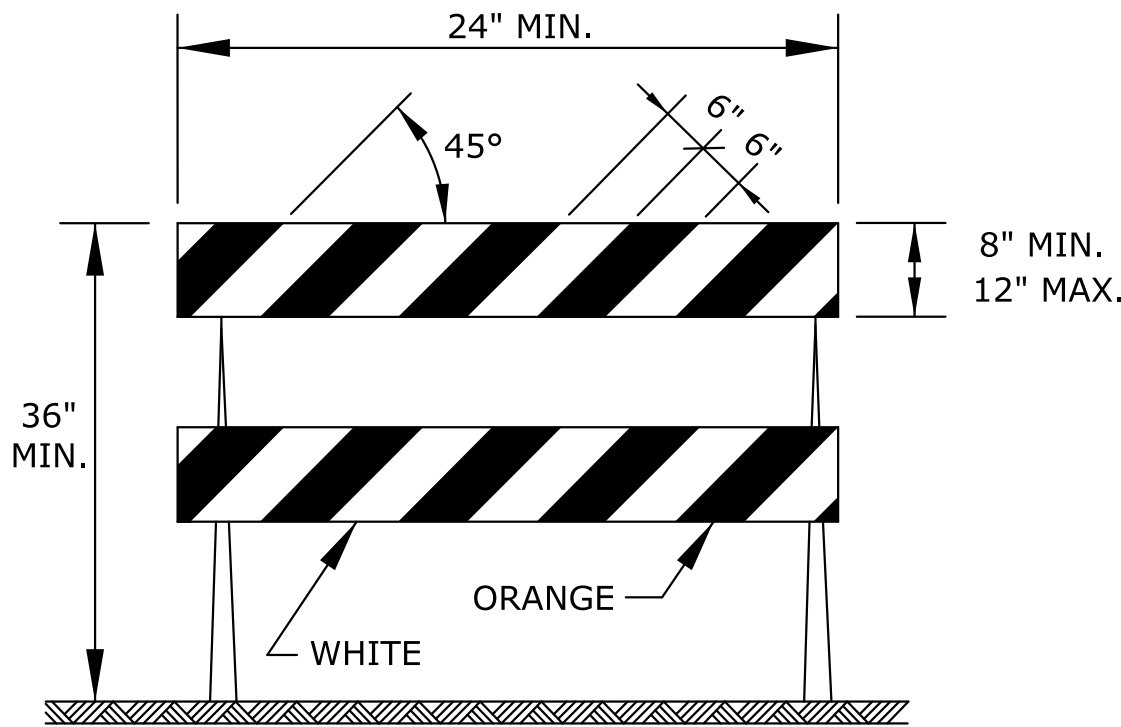
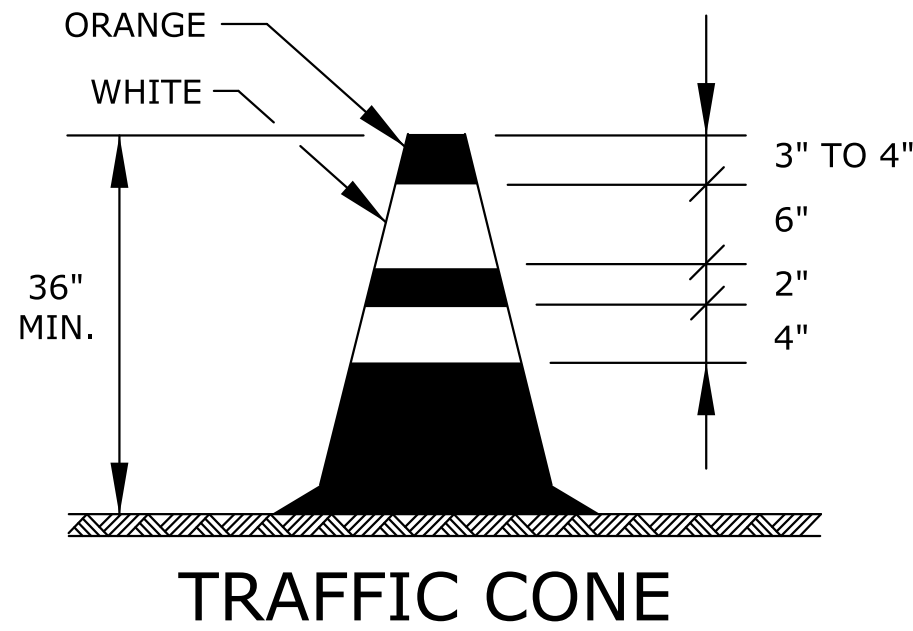
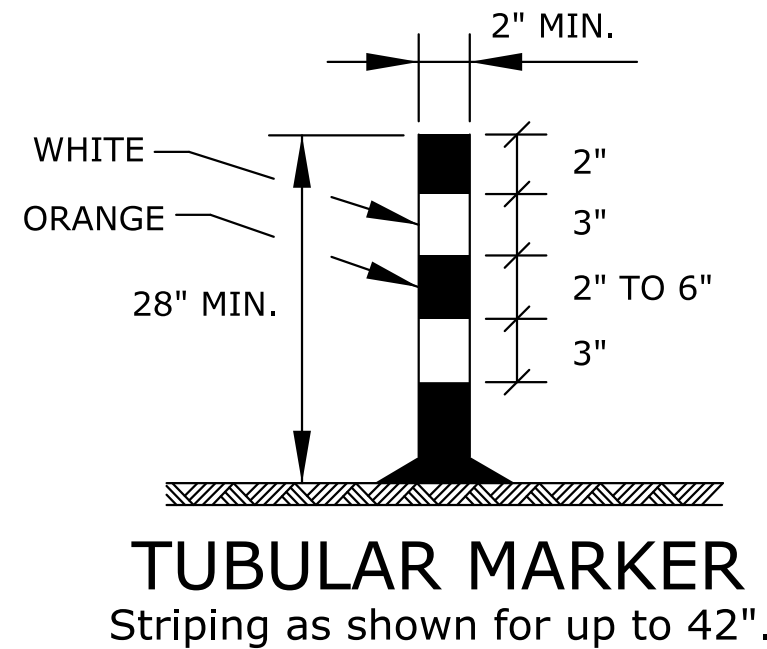
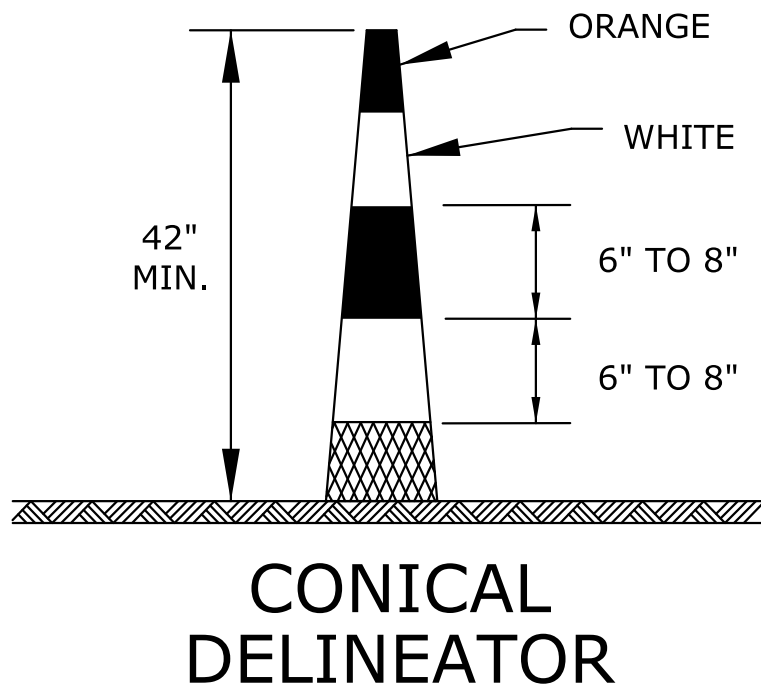
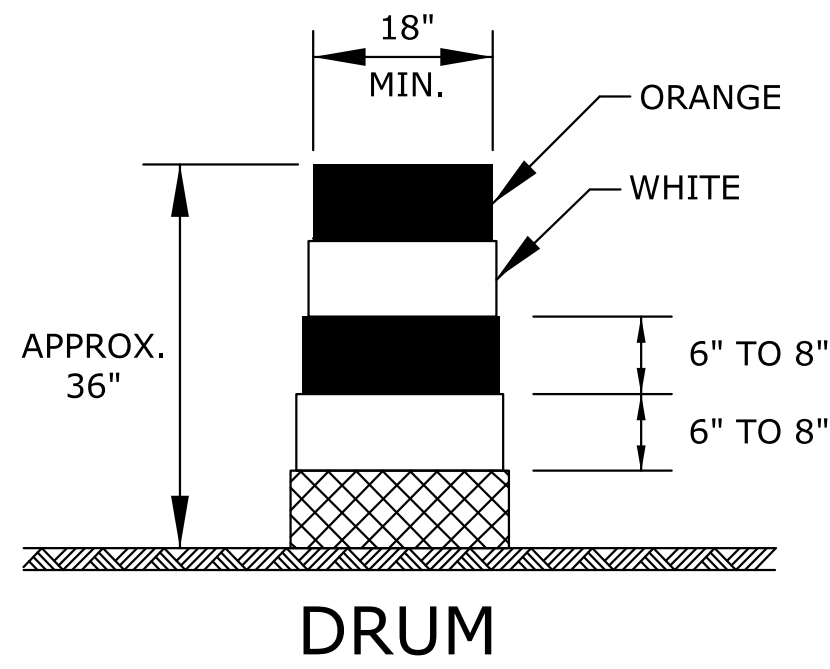
SPEED (MPH) *	20	25	30	35	40	45	50	55	60	65	70	75
LENGTH (ft)	115	155	200	250	305	360	425	495	570	645	730	820

* Posted speed prior to work starting

Neither work activity nor storage of equipment, vehicles, or material should occur in the buffer space. When a protection vehicle is placed in advance of the work space, only the space upstream of the vehicle constitutes the buffer space.

If temporary concrete safety barrier system is used to seperate approaching traffic from the work space, the barrier system shall be considered part of the activity area. A full lane width should be available throughout the length of the buffer space. See typical work zone components above.

3					
2					
1	08/18/15	Channelizer spacing Info	R.W.B.	K.E.	
NO.	DATE	REVISIONS	BY	APP'D	
KANSAS DEPARTMENT OF TRANSPORTATION					
TRAFFIC CONTROL GENERAL NOTES					
TE700					
FHWA APPROVAL	08/18/15	APP'D	Kristina Ericksen		
DESIGNED	B.A.H.	DETAILED	R.W.B.	QUANTITIES	TRACED
DESIGN CK.	DETAIL CK.	QUAN. CK.	TRACE CK.		



PEDESTRIAN CHANNELIZER

1. Support device shall not project beyond the detection plate into the pathway.
2. Hand trailing edges and detection plates are optional for continuous walls.
3. Interconnect pedestrian channelizers to prevent displacement and to provide continuous guidance through or around work.
4. Alternate pathways shall be firm, stable, and slip resistant.
5. Treat height differentials > 1/2" in the surfaces of alternate paths with a firm, stable, and slip resistant temporary ramp having a slope of 12:1 or flatter and having a width equal to the alternate path.
6. Use alternating orange/white on interconnected devices.

ITEM		LOCATION									
		Cross-overs	Shoofly Divisions	Tangents	Tapers	Ramps	Head to Head	Object Identifier	Lead-in Devices	Gores	
PORTABLE	Drums	Yes	Yes	Yes	Yes	Yes	(1)	Yes	Yes	Yes	
	Conical Delineators	Yes	Yes	Yes	Yes	Yes	(1)	Yes	Yes	Yes	
	Vertical Panels	(2)	(2)	(2)	(2)	(2)	(1,2)	YES	(2)	(2)	
	Direction Indicator Barricade	NO	NO	NO	Yes	NO	NO	NO	NO	NO	
	Type 2 Barricade	(2)	(2)	(2)	(2)	NO	NO	Yes	NO	NO	
	Traffic Cones	NO	NO	(4)	(4)	(4)	NO	(4)	(4)	(4)	
FIXED											
	Tubular Markers	(3)	(3)	(3)	NO	(3)	Yes	NO	Yes	Yes	
	Vertical Panels	(3)	(3)	(3)	(3)	(3)	(3)	Yes	(2,3)	(2)	

- (1) Not allowed on centerline delineation along freeways or expressways.
(2) The stripes shall slope downward to the traffic side for channelization.
(3) May be used upon the approval of the engineer.
(4) Daytime operations only.

3					
2					
1					
NO.	DATE	REVISIONS	BY	APP'D	
KANSAS DEPARTMENT OF TRANSPORTATION					
TRAFFIC CONTROL CHANNELIZING DEVICES					
TE702					
DESIGNED	L.E.R.	DETAILED	R.W.B.	QUANTITIES	TRACED
DESIGN CK.	DETAIL CK.	QUAN. CK.	TRACE CK.		

Note: Signs shown for one approach to work zone.

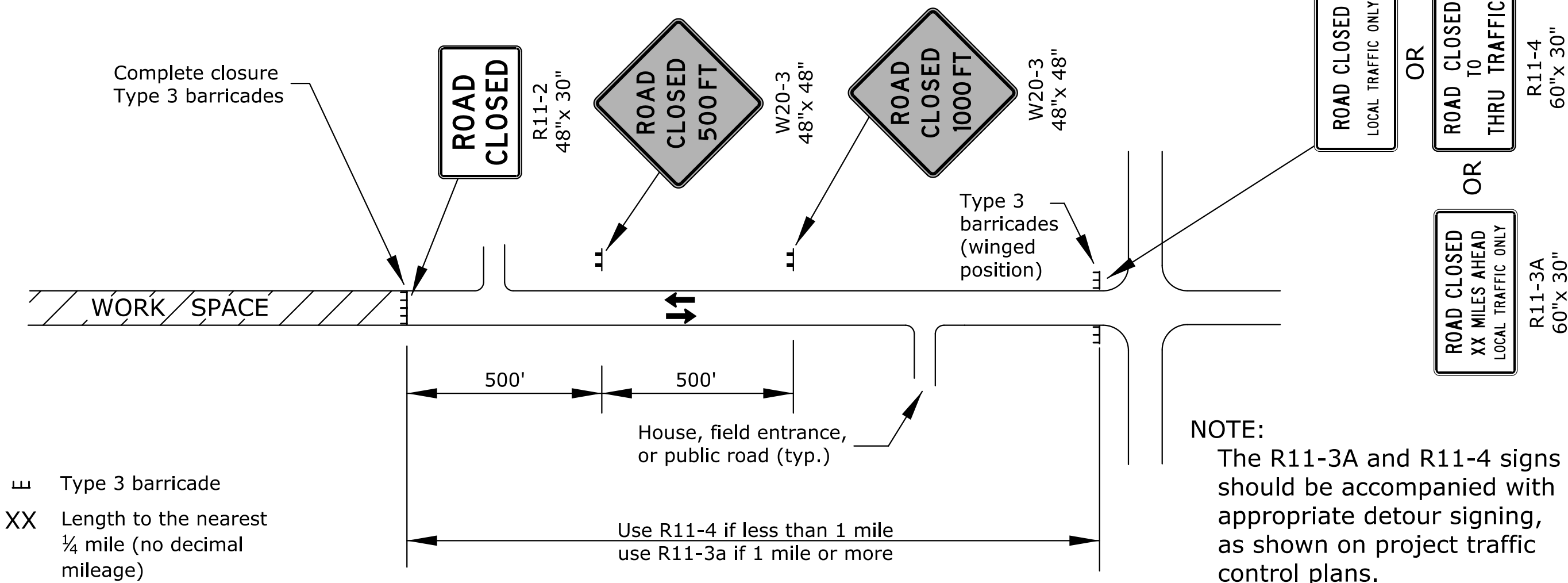


FIGURE 1: TYPICAL SIGNING FOR ROAD CLOSURE (MAINLINE OR SIDE ROAD)

Note: Sign shown for one approach to intersection (work zone).

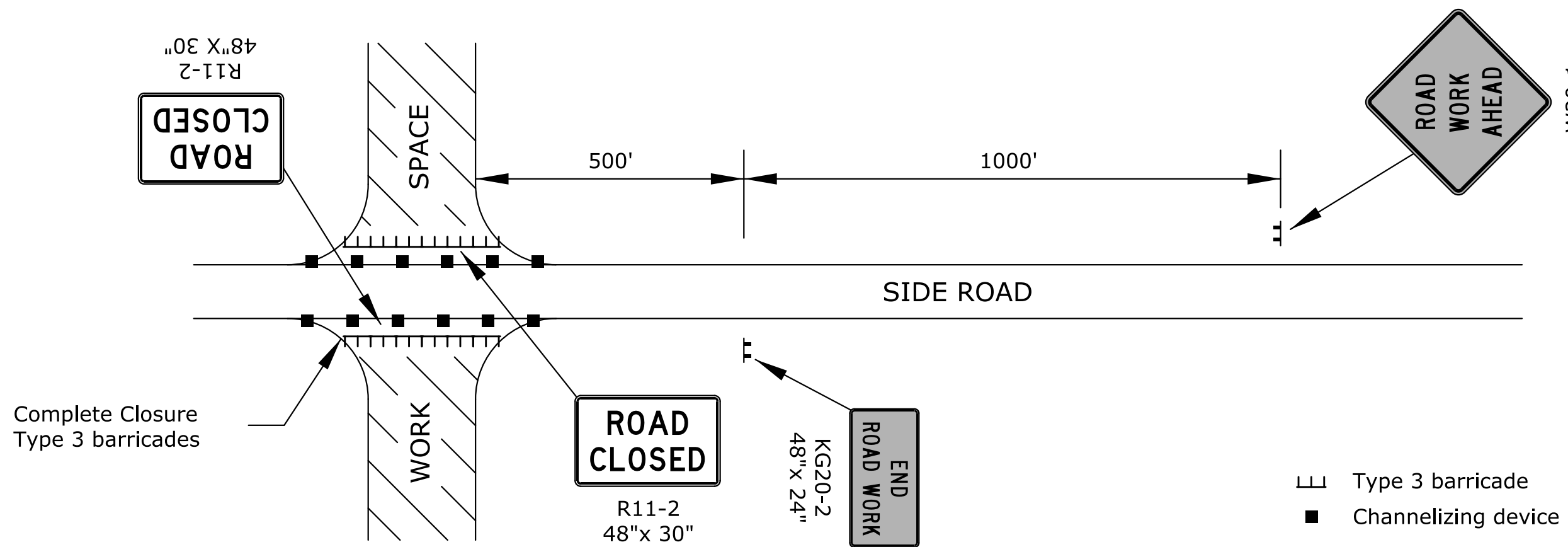


FIGURE 2: TYPICAL SIGNING FOR SIDE ROAD OPEN

Note: Signs shown for one approach to work zone.

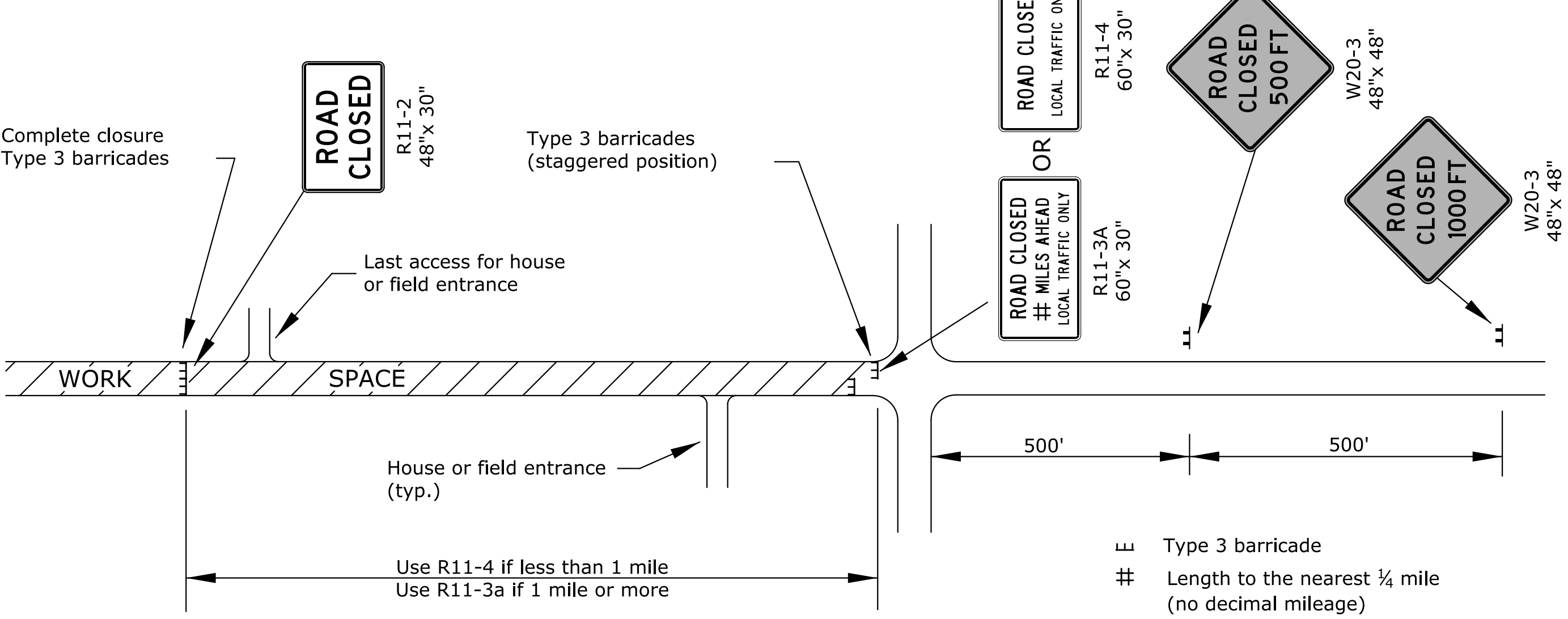


FIGURE 3: TYPICAL SIGNING FOR ROAD CLOSURE - LOCAL TRAFFIC ACCESS

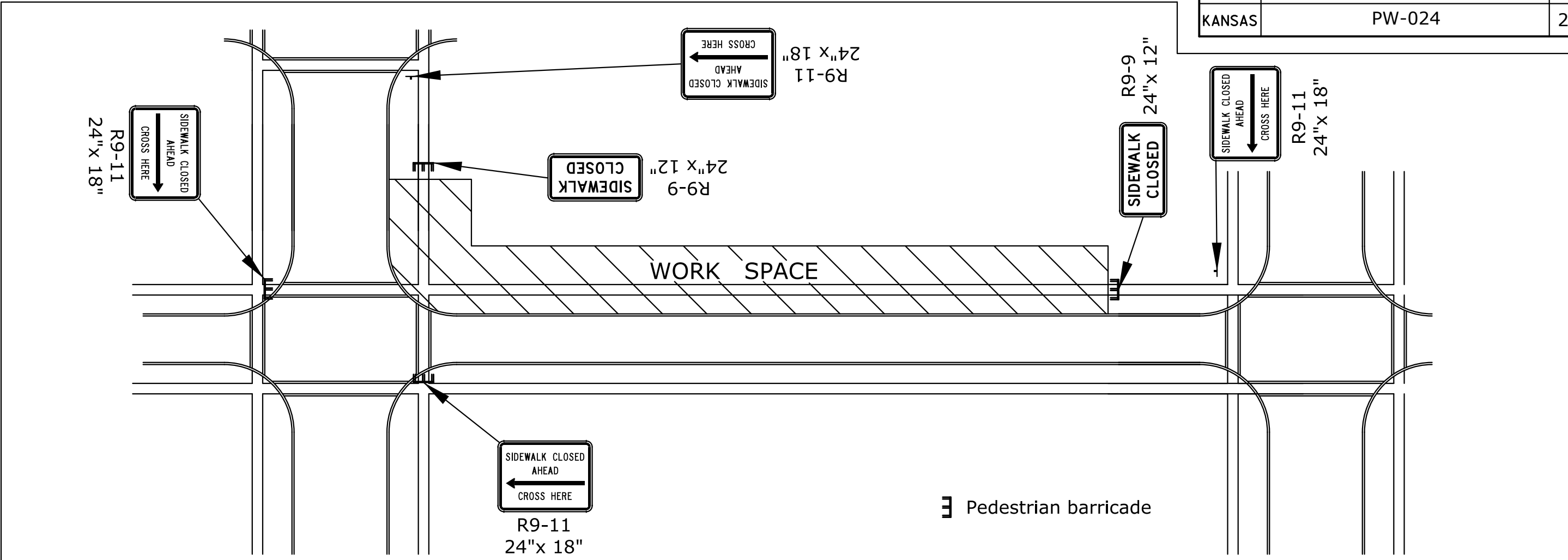
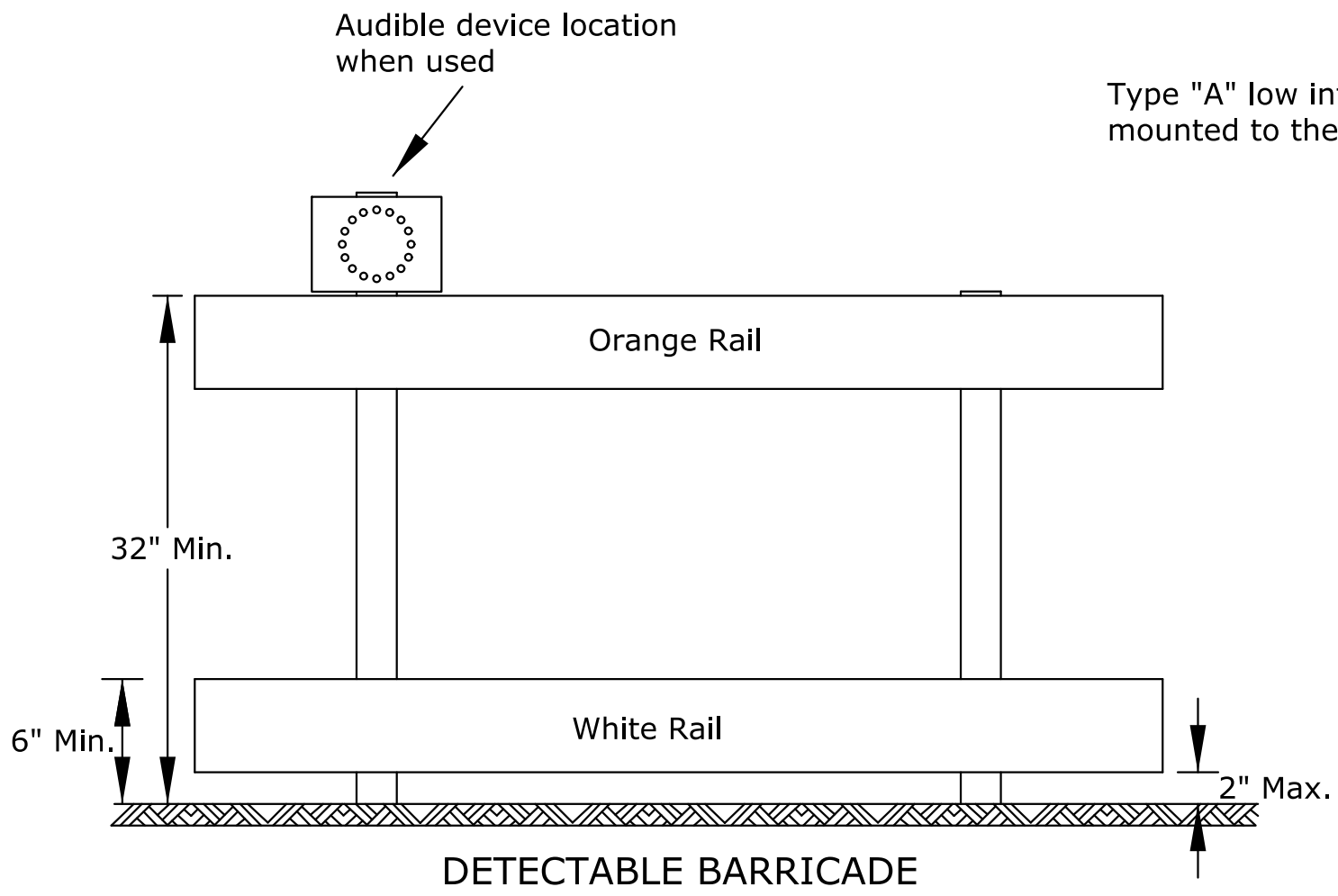
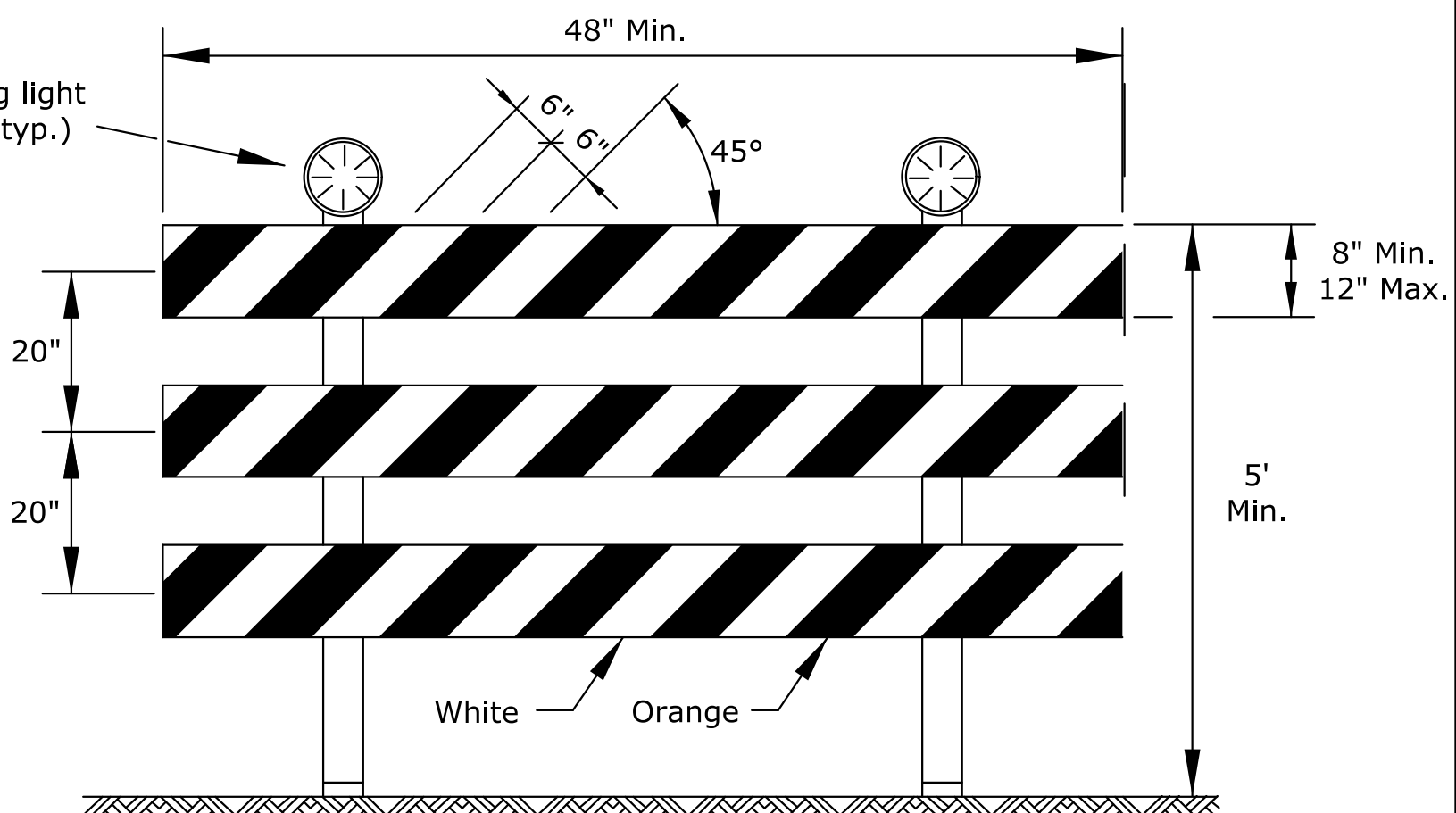


FIGURE 4: TYPICAL SIGNING FOR SIDEWALK CLOSED WITH OPPOSITE SIDEWALK AVAILABLE



1. Support device shall not project beyond the detection plate into the pathway.
2. Barricades shall be used to close the entire width of the pathway.
3. Do not use warning lights on pedestrian barricades.
4. Do not use warning lights on audible devices.



TYPE 3 BARRICADE WITH LIGHTS

Approved signs mounted on Type 3 barricades should not cover more than 50% of the top two rails or 33% of the total area of the three rails.

When barricades are placed end-to-end or staggered, a Type "A" low intensity warning light shall be mounted to the vertical post near each outside corner of the end barricades.

ROAD CLOSED GENERAL NOTES

As shown in Figure 1, at the point where thru traffic must detour and local traffic can proceed to the location where the roadway is completely closed, the R11-3a (ROAD CLOSED # MILES AHEAD LOCAL TRAFFIC ONLY) or R11-4 (ROAD CLOSED LOCAL TRAFFIC ONLY or ROAD CLOSED TO THRU TRAFFIC) sign shall be used with Type 3 barricades (winged position), placed on the shoulders of roadway.

As shown in Figure 3, when local traffic must be allowed access into the work zone, Type 3 barricades shall be longitudinally staggered to maintain the appearance of a closed roadway. A second line of end-to-end Type 3 barricades shall be placed just beyond the last access point in the work zone, to completely close the roadway.

The R11-4 (ROAD CLOSED TO THRU TRAFFIC or ROAD CLOSED LOCAL TRAFFIC ONLY) sign shall be used when the distance to the point of complete closure of the roadway is less than 1 mile.

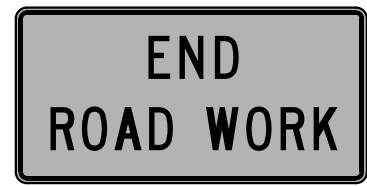
The R11-3a (ROAD CLOSED # MILES AHEAD LOCAL TRAFFIC ONLY) sign shall be used when the distance to the point of complete closure of the roadway is 1 mile or greater.

The words "BRIDGE OUT" (or BRIDGE CLOSED) may be substituted for the words "ROAD CLOSED" on the R11-3a or R11-4 sign where applicable.

3					
2					
1					
NO.	DATE	REVISIONS	BY	APP'D	
KANSAS DEPARTMENT OF TRANSPORTATION					
TRAFFIC CONTROL CLOSURES					
TE704					
DESIGNED	B.A.H.	DATE	06/01/15	APP'D	Kristina Ericksen
DESIGN CK.	DETAIL CK.	QUANTITIES	QUAN. CK.	TRACED	TRACE CK.

Drawn By : mushook
File : te710.dgn
Plotted : 01-JUN-2015 13:54
Traffic

SIGN LAYOUT INFORMATION



KG20-2

STD. SIZE
EXPWY/FREEWAY

6" C
48"x 24"



KG20-5

STD. SIZE
EXPWY/FREEWAY

6" C
48"x 24"



KM4-20

STD. SIZE
EXPWY/FREEWAY

3" C 6" C
24"x 6" 48"x 12"



W7-3a

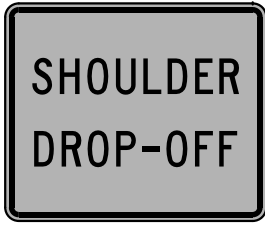
Mileage to be determined
by the engineer.



W8-17

STD. SIZE
EXPWY/FREEWAY

48"x 48"



W8-17P

(OPTIONAL)

STD. SIZE
EXPWY/FREEWAY

30"x 24"



W8-15

STD. SIZE
EXPWY/FREEWAY

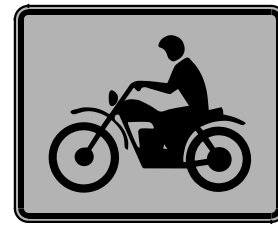
8" D
48"x 48"



W8-7

STD. SIZE
EXPWY/FREEWAY

8" D
48"x 48"



W8-15p

STD. SIZE
EXPWY/FREEWAY

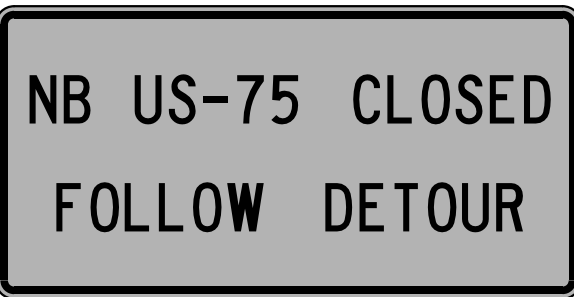
30"x 24"



W8-11

STD. SIZE
EXPWY/FREEWAY

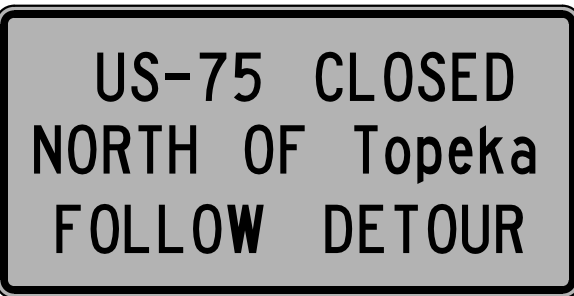
8" D
48"x 48"



SP-01
(SPECIAL SIGN)

STD. SIZE
EXPWY/FREEWAY

6" C 10" D

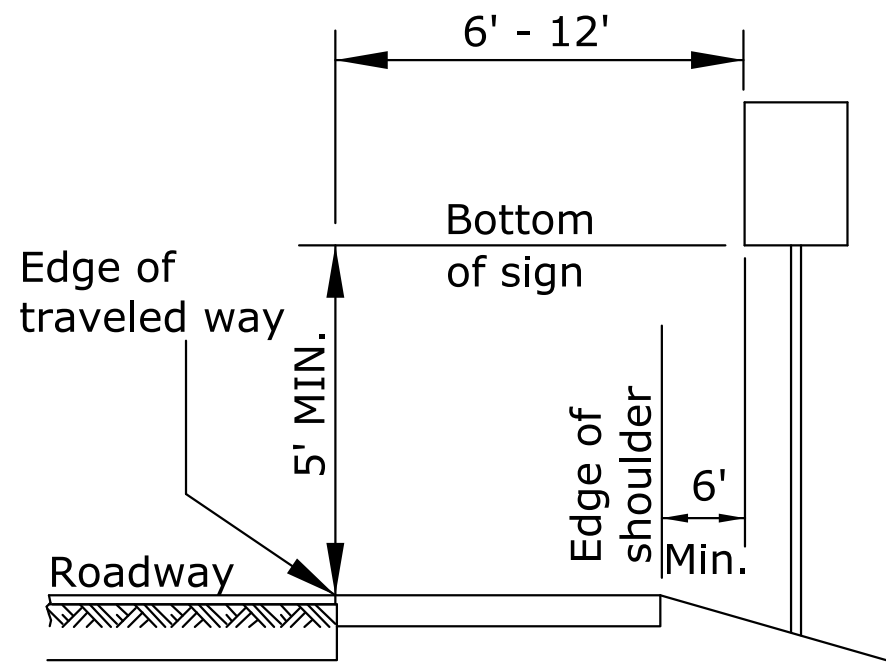


SP-02
(SPECIAL SIGN)

STD. SIZE
EXPWY/FREEWAY

UPPERCASE: 6" C 10" D
LOWERCASE: 4.5" C 8" D

ALL CITY NAMES AND STREET NAMES ON SPECIAL SIGNS AND DESTINATION SIGNS
MUST HAVE UPPER AND LOWER CASE LETTERS.

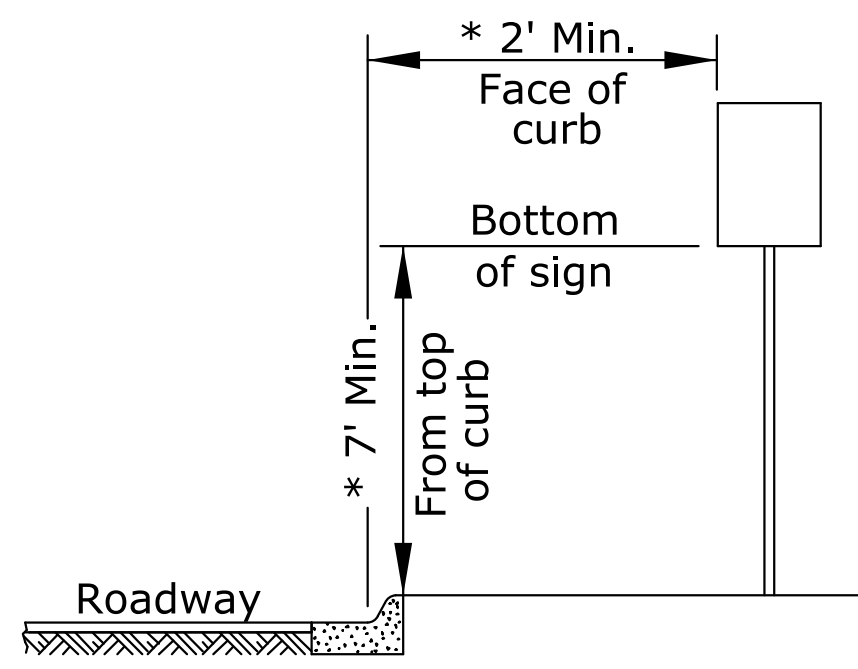


Rural

1) Ground-mounted signs shall be mounted at a minimum height of 5' measured from the bottom of sign to the near edge of the pavement.

2) Large signs having an area exceeding 50 square feet installed on multiple breakaway posts shall be mounted a minimum of 7' above the ground.

3) The height of the secondary sign mounted below another sign may be 4' measured from the bottom of the sign to the near edge of the pavement. Signs shall not overlap each other.



Urban

1) Signs shall be mounted at a minimum height of 7' measured from the bottom of sign to the near edge of the pavement.

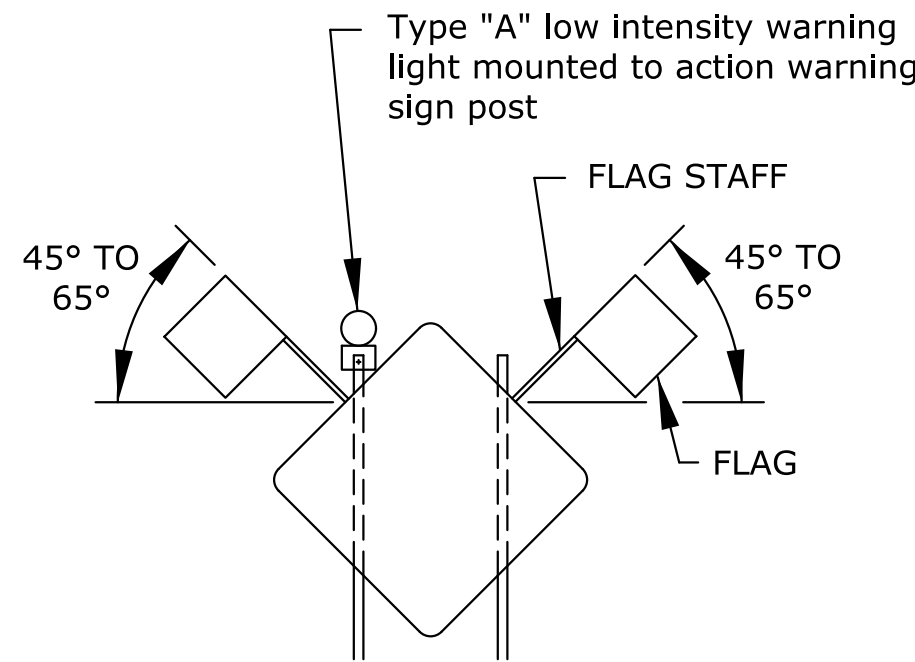
2) Neither portable nor permanent sign supports should be located on sidewalks or areas designated for pedestrian or bicycle traffic.

3) Signs mounted lower than 7' should not project more than 4" into pedestrian facilities.

4) The height from of the secondary sign mounted below another sign may be 6' measured from the bottom of sign to the near edge of the pavement. Signs shall not overlap each other.

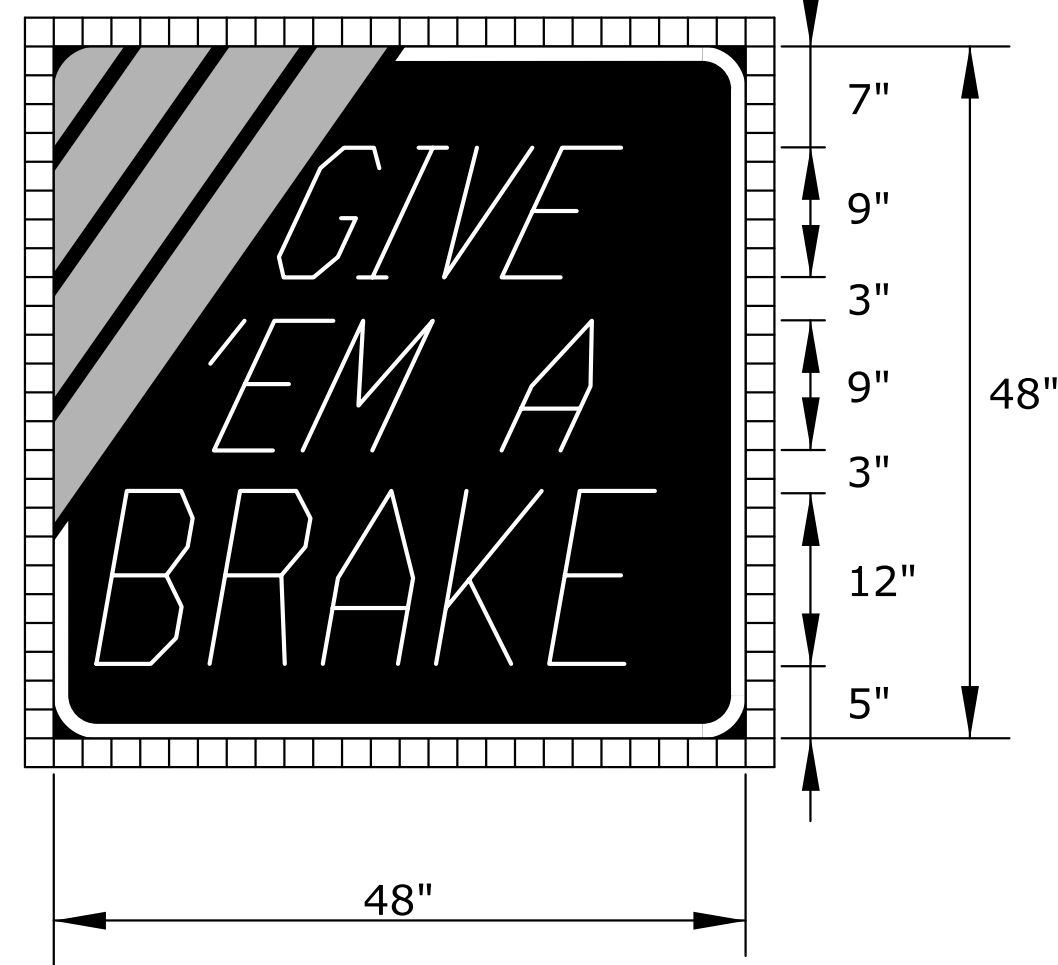
5) Large signs having an area exceeding 50 square feet installed on multiple breakaway posts shall be mounted a minimum of 7' above the ground.

* 6) Pedestrian detour signing shall be a minimum of 2' measured from the top of the pedestrian pathway to the bottom of the sign and shall not protrude into the walkway nor shall it project beyond the back of curb.

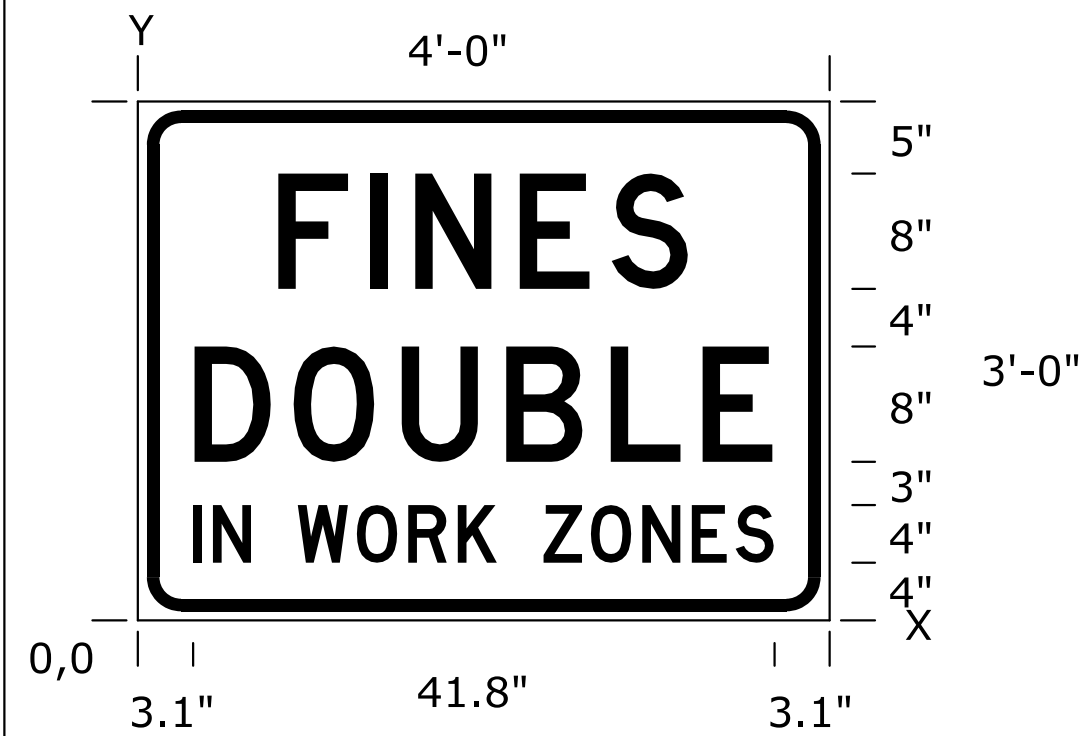


When the sign width is equal to or greater than 9', three or more wood posts may be used with a minimum of 4' between the centerline of each post. All signs less than 9' in width shall use a maximum of two wood posts.

In the case of hitting rock when driving posts
1. Shift the sign location. Do not violate minimum sign spacing.
2. With the engineer's approval, use acceptable alternative sign stands.



KI-104a



KI-105a

STATE	PROJECT NO.	YEAR	SHEET NO.	TOTAL SHEETS
KANSAS	PW-024	2016	18	27

SIGN NUMBER	GIVE EM A BRAKE
WIDTH x HEIGHT	4'-0" x 4'-0"
BORDER WIDTH	1.0"
CORNER RADIUS	4.0"
STRIPE WIDTH	3.0"
MOUNTING	GROUND
BACKGROUND	TYPE: NON-REFLECTIVE COLOR: BLACK
LEGEND/BORDER	TYPE: REFLECTIVE COLOR: WHITE
LEGEND FONT	DUTCH 801 ROMAN SWC 25 DEGREE SLANT
STRIPES	TYPE: REFLECTIVE COLOR: ORANGE

SIGN NUMBER	FINES DOUBLE
WIDTH x HEIGHT	4'-0" x 3'-0"
BORDER WIDTH	0.9"
CORNER RADIUS	3.0"
MOUNTING	GROUND
BACKGROUND	TYPE: REFLECTIVE COLOR: WHITE
LEGEND/BORDER	TYPE: NON-REFLECTIVE COLOR: BLACK

DIMENSIONS IN INCHES

SPACINGS ARE TO START OF NEXT LETTER

Y FONT	LETTER SPACINGS																HT LEN
23.0 D	9.7	6.4	3.2	7.3	6.4	5.4	9.7										8.0
11.0 D	3.9	6.9	7.5	7.3	6.4	4.9	3.9										28.6
4.0 D	3.1	1.6	2.7	3.2	4.3	3.8	3.6	2.8	3.2	3.4	3.8	3.6	3.2	2.7	3.1		8.0
																	40.3
																	4.0
																	41.8

Notes:

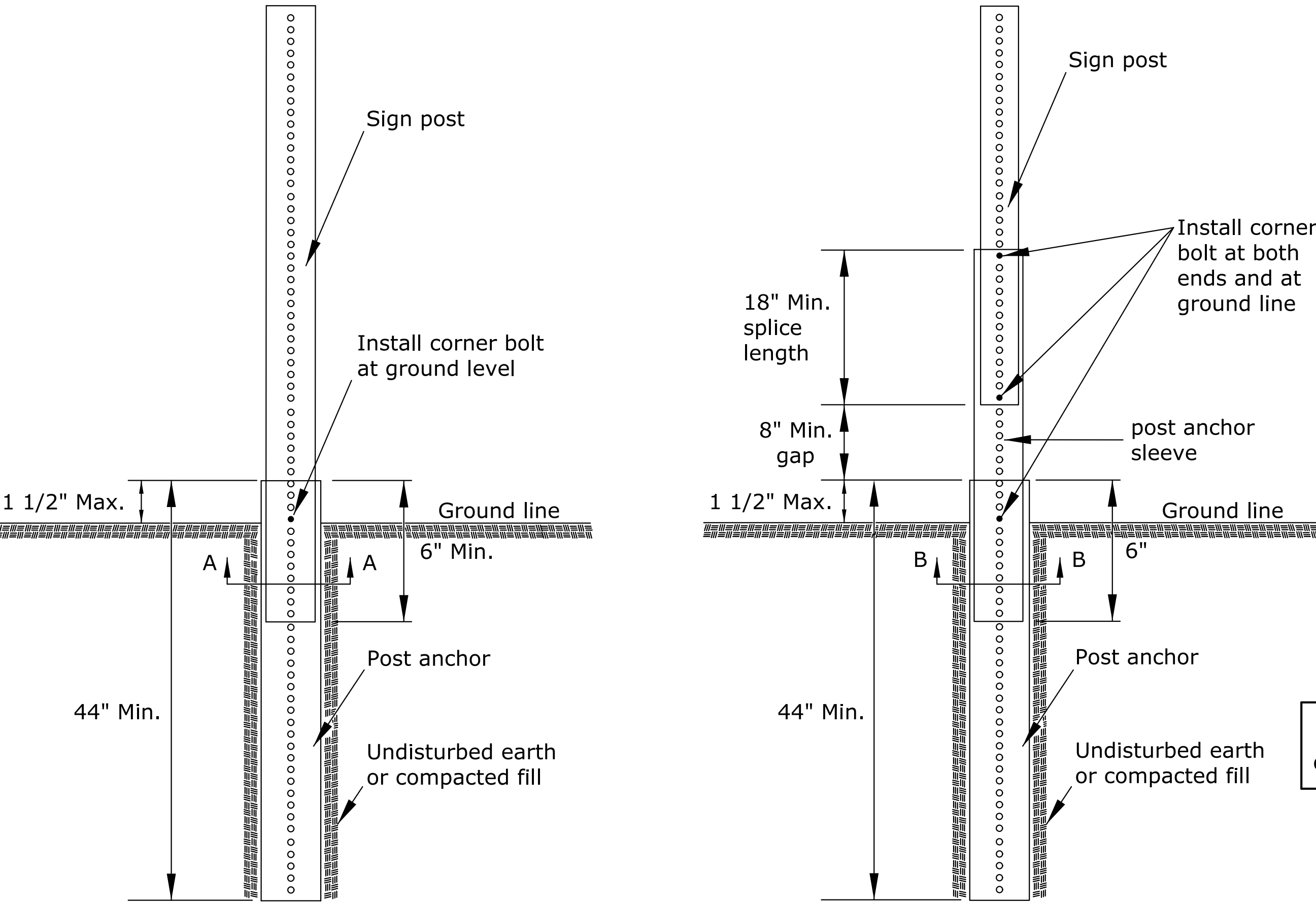
Typically, there are two sets of informational signs installed per project: one for each direction of traffic.

Install signs a minimum of 500' in advance of the road work ahead sign. The engineer may designate a more appropriate location if conditions dictate.

The informational signs are not to interfere with the traffic control signs for the project.

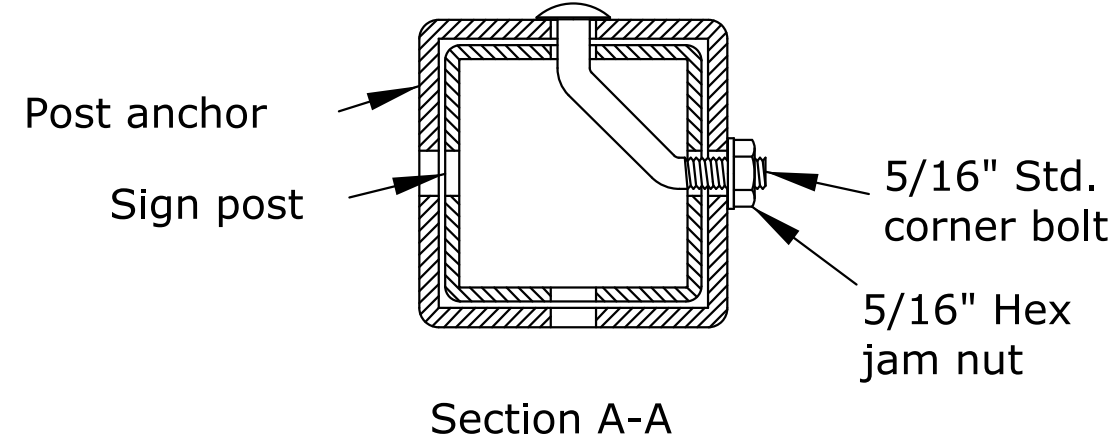
3					
2					
1					
NO.	DATE	REVISIONS	BY	APP'D	
KANSAS DEPARTMENT OF TRANSPORTATION					
TRAFFIC CONTROL SIGN INFORMATION					
TE710					
FHWA APPROVAL		06/01/15	APP'D	Kristina Pyle	
DESIGNED	R.W.B.	DETAILED	R.W.B.	QUANTITIES	TRACED
DESIGN CK.	DETAIL CK.	QUAN. CK.	TRACE CK.		

Perforated square steel tube (P.S.S.T.) post setup

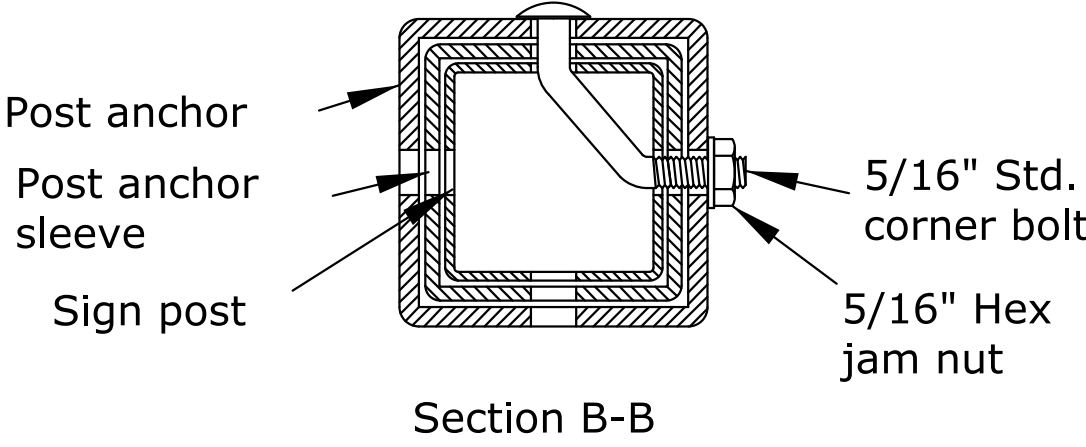


P.S.S.T. detail

Telescoping P.S.S.T. detail



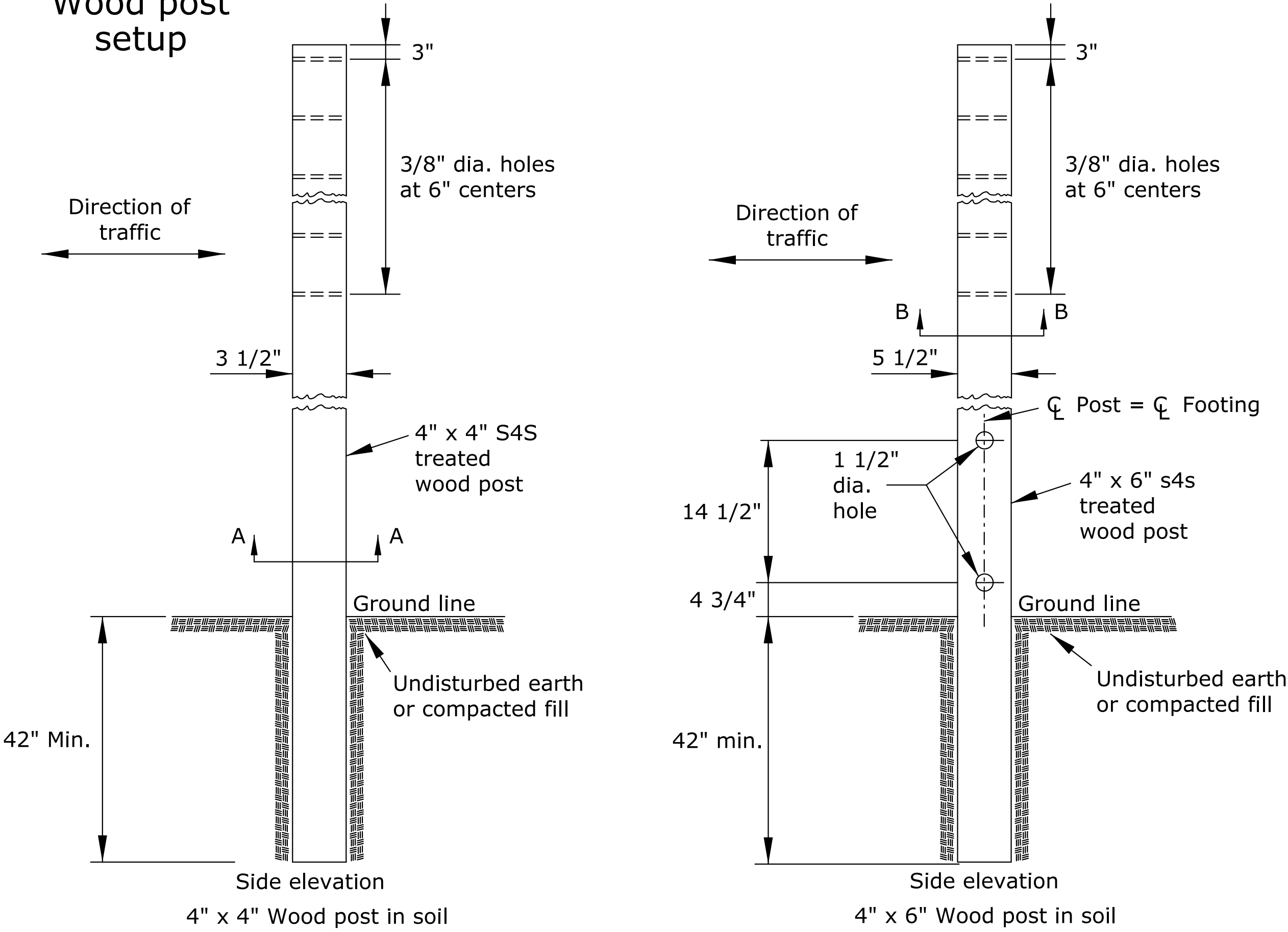
Section A-A



Section B-B

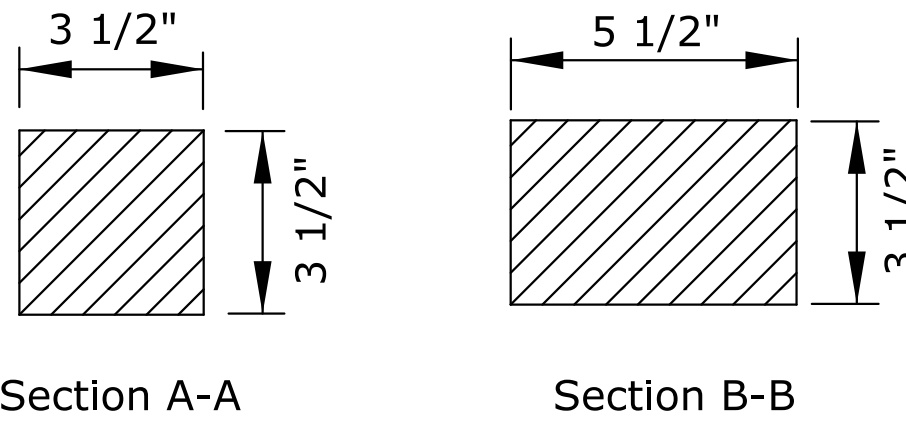
Details for 2", 2 1/4", or 2 1/2" sign posts
Place bolts in the same corner along each sign post.

Wood post setup



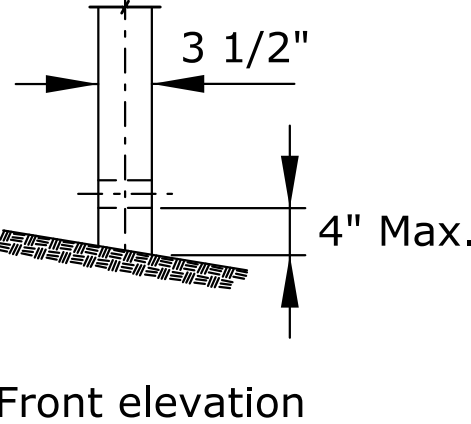
Side elevation
4" x 4" Wood post in soil

Side elevation
4" x 6" Wood post in soil



Section A-A

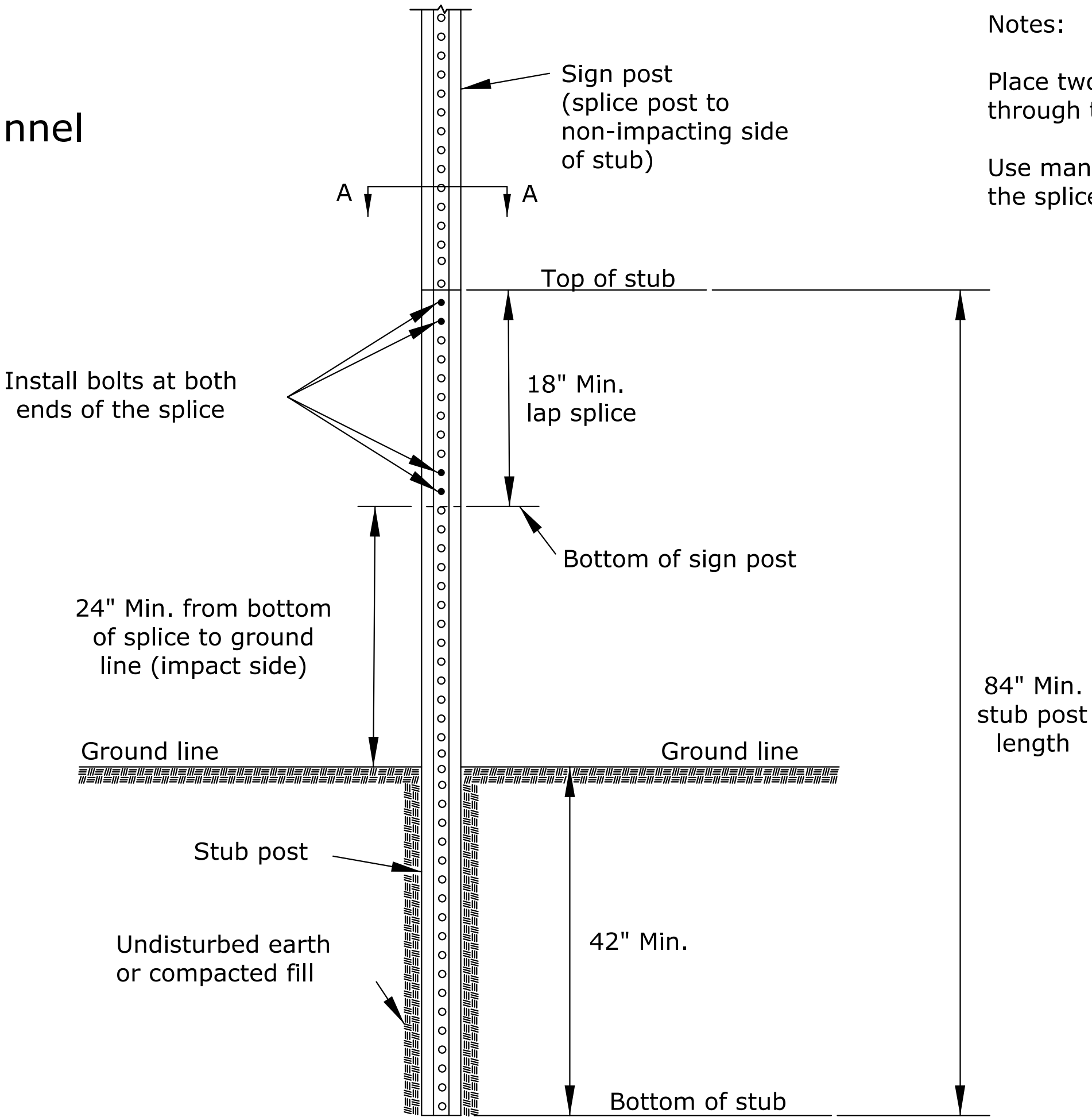
Section B-B



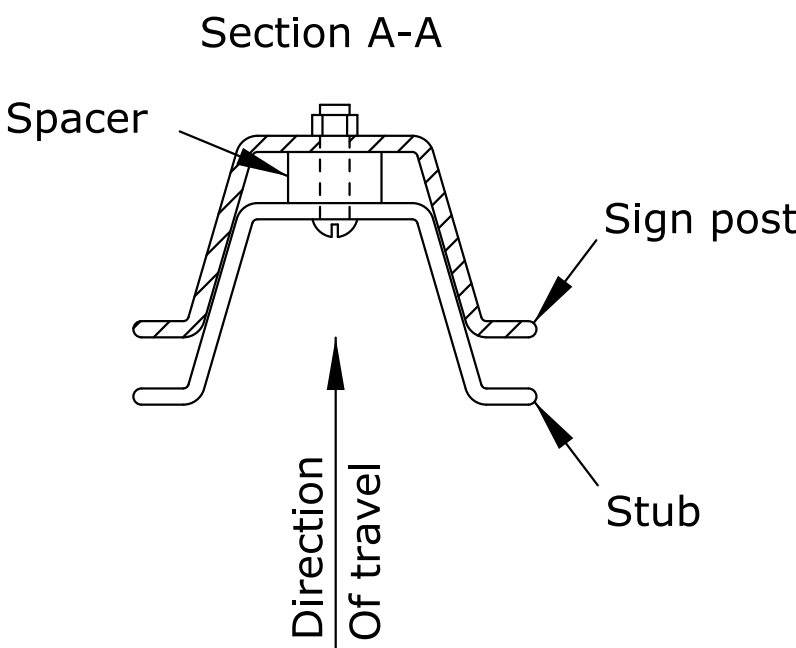
Front elevation

See TE710 for additional details and requirements

3 lb/f U-Channel setup



Notes:
Place two bolts at both ends of the splice through the holes nearest the ends of the splice.
Use manufacturer recommended spacers over the bolts between the spliced pieces of U-Channel.



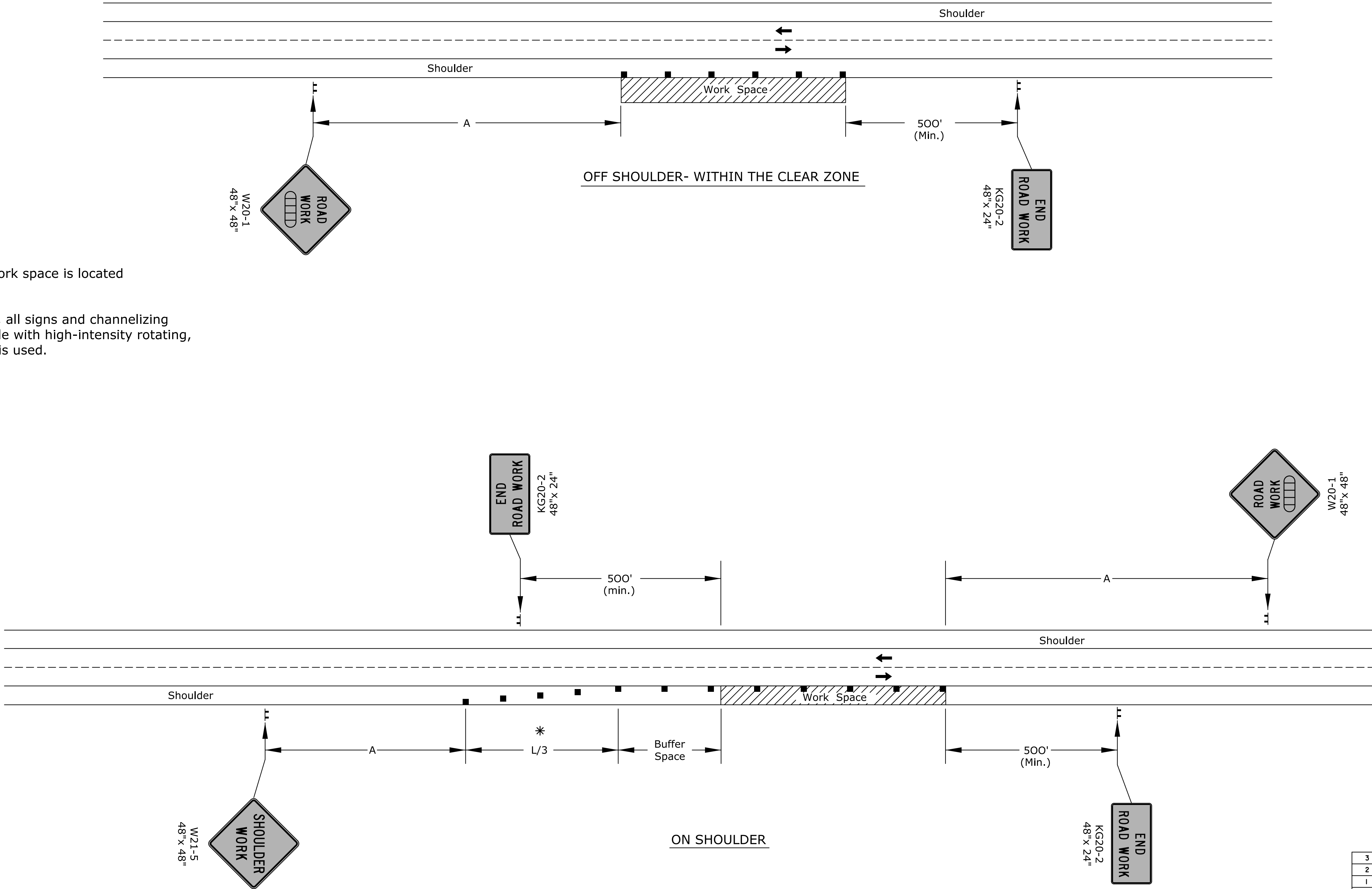
3					
2					
1					
NO.	DATE	REVISIONS	BY	APP'D	
KANSAS DEPARTMENT OF TRANSPORTATION					
TRAFFIC CONTROL SIGN POSTS					
TE712					
DESIGNED	B.A.H.	DETAILED	R.W.B.	QUANTITIES	TRACED
DESIGN CK.	DETAIL CK.	QUAN. CK.	TRACE CK.		

STATE	PROJECT NO.	YEAR	SHEET NO.	TOTAL SHEETS
KANSAS	PW-024	2016	20	27

Notes:

No traffic control is required if the work space is located outside of the clear zone.

For operations of 60 minutes or less, all signs and channelizing devices may be eliminated if a vehicle with high-intensity rotating, flashing, oscillating, or strobe lights is used.



* Omit taper if paved shoulder is less than 8' wide.

- Channelizing device
- ⏏ Ahead, 1500 ft, or 1 mile

3					
2					
1					
NO.	DATE	REVISIONS	BY	APP'D	
KANSAS DEPARTMENT OF TRANSPORTATION					
TRAFFIC CONTROL SHOULDER WORK UNDIVIDED ROADWAY					
TE720					
FHWA APPROVAL		06/01/15	APP'D	Kristina Ericksen	
DESIGNED	L.E.R.	DETAILED	R.W.B.	QUANTITIES	TRACED
DESIGN CK.	DETAIL CK.	QUAN. CK.	TRACE CK.		

STATE	PROJECT NO.	YEAR	SHEET NO.	TOTAL SHEETS
KANSAS	PW-024	2016	21	27

Summary Of Traffic Control Devices (Each)

[illegible]

Summary Of Traffic Control Devices (Each Per Day)

* Quantity Most Used On The Project At Any One Time

[illegible]

Barricades *		Channelizing Devices *		
Type 3 (4' To 12')	Pedestrian	Fixed	Portable	Pedestrian
27	16		50	

Lighted Devices *	
Work Zone Warning Light (Type "A" Low Intensity)	22
Work Zone Warning Light (Red Type "B" High Intensity)	
Arrow Display	
Portable Changeable Message Sign	

[illegible]

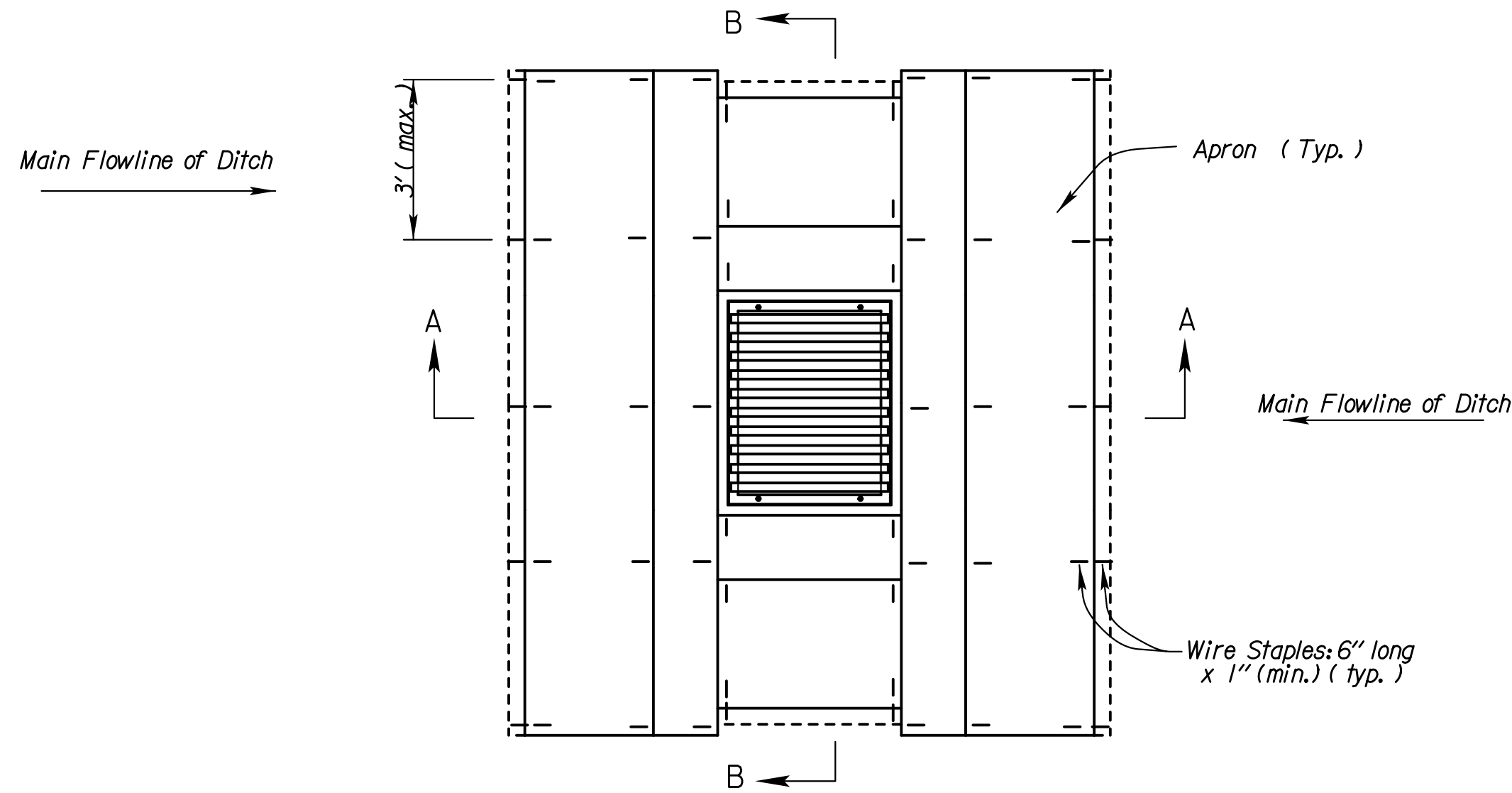
3				
2				
1				
NO.	DATE	REVISIONS	BY	APP'D

KANSAS DEPARTMENT OF TRANSPORTATION

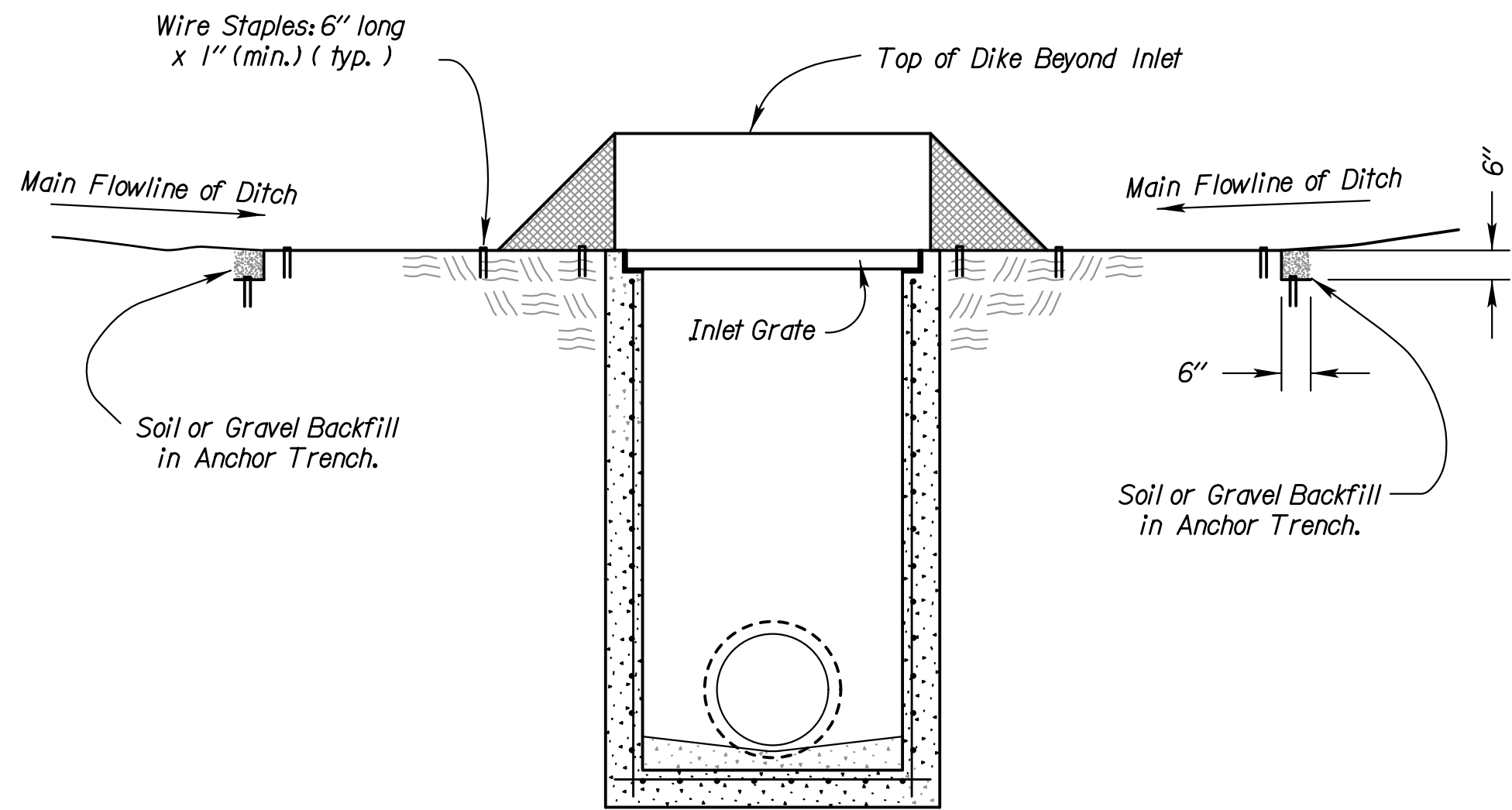
TRAFFIC CONTROL
SUMMARY OF DEVICES
RECAPITULATION OF QUANTITIES

TE795

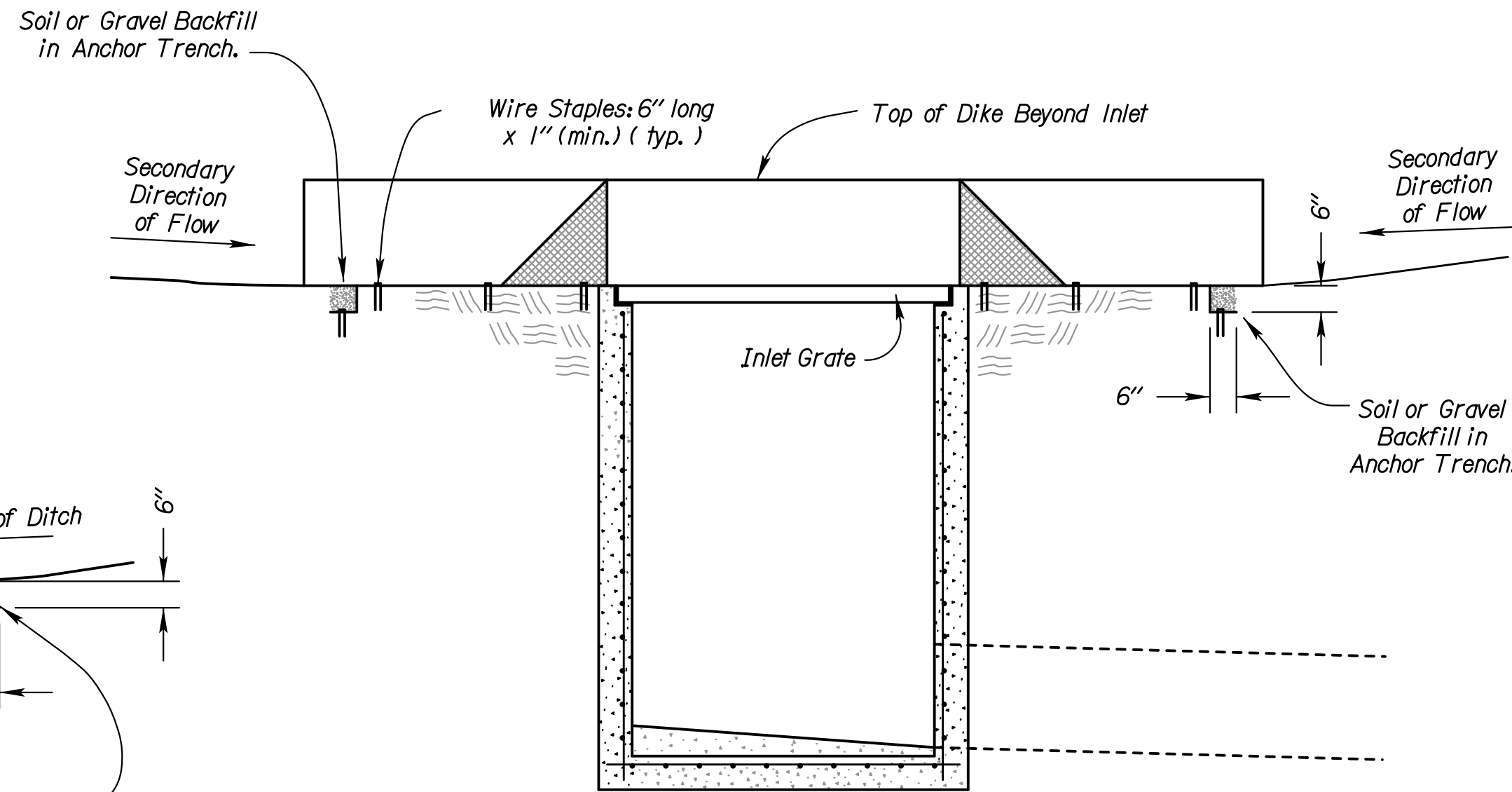
FHWA APPROVAL		06/01/15	APP'D	Kristina Erlakson
DESIGNED	B.A.H.	DETAILED	R.W.B.	QUANTITIES
DESIGN CK.	DETAIL CK.		QUAN. CK.	TRACE CK.



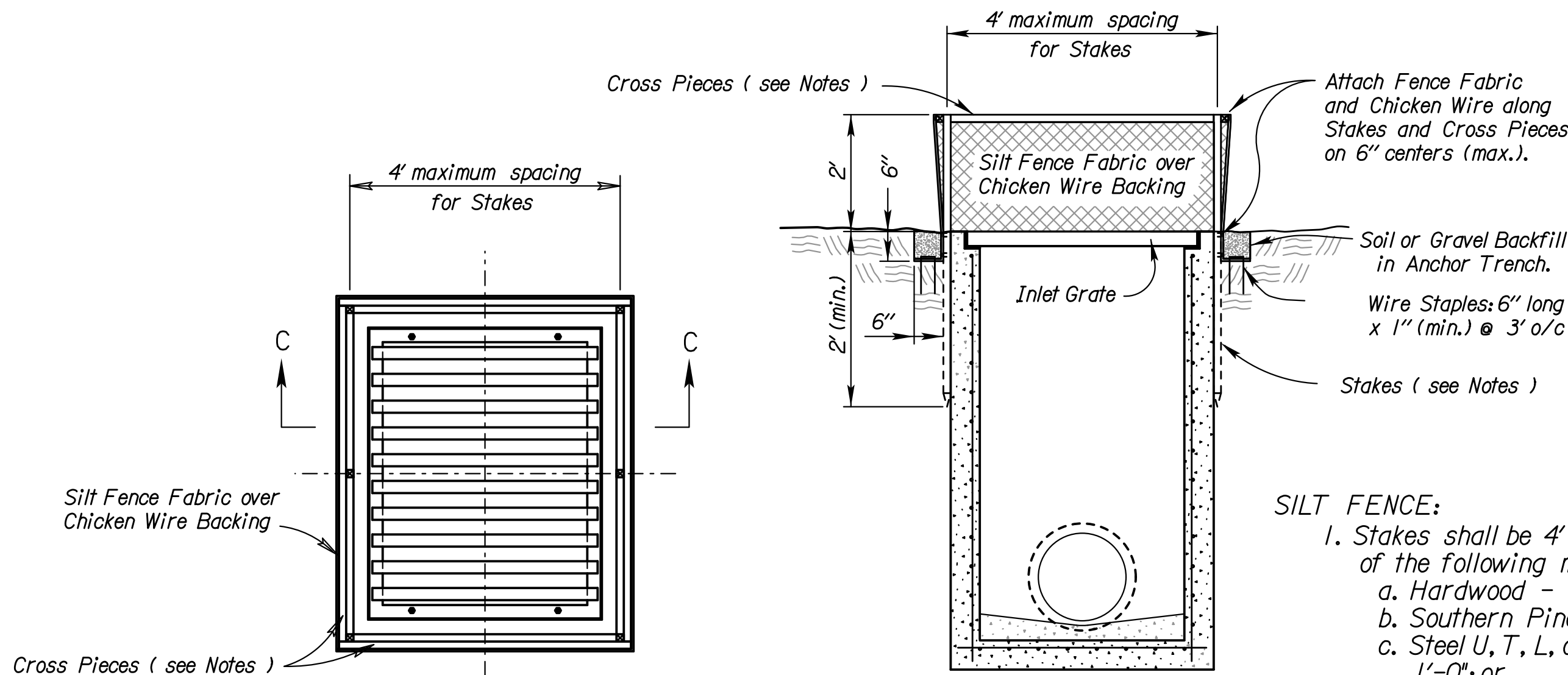
PLAN
TEMPORARY INLET SEDIMENT BARRIER
(TRIANGULAR SILT DIKE METHOD)
NO SCALE



SECTION A - A

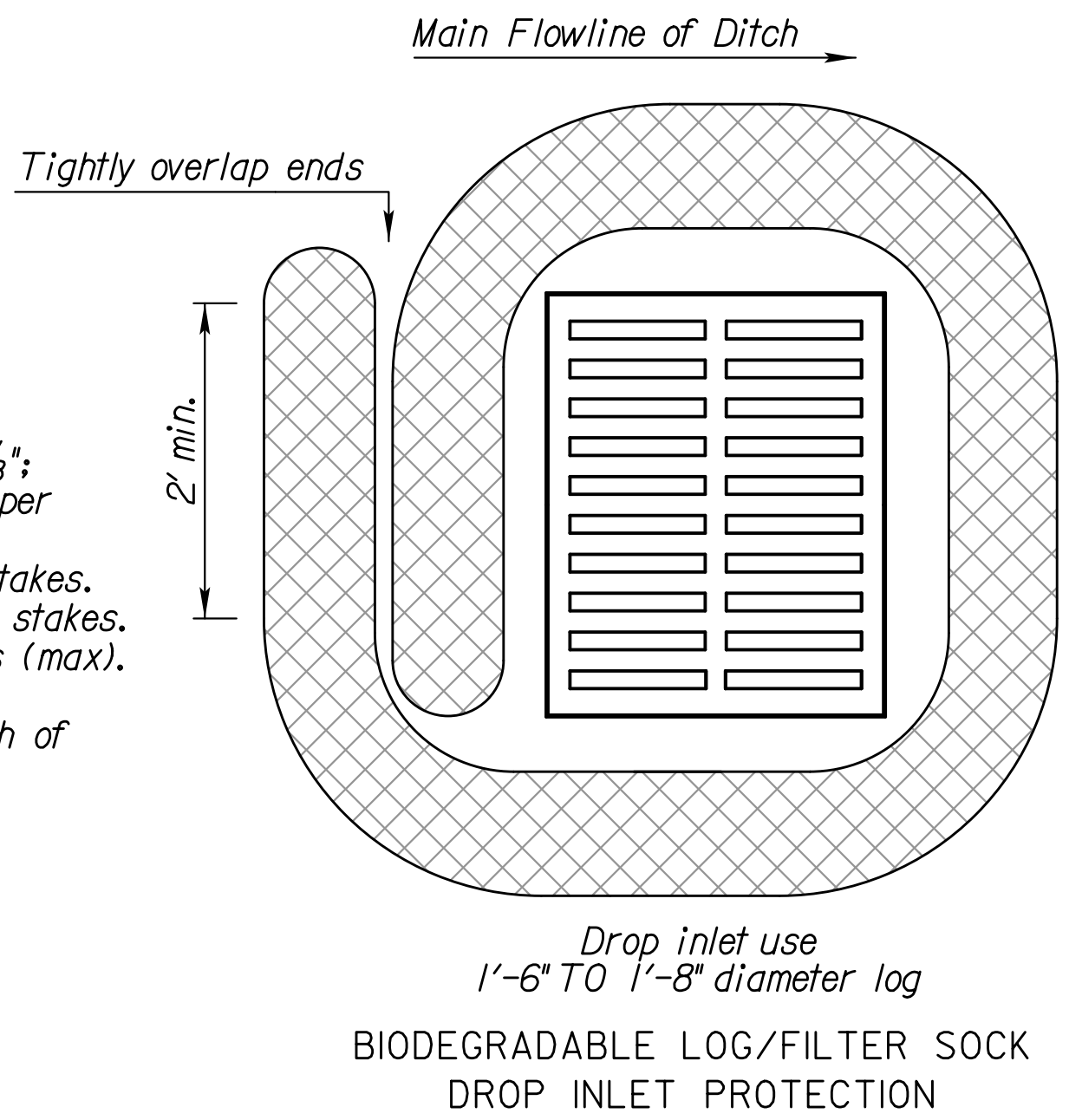


SECTION B - B



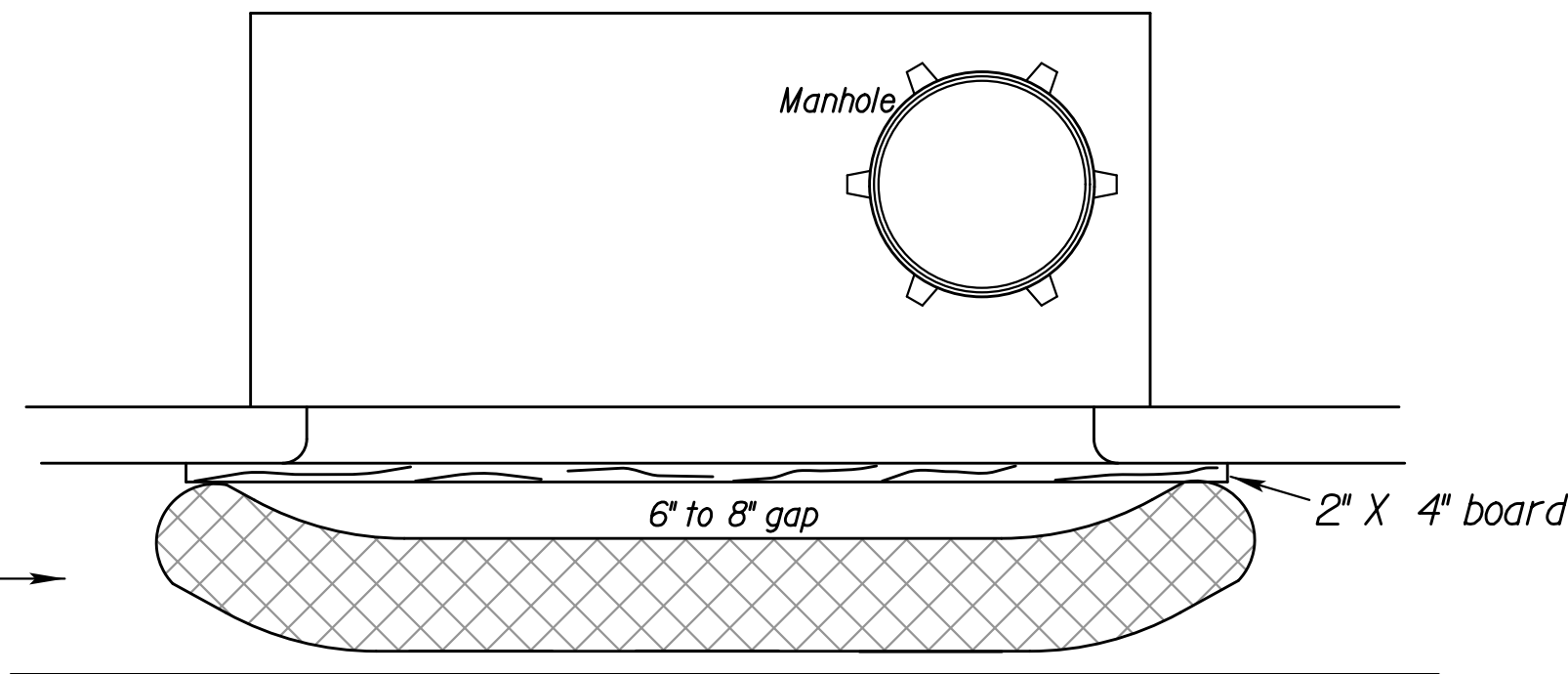
PLAN
TEMPORARY INLET SEDIMENT BARRIER
(SILT FENCE METHOD)
NO SCALE

- SILT FENCE:**
1. Stakes shall be 4' (min.) long and of one of the following materials:
 - a. Hardwood - 1 3/16" x 1 3/16";
 - b. Southern Pine (No. 2) - 2 5/8" x 2 5/8";
 - c. Steel U, T, L, or C Section - .95 lbs. per 1'-0"; or
 - d. Synthetic - same strength as wood stakes.
 2. Cross pieces shall be of same material as stakes.
 3. Attach fence fabric securely on 6" centers (max).
 4. Use of high flow material is acceptable.
 5. Refer to plan sheets to estimate the length of silt fence required.



Bags = synthetic net (3mm mesh) or burlap bags

Rock = approximately 1" to 2" diameter



CURB INLET PROTECTION

1. If multiple gravel bags are required, place them in such a way that no gaps are evident.
2. Height of bags (8" minimum diameter) must not be above top of curb.
3. Alternative products may be used other than gravel bags such as the "Gutter Buddy". Products must be approved by the Engineer.
4. Curb inlet protection will be measured and paid for as Filter Sock.

Note: 25% of log shall be keyed into ground during installation.
Stake every 4'

Material Requirements	
Use 100% shredded mulch or other non-compost biodegradable material as fill for logs.	
No compost or fines.	
No hay or straw.	
Do not use material which prohibits water infiltration.	
Log Mesh:	
Use mesh with 1/4" openings or larger. Mesh must allow water infiltration but also hold fill material in place.	

NO.	DATE	REVISIONS	BY	APP'D
3	3/01/15	Revised Standard	RA	SHS
2	6/01/13	Revised Standard	MRM	SHS
1	3/01/13	Revised Standard	MRM	SHS
KANSAS DEPARTMENT OF TRANSPORTATION				
TEMPORARY EROSION AND POLLUTION CONTROL				
TEMP. INLET SEDIMENT BARRIER (SILT FENCE)				
TEMP. INLET SEDIMENT BARRIER (T.S.D.)				
CURB INLET PROTECTION				
DROP INLET PROTECTION				
LA852C				
FHWA APPROVAL 3/10/2015 APP'D Scott H. Shields				
DESIGNED	RA	DETAILED	RA	QUANTITIES
DESIGN CK.	SHS	DETAIL CK.	SHS	QUAN. CK.

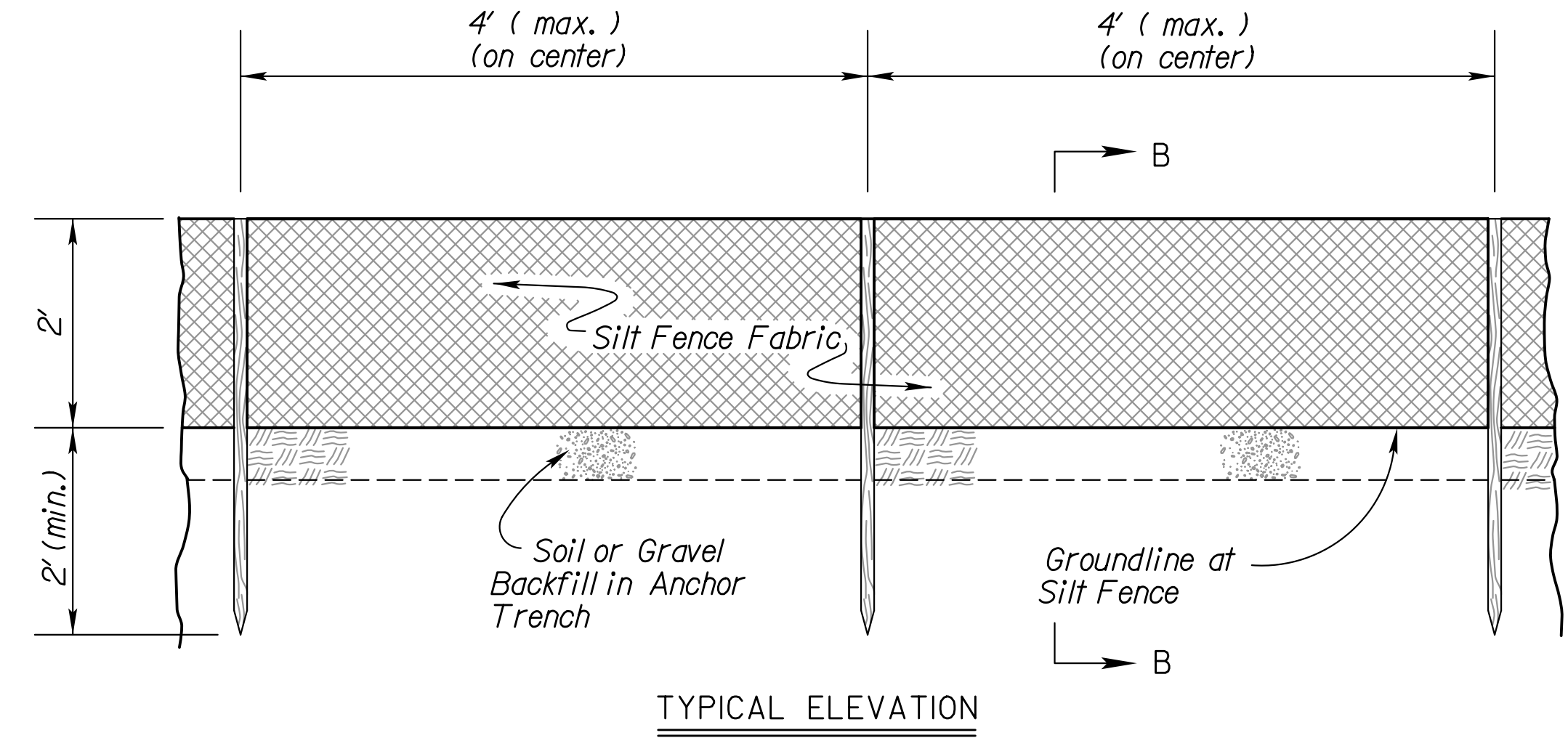
Std. Base File:

Plotted By: melissa

File: la852d.dgn

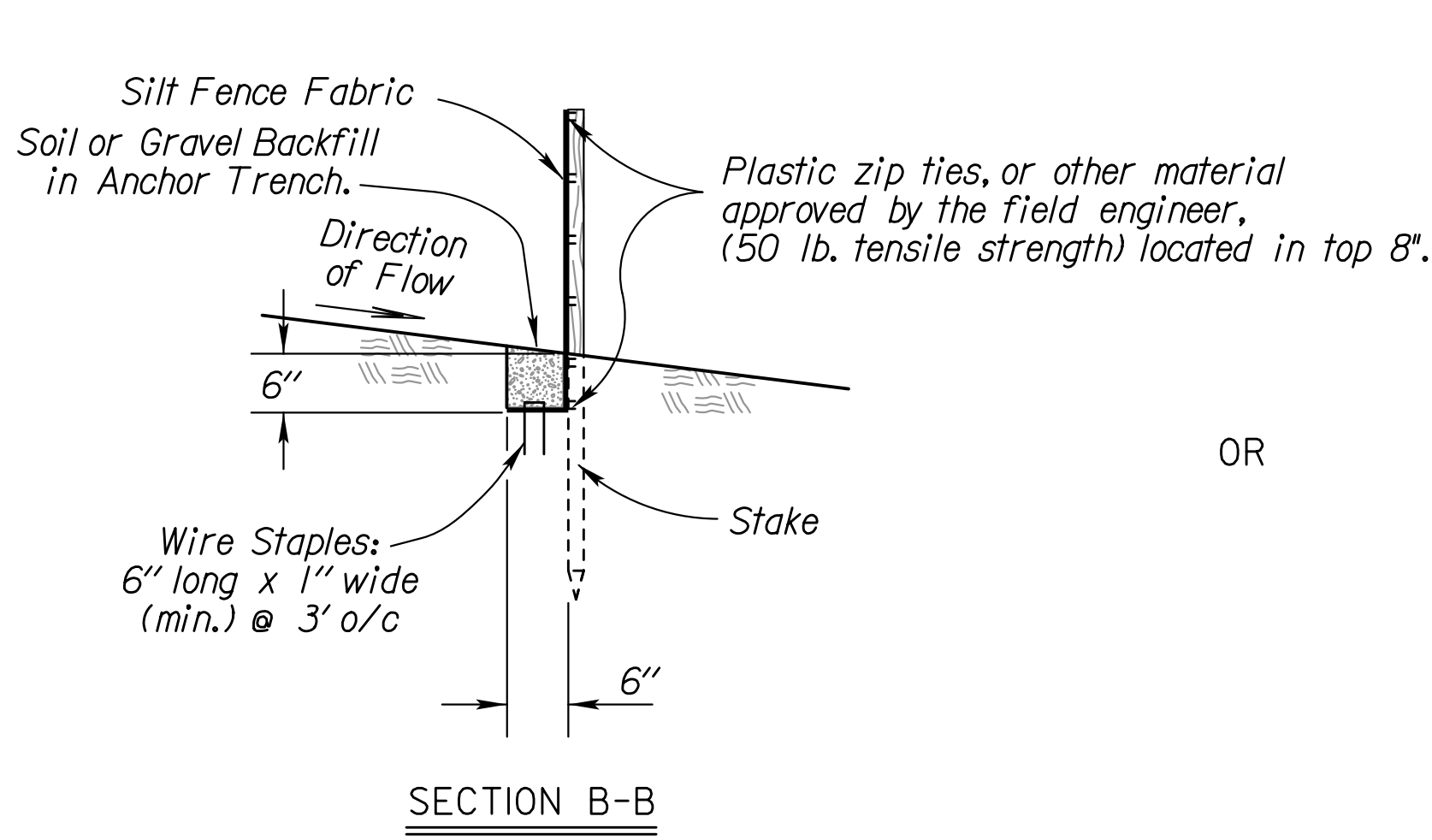
Plot Location: Landscape

Plot Date: 12-MAR-2015 11:30



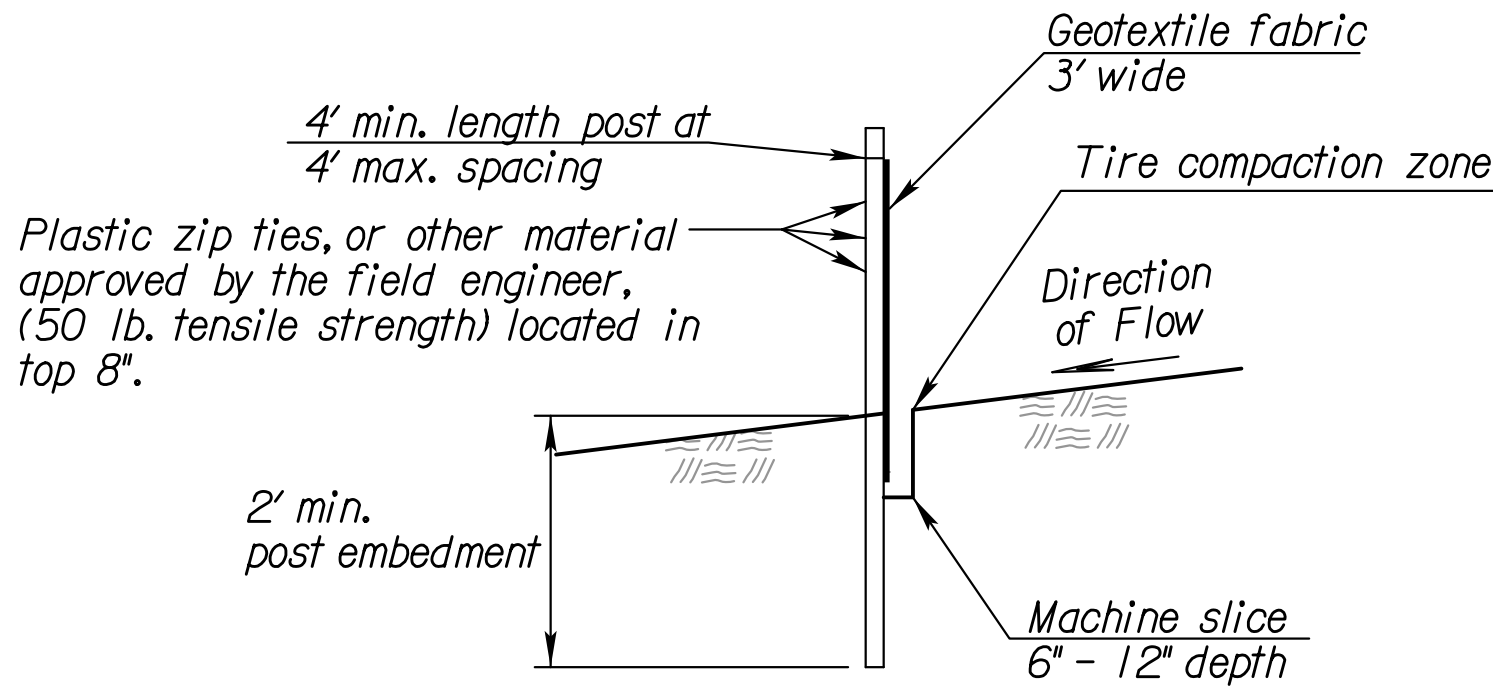
TYPICAL ELEVATION

SILT FENCE SLOPE BARRIER
NO SCALE

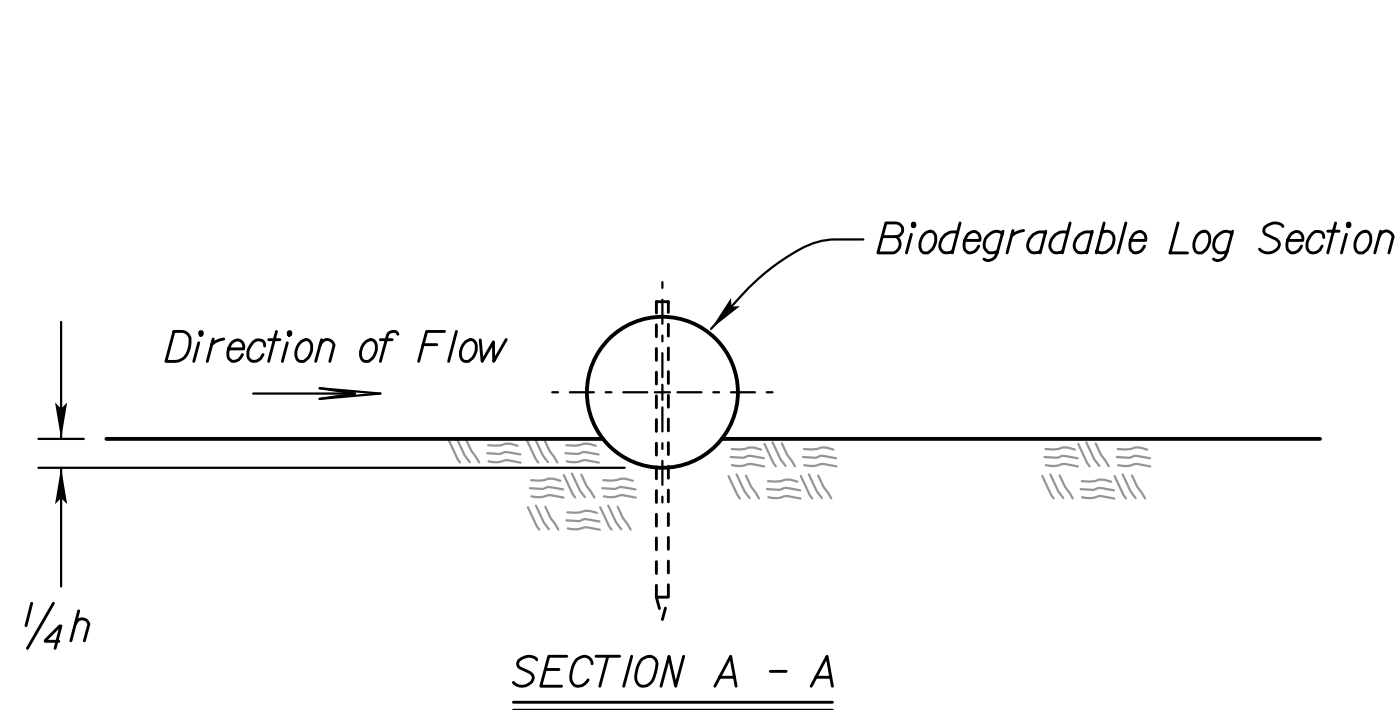


SECTION B-B

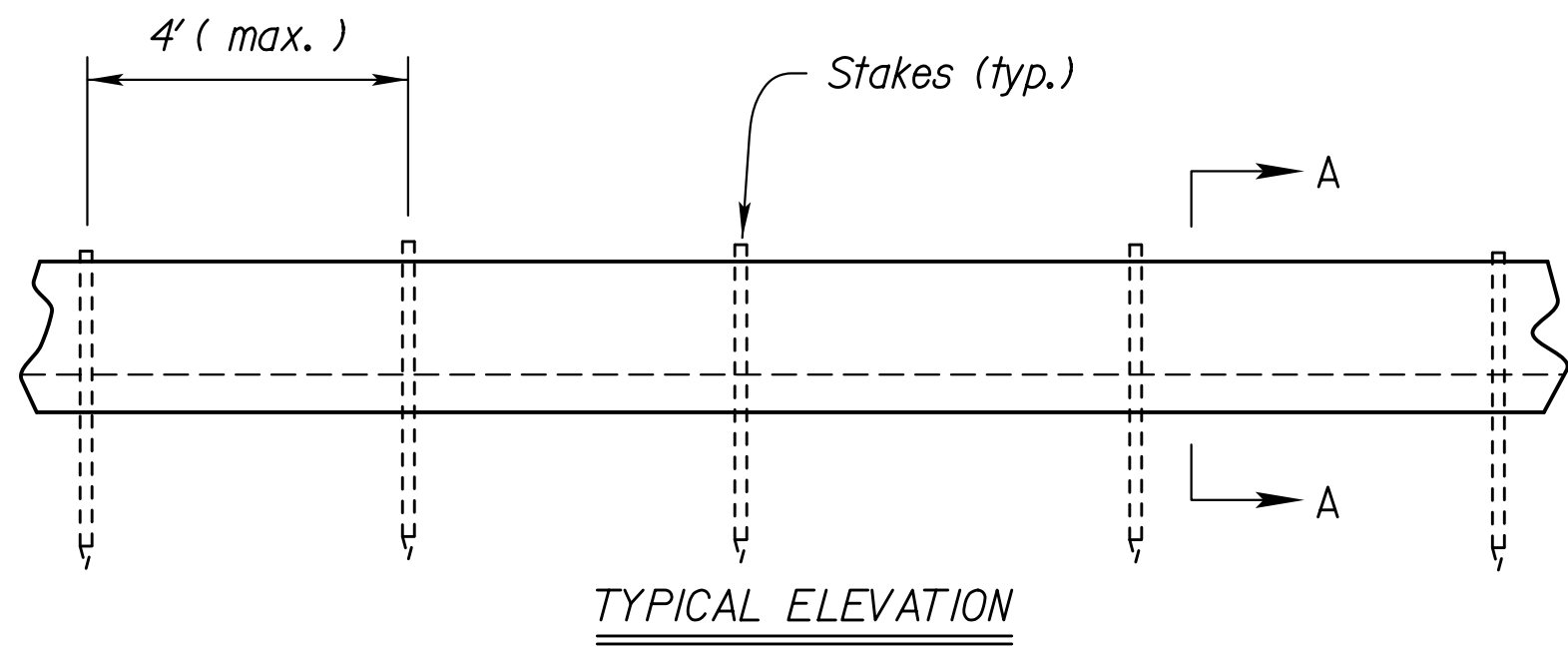
OR



SECTION B-B



SECTION A - A



TYPICAL ELEVATION

BIODEGRADABLE LOG SLOPE BARRIER
NO SCALE

INSTALLATION NOTES

- SILT FENCE:
- Stakes shall be 4' (min.) long and of one of the following materials:
 - Hardwood - 1 3/16" x 1 3/16";
 - Southern Pine (No. 2) - 2 5/8" x 2 5/8";
 - Steel U, T, L, or C Section - .95 lbs. per 1'-0"; or
 - Synthetic - same strength as wood stakes.
 - Cross pieces shall be of same material as stakes.
 - Attach fence fabric securely on 6" centers (max.).
 - Use of high flow material is acceptable.
 - Refer to plan sheets to estimate the length of silt fence required.

BIODEGRADABLE LOG BARRIERS

- Place biodegradable logs tightly together.
- Wood stakes shall be 2" x 2" (nom.).
- Wire staples shall be 6" long x 1" wide (min.) and placed on 4' (max.) centers.
- Refer to plan sheets to estimate length of biodegradable log barriers required.
- Logs should be keyed into the ground at a minimum of 25% of its height.
- Length of stakes should be 2 times the height of the log at a minimum.

Biodegradable Logs, Straw Wattles
& Sediment Logs

PRODUCT				
		9" Sediment Log & 9" Straw Wattle (ft)	12" Sediment Log & 12" Straw Wattle (ft)	20" Sediment Log & 20" Straw Wattle (ft)
Slope Gradient	≤4H:1V	40	60	80
	3H:1V	30	45	60
	2H:1V	20	30	40
	1H:1V	10	15	20

BIODEGRADABLE LOG MATERIAL		
	LOW FLOW	HIGH FLOW
9"	Straw/Compost	Excelsior / Wood Chips / Coconut Fiber
12"	Straw/Compost	Excelsior / Wood Chips / Coconut Fiber
18"-20"	Straw/Compost	Excelsior / Wood Chips / Coconut Fiber

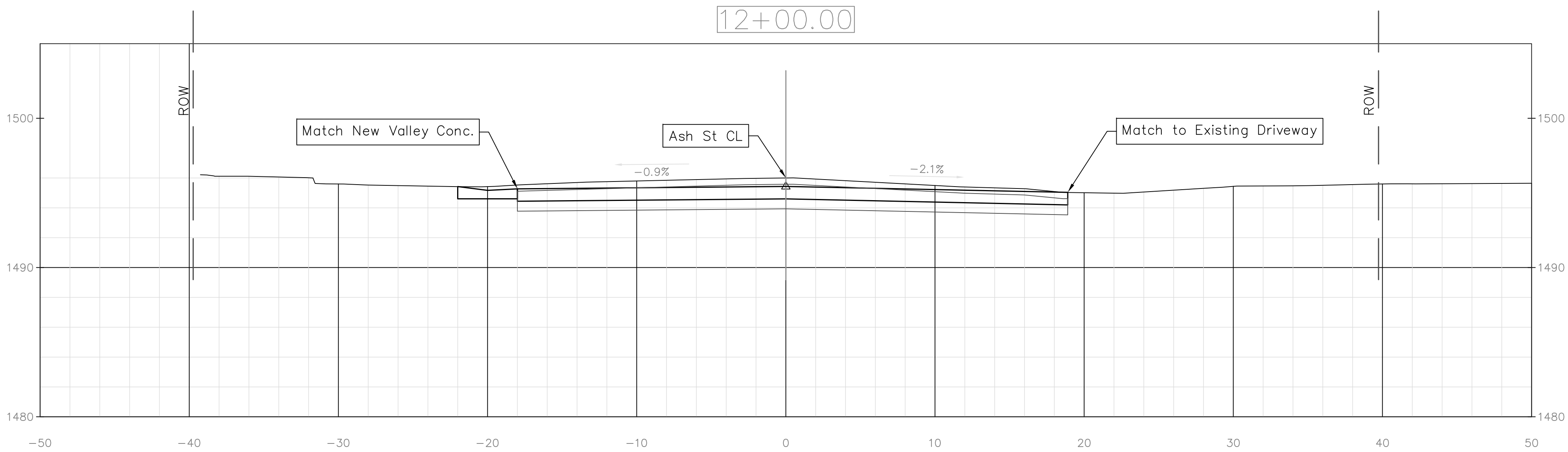
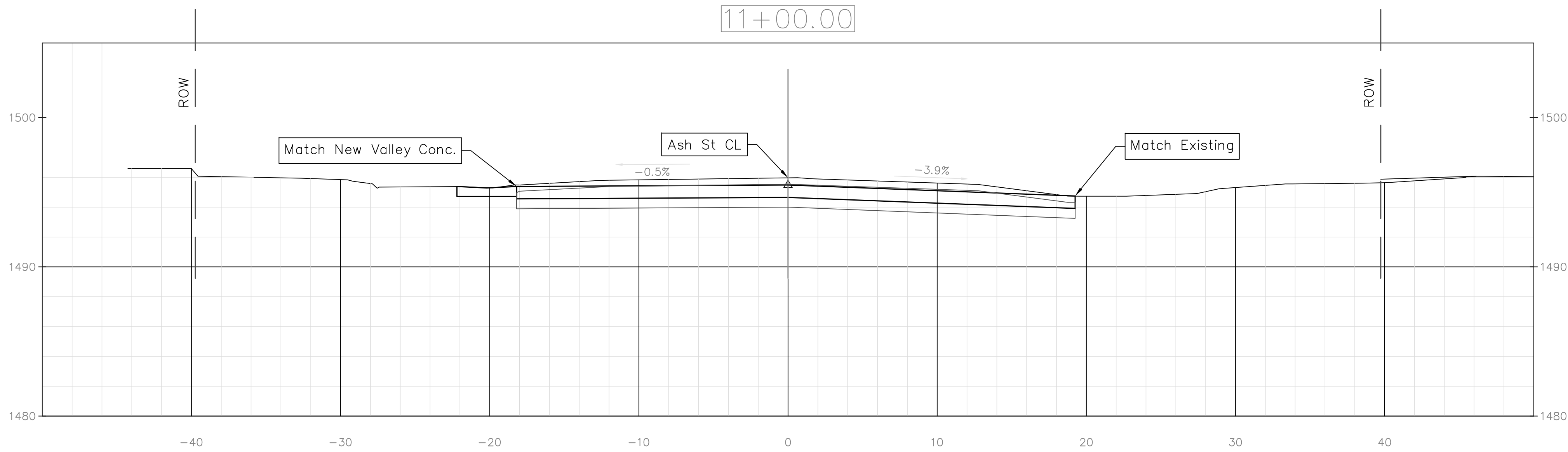
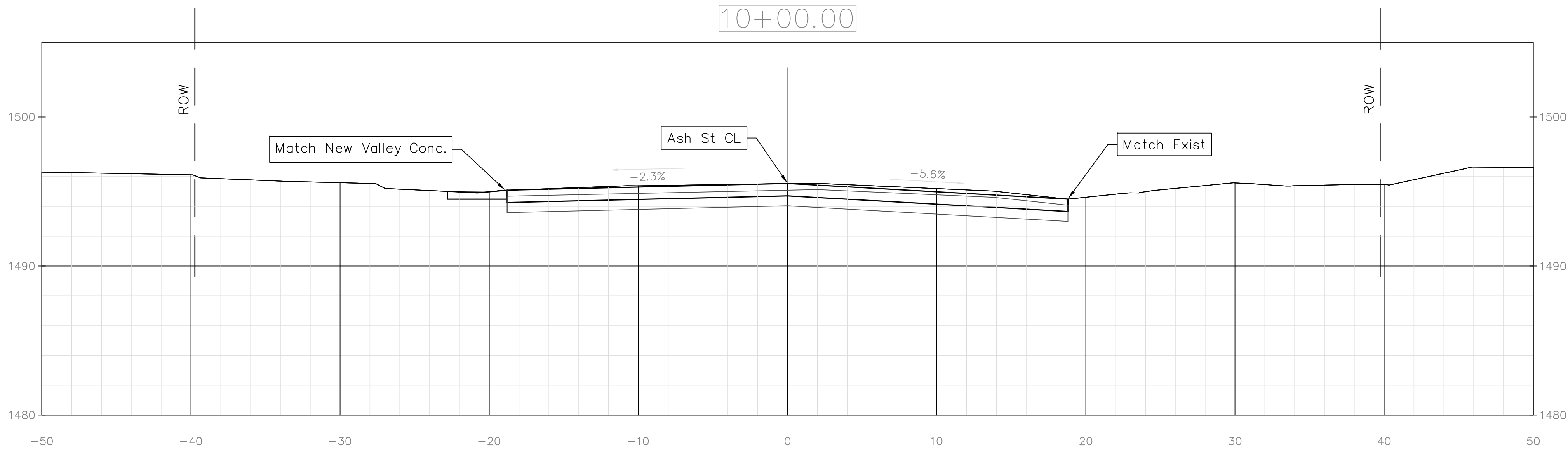
9" and 12" material should only be used in areas which have been seeded and mulched. 20" material should be used in all other areas. Deviations should be approved by the Field Engineer.

GENERAL NOTES

- The slope barriers shall be placed along contour lines, with a short section turned upgrade at each end of the barrier. The maximum length of the slope barrier shall not exceed 250 feet, and the barrier ends need to be staggered.
- At culverts, the Silt Fence shall be placed over the culvert, not through the streambed flowline.
- Barriers damaged by Contractor's negligence, including improper maintenance or lack of maintenance, shall be repaired immediately by Contractor at no additional cost to KDOT.
- Agricultural products, such as native prairie hay, used for mulching and erosion control practices, excluding wood based mulch, shall meet the North American Weed Free Forage Standards.

3	3/01/15	Revised Standard	RA	SHS
2	6/01/13	Revised Standard	MRM	SHS
1	3/01/13	Revised Standard	MRM	SHS
NO.	DATE	REVISIONS	BY	APP'D

KANSAS DEPARTMENT OF TRANSPORTATION				
TEMPORARY EROSION AND POLLUTION CONTROL				
SILT FENCE SLOPE BARRIERS				
BIODEGRADABLE LOG SLOPE BARRIERS				
LA852D				
FHWA APPROVAL		3/10/2015	APP'D	Scott H. Shields
DESIGNED	RA	DETAILED	RA	QUANTITIES
DESIGN CK.	SHS	DETAIL CK.	QUAN. CK.	CADD CK.



DATE	REVISIONS	BY	APP'D

Cross Sections
10+00 - 12+00

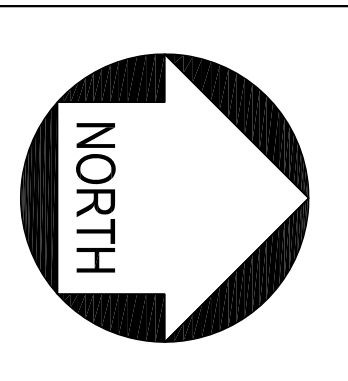
VERTICAL SCALE
1" = 10'
1/32" = 1'

HORIZONTAL SCALE
1" = 40'
1/16" = 5'

1 2 3 4

McPHERSON
CITY OF McPHERSON, KANSAS
PUBLIC WORKS DEPT.

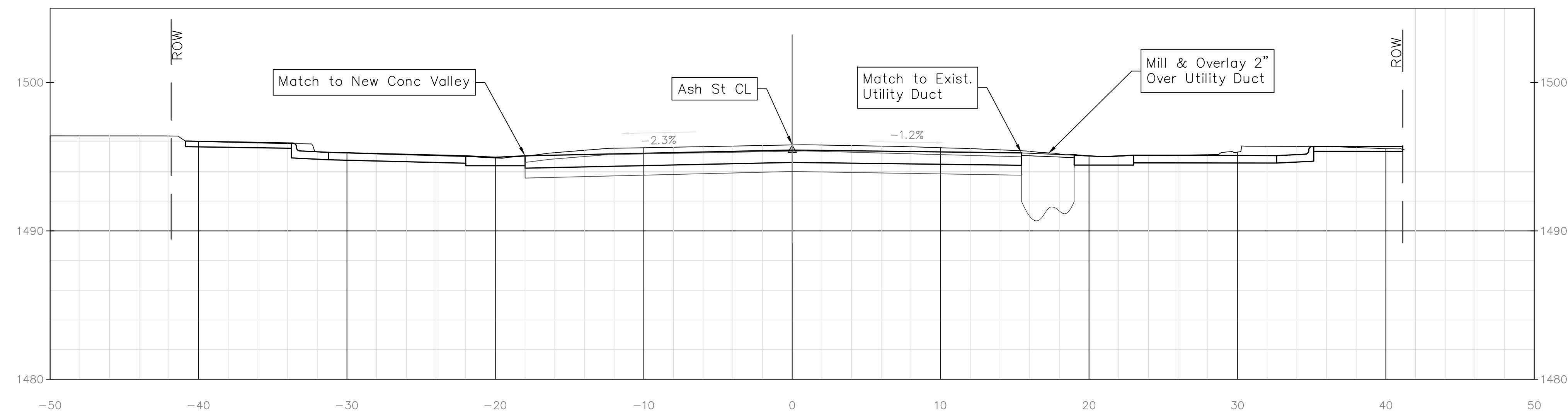
FINAL PLANS
APPROVED FOR
CONSTRUCTION



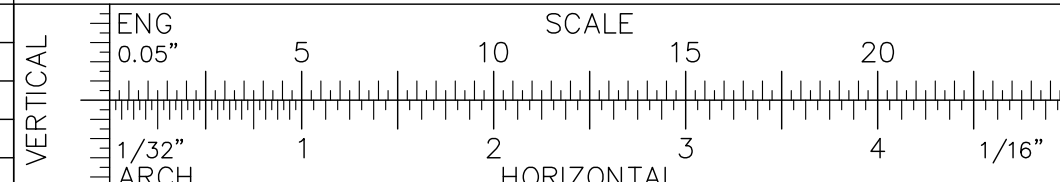
PLANS	BY	DATE
SURVEY:		
DESIGNED:	JustinW	3/21/2016
DRAWN:	JustinW	3/21/2016
CHECKED:	JeffW	2016-05-06
PLOT BY:	JustinW	2016-05-06
FILE:	2016-05-03_AshStDesign.dwg	

PROJECT NO:	PW-024	SHEET	24	OF	27
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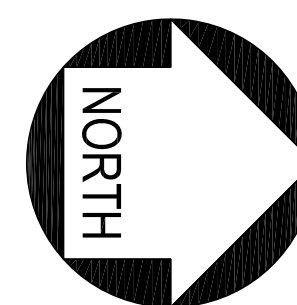
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Cross Sections
13+38.96 - 15+00

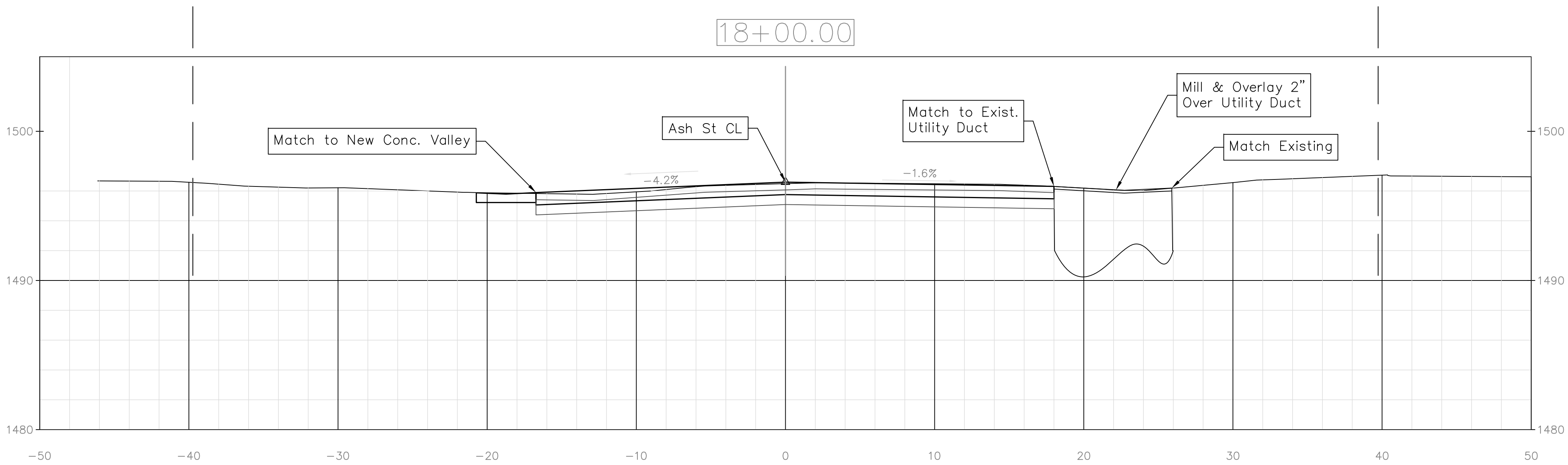
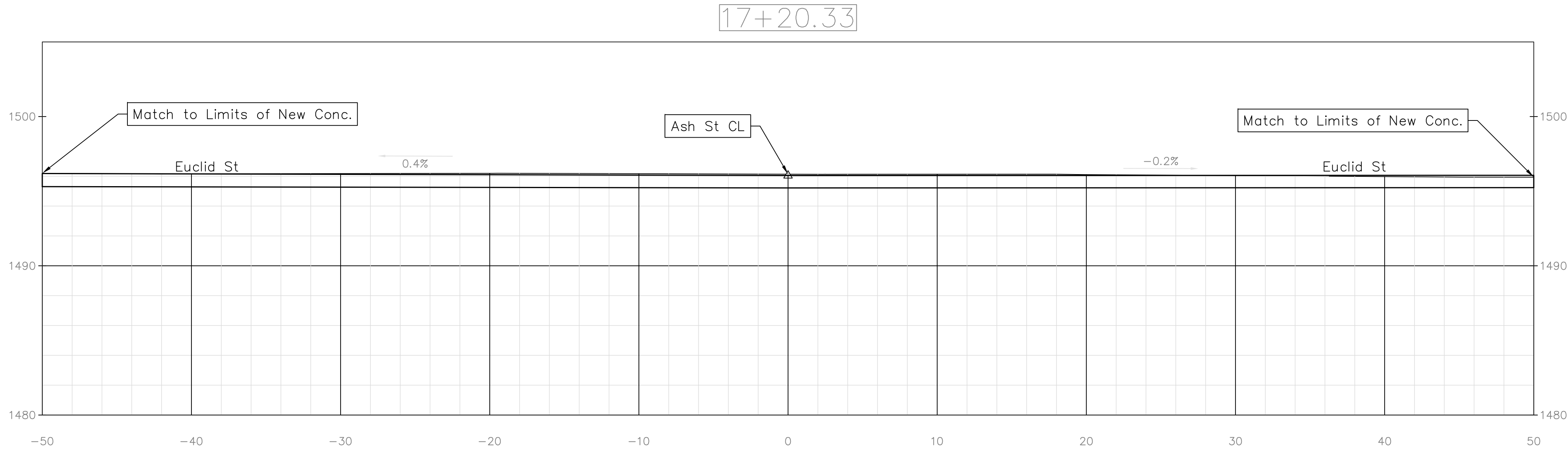
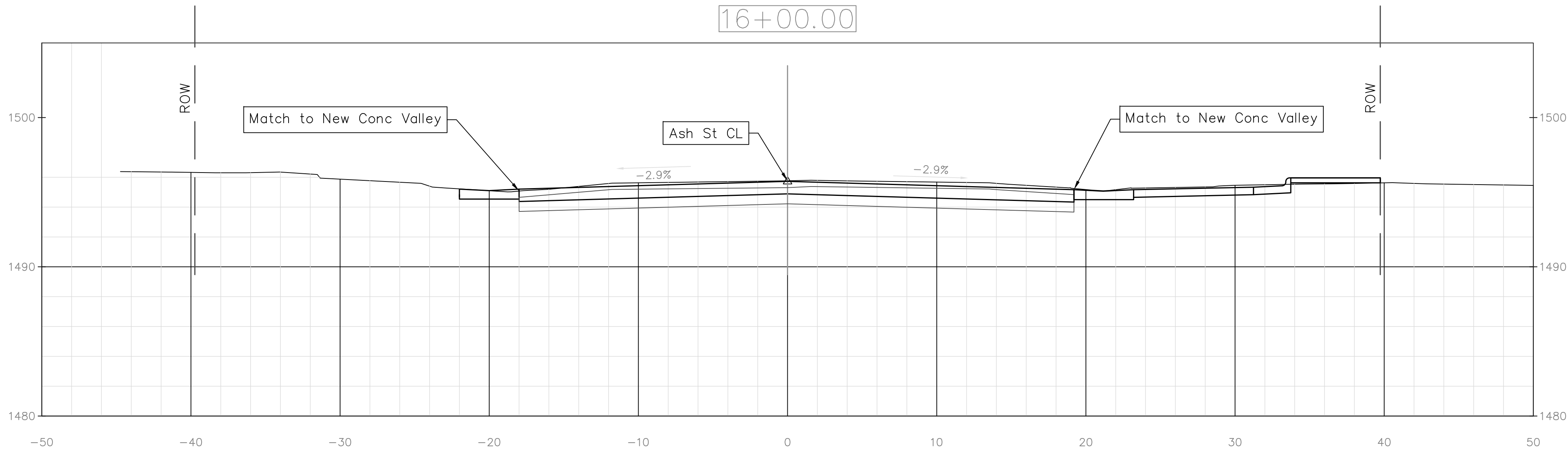


McPHERSON
CITY OF McPHERSON, KANSAS
PUBLIC WORKS DEPT.

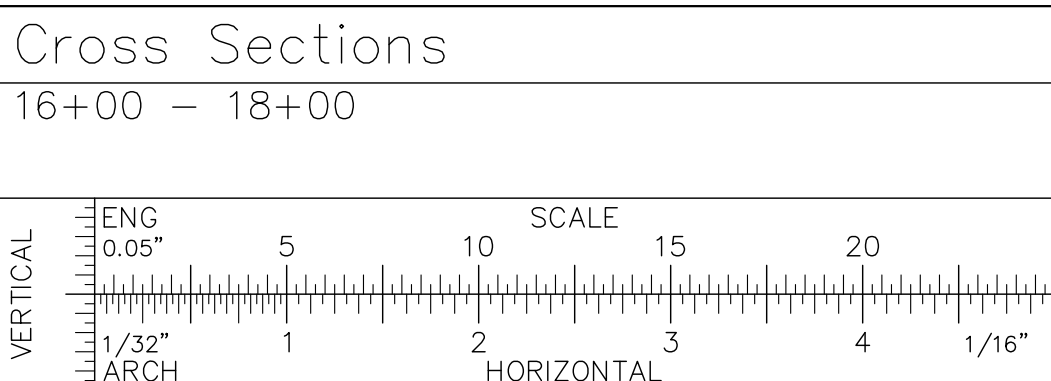
FINAL PLANS
APPROVED FOR
CONSTRUCTION



PLANS		BY		DATE	
SURVEY:					
DESIGNED:		JustinW			
DRAWN:		JustinW		3/21/2016	
CHECKED:		JeffW		3/21/2016	
PLOT BY:		JustinW		2016-05-06	
FILE:		2016-05-03_AshStDesign.dwg			
PROJECT NO:		PW-024		SHEET 25 OF 27	



DATE	REVISIONS	BY	APP'D

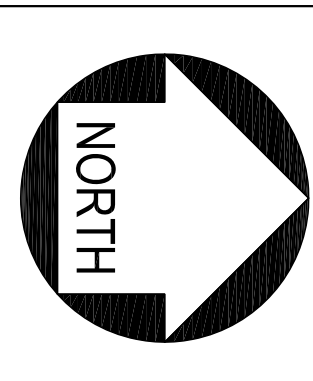


McPHERSON

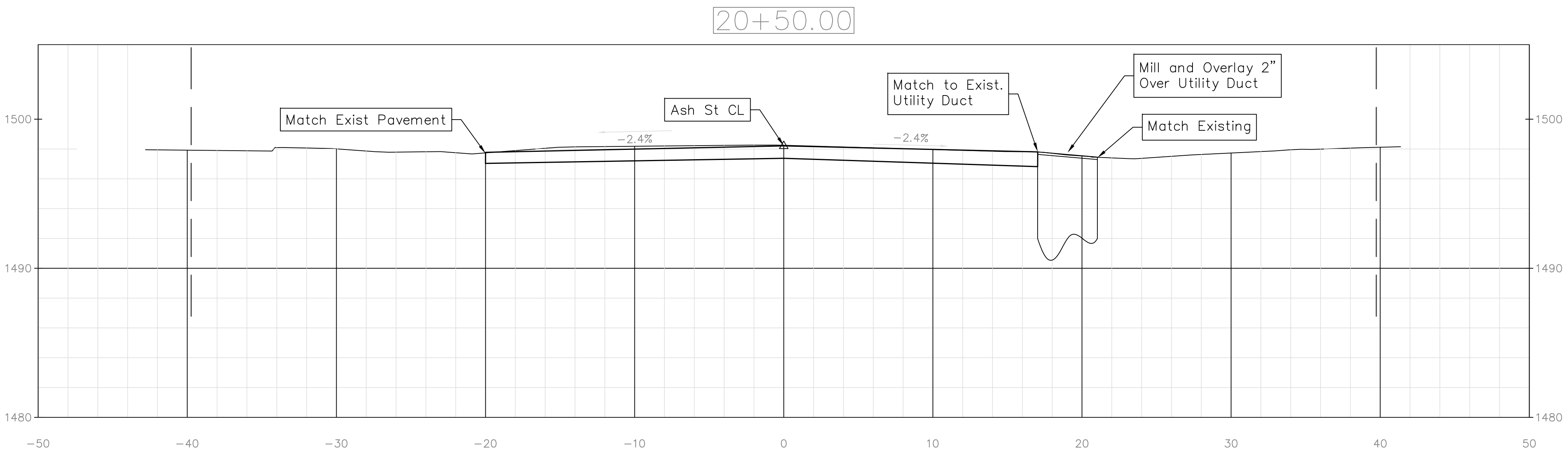
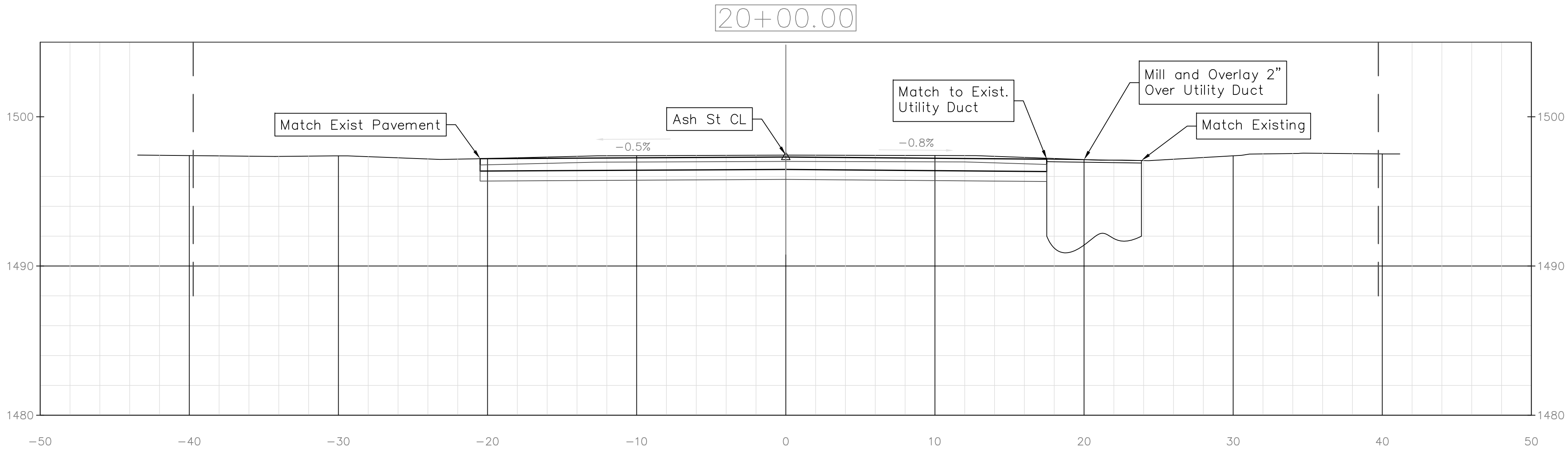
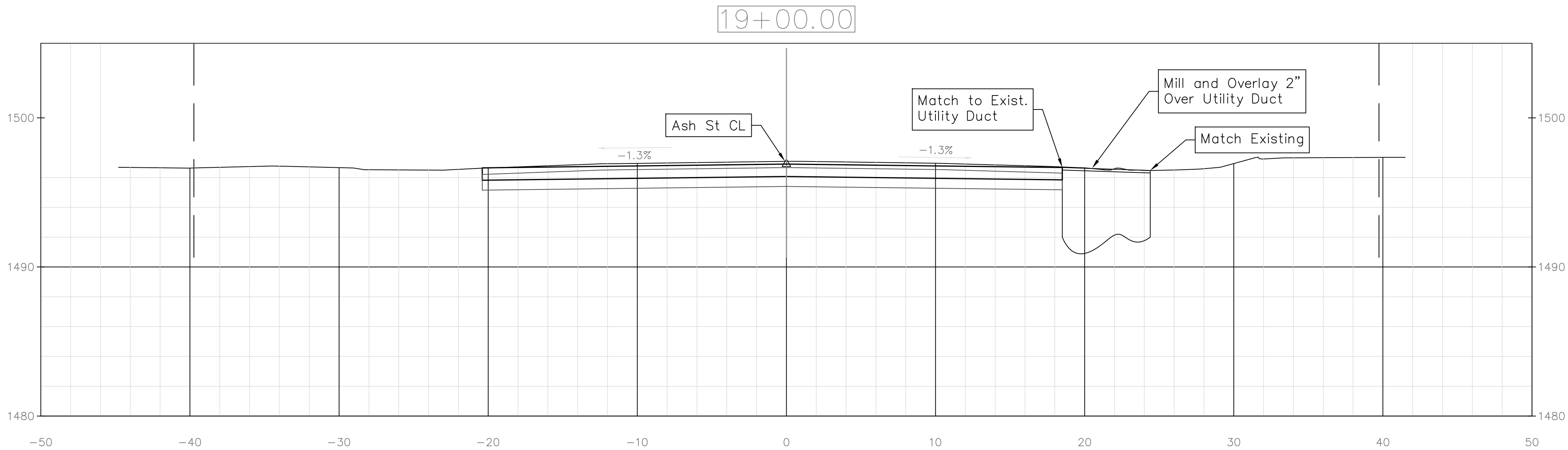
CITY OF McPHERSON, KANSAS
PUBLIC WORKS DEPT.

FINAL PLANS

APPROVED FOR
CONSTRUCTION

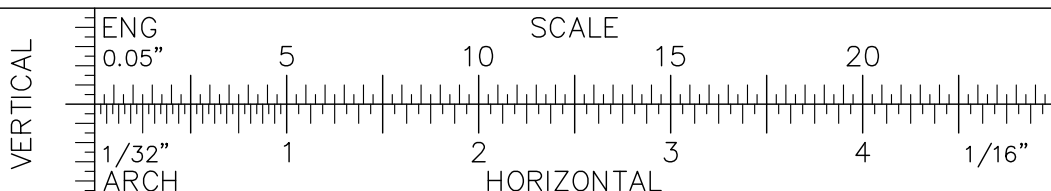


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SURVEY:		
DESIGNED:	JustinW	
DRAWN:	JustinW	3/21/2016
CHECKED:	JeffW	2016-05-06
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FILE:	2016-05-03_AshStDesign.dwg	
PROJECT NO:	PW-024	
SHEET	26	OF 27



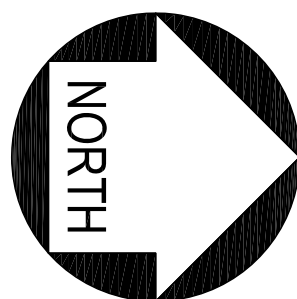
DATE	REVISIONS	BY	APP'D

Cross Sections
19+00 - 20+50



McPHERSON
CITY OF McPHERSON, KANSAS
PUBLIC WORKS DEPT.

FINAL PLANS
APPROVED FOR
CONSTRUCTION



PLANS	BY	DATE
SURVEY:		
DESIGNED:	JustinW	
DRAWN:	JustinW	3/21/2016
CHECKED:	JeffW	2016-05-06
PLOT BY:	JustinW	2016-05-06
FILE:	2016-05-03_AshStDesign.dwg	
PROJECT NO:	PW-024	SHEET 27 OF 27